PARKS CANADA
IDENTITY PROGRAM

Exterior Signage

Standards and Guidelines

VERSION 1
MARCH 2007

Parks Canada Parcs Canada

Canada

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IDENTITY PROGRAM

Exterior Signage

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VERSION 1
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Canada, 2007.

Library and Archives Canada Cataloguing in Publication

Parks Canada

Parks Canada Identity Program
Exterior Signage: Standards and Guidelines

Issued also in French under the title:
Programme de l'image de marque de Parcs Canada
Signalisation extérieure : normes et lignes directrices

ISBN 978-0-662-45715-2

Cat. no.: R62-393/2007E

1. Park signs–Canada–Handbooks, manuals, etc.
2. National parks and reserves–Canada–
Communication systems.
3. Parks Canada.
I. Title.

GV191.44.P36 2007

353.7'8

C2007-980088-2

Front Cover Image:
© Parks Canada

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Introduction

Introduction

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1.1 Overview

The fundamental idea behind a corporate identity program is that everything the institution does, everything it owns, every service it provides, should project a clear idea of what the institution and its goals are.

Treasury Board of Canada Secretariat

A strong Parks Canada Identity Program will ensure that Canadians recognize at a glance their government at work for them protecting, presenting, and fostering enjoyment of Canada's heritage places for present and future generations. A good sign system is not only important to transmit information and ensure the safety of visitors, but also vital to Parks Canada's public image and identity. A coordinated and effective identity program will enable Parks Canada to be identified in a distinct, consistent way the public can recognize in all circumstances. It helps symbolize the values, goals and dedication Parks Canada strives for in protecting and presenting nationally significant examples of Canada's natural and cultural heritage.

Whether it is a primary identification sign at the entrance to a national park, a trailblazer sited along a roadway directing visitors to a national historic site or a directional sign providing visitors with essential information as they navigate through a national marine conservation area, signage is an integral element of the Parks Canada experience and a highly visible component of its identity. Signage also plays an important role in managing the human impact on sensitive natural environments and fragile cultural resources that Parks Canada is entrusted to protect for future generations.

Finally, the Parks Canada signage system plays a fundamental role in the visitor experience by:

- enabling visitors to find Canada's national parks, national historic sites and national marine conservation areas;
- ensuring that visitors, once they arrive, can enjoy and navigate these heritage places in a safe and effective way; and
- ensuring that essential information needed for visitors to experience Canada's natural and cultural heritage is properly and effectively conveyed.

Parks Canada is subject to the Government of Canada's Federal Identity Program (FIP) and so must manage its signage program within the spirit and framework established by FIP. Parks Canada's signage requirements regarding layout, typeface, graphic symbols, colours and the visual equality of both of Canada's official languages have been informed by FIP as well as human factors testing in regard to legibility, comprehensibility and safety. All Parks Canada administered sites are required to follow these standards and guidelines for all exterior signage requirements.

1.2 A System Approach

Signs function in the context of their environment and inform visitors about important elements within a park, site or marine conservation area. Signs serve to inform, guide, direct, regulate and warn visitors as they experience Canada's heritage places. When well planned and consistently implemented, signs function as part of a comprehensive information and wayfinding system. A successful signage system must take into consideration a multitude of factors relating to human behaviour, information processing, and the environment in which the signs are situated. In short, an effective signage system must take the needs of the user into consideration.

In addition to the general requirements that make up any signage system, Parks Canada is subject to various Government of Canada legislative and policy requirements, as well as various provincial and municipal requirements. Most notably, signage is considered a mandatory application under the Federal Identity Program and Parks Canada must manage its signage program within this framework. Other legislative and policy requirements include but are not limited to:

- Canada National Parks Act, 2000 and Regulations
- Canada Shipping Act, 2001
- Canadian Aids to Navigation System
- Canadian Charter of Human Rights and Freedoms
- Communications Policy of the Government of Canada
- Federal Identity Program — Policy, Guidelines and Standards
- Manual of Uniform Traffic Control Devices for Canada, 4th Edition
- Official Languages Act, related requirements and policies
- Provincial/territorial uniform traffic control devices manuals
- Treasury Board Real Property Accessibility Policy

It is important to note that Parks Canada's signage standards should be used in conjunction with relevant legislation, regulations and provincial and municipal standards. Section 2 provides more information on the legislative and policy requirements pertaining to signage.

The Parks Canada signage system has been developed to meet the objectives of government policy, Parks Canada's specific requirements and, most importantly, the needs of the visitor. Visitors and employees with special needs must be considered when planning signage for a park, site, marine conservation area or facility within. Their requirements may influence a sign's location, placement and/or size.

1.3 Objective

The objective of these standards and guidelines is to outline Parks Canada's requirements with respect to exterior signage and provide direction to sites on addressing signage requirements. The application of these standards and guidelines will strengthen federal presence and enhance Parks Canada's identity by enabling Parks Canada to clearly distinguish itself from provincial and other parks and historic sites.

The Parks Canada signage system is designed with the following goals in mind:

- to enable visitors, from Canada and around the world, to recognize Parks Canada facilities clearly and instantly by means of a uniform sign system that communicates information effectively and efficiently;
- to ensure that visitors and park/site users can find, and navigate safely and easily within, Canada's national parks, national historic sites and national marine conservation areas;
- to ensure that signs function within the context of their environment and do not detract from the visitor experience or harm the ecological and commemorative integrity of the parks and sites in which they are situated;
- to ensure that signage sited within individual national parks, national historic sites and national marine conservation areas is designed to meet the requirements of the visitor;
- to ensure that Parks Canada's signage clearly and consistently addresses visitor safety; and
- realize cost savings through standardization of both design and material and to improve the effectiveness and efficiency of the design, fabrication, installation and maintenance of Parks Canada's operational signs.

These standards and guidelines are based on operational, policy and human factors requirements.

Questions or comments regarding these standards and guidelines or requests for more information related to the testing and development of these standards may be addressed by e-mail to the Parks Canada Identity Program Officer at pimpc-pcip@pc.gc.ca.

1.4 Scope

These standards and guidelines set out Parks Canada's signage requirements and provide direction related to planning, designing, fabrication and installation of signage for national parks, national historic sites and national marine conservation areas administered by Parks Canada. This includes signs situated on-site or off-site designed for direction, location, information, regulatory, warning, emergency and special purposes.

It is important to note that these standards and guidelines **do not** address interior signage within Parks Canada buildings. Further clarification from Treasury Board on Parks Canada's approved colours for interior signage is still pending. In addition, these standards and guidelines do not provide specifications for signs that are prescribed by other regulatory authorities, such as traffic signs subject to provincial/territorial standards (ex. Manual of Uniform Traffic Control Devices for Canada) and navigational aids for historic canals which must conform to the Canadian Aids to Navigation System.

These standards and guidelines have been approved by the Parks Canada Executive Board and supersede the *Parks Canada Sign Manual, 1975* and the *Canadian Parks Service Sign Manual, Working Draft – June 7, 1993*. All new Parks Canada signs must conform to these standards and guidelines which will be updated as required. Parks, sites and marine conservation areas must therefore ensure they have the latest updates prior to beginning any new signage initiative.

1.5 Parks Canada Identity Program

As signage is a mandatory application under the Federal Identity Program, and is a key element of the Parks Canada Identity Program, it is essential that all appropriate identity elements used on signage follow approved standards. To ensure consistency in presentation and comprehension, only approved digital master artwork files for these symbols can be used. The Parks Canada identity elements to be used on signage include:

1. The "Canada" wordmark

   - Used on primary identification signs and approved special-purpose signs

Canada

2. Parks Canada signature

   - Used on various sign types to demonstrate the authority of the sign

Parks Canada Parcs Canada

3. Parks Canada identifier (Beaver symbol + the "Canada" wordmark)

   - Used on off-site highway advance signs, trailblazer signs and boundary marker signs only

Canada

4. Parks Canada beaver symbol

- Used on approved on-site special-purpose signs only, e.g. fee signs, fees at work signs, welcome signs, day-use area signs, street name signs

5. Parks Canada symbol set

- Adapted from the National Standard of Canada CAN/CSA-Z321

6. Parks Canada colours

   Parks Canada has received special permission from Treasury Board to have green and white as the approved colours for exterior signage. The basic colours of the Parks Canada signage system are:

   - Parks Canada Heritage Green
   - White retroreflective lettering

   Other colours include:

   - Black
   - FIP Red
   - CSA Yellow
   - CSA Blue
   - CSA Green
   - CSA Red
   - Boundary Marker Yellow

7. Typeface

   The two typefaces below are the only typefaces permitted on Parks Canada signs:

   - Helvetica Neue 75© is used for the Parks Canada signature and sign messages.
   - Palatino Bold Italic© is used only for the welcome messages on approved special-purpose signs.

1.6 Elements of the manual

Parks Canada Standards and Guidelines for Exterior Signage is divided into several sections in order to facilitate the entire signage process—from sign planning through to installation and maintenance. While some sections are aimed at specific audiences, it is important that all park and site staff responsible for signage become familiar with the entire manual.

The following provides a brief overview of each section and highlights some of the key elements to be aware of.

Section 2 – Legislative and Policy Requirements
This section provides an overview of the principal legislative and policy requirements that influence and impact the Parks Canada signage system. In addition, it provides context and rationale related to Parks Canada's approach to signage. This section is to be used in conjunction with other applicable policies and guidelines to which Parks Canada must adhere.

Section 3 – Sign Planning
Perhaps the most important element of an effective signage system is the development of a comprehensive sign plan. Sign planning, or wayfinding, helps facilitate the visitor experience by creating a signage system that effectively communicates, both verbally and visually, essential information to the visitor.

This section highlights the objectives of a sign plan, introduces the Parks Canada signage system and its classification system and provides parks and sites with principles, tools and templates to facilitate the sign planning process. It is essential that all Parks Canada staff involved in signage become familiar with this section prior to beginning any signage project.

Section 4 – Layout System
Aimed specifically at those responsible for designing Parks Canada's signs, Section 4 provides detailed information on how to lay out Parks Canada signs. This section introduces the design approach developed for Parks Canada and provides essential information on each sign element and its specific layout requirements.

It is not expected that all Parks Canada staff learn how to design a Parks Canada sign. However, it is essential that those responsible for reviewing and approving signs are familiar enough with the layout standards to perform the role of a knowledgeable client.

Section 5 – Material and Fabrication Specifications
This section provides detailed technical information on the material and fabrication requirements for Parks Canada signs. Information concerning substrate options, post requirements and sign sheeting specifications is provided. It is important that those employees responsible for ordering signs become familiar with the requirements in this section.

Section 6 – Installation and Maintenance
Installation and maintenance plays a critical role in creating an effective signage system. The "look" of a sign is not only derived from the design on the sign face, but also from how a sign is positioned within a specific environment. This section provides information on how to achieve a coherent system-wide signage look by providing consistent installation heights and requirements.

In addition, Section 6 provides information specific to vehicular signs that will ensure Parks Canada signs are installed according to appropriate standards, whether they be provincial or Canada-wide standards for vehicular signs. Finally, Section 6 provides context for why sign maintenance is essential for any signage system.

Appendix A – Symbols
The Parks Canada symbol set plays an essential role in visually communicating information to the visitor. Appendix A contains the approved symbol set for Parks Canada, which is to be used across all media, from signs to publications.

Appendix B – Standard Signage Messages
To facilitate consistent messaging on Parks Canada signs, various common messages have been included with associated translations. This section will assist the sign planning team during the planning process. This section will be updated and expanded on a regular basis to facilitate common messaging across the Parks Canada system.

Appendix C – Manual of Uniform Traffic Control Devices for Canada

Road signs within national parks, national historic sites and national marine conservation areas must conform to the appropriate traffic control standards of the province where the site is located. In an effort to introduce the sign planning team to the requirements of various traffic control standards, this section provides basic information on MUTCD signs and their application. This section is to be used in conjunction with the appropriate traffic control standards.

Appendix D – Sign Type Configurations

Appendix D provides a detailed breakdown of the sign classification system outlined in Section 3. It introduces layout options by sign type and provides information concerning sign function and application.

Detailed planning information is outlined to assist the sign planner in applying the optimum layout configuration.

Appendix E – Layout Specifications

Appendix E augments Section 4, Layout System, by providing detailed layout specifications for each sign type and layout option. The sign designer will be the primary user of Appendix E in the preparation of general assembly drawings.

Appendix F – Text Message Hierarchy

Hierarchy is a technique used to give emphasis to a text message element. A hierarchy is created by enlarging the primary text message and contrasting this against the secondary text message(s). This appendix provides the detailed layout specifications required to create a text message hierarchy.

Appendix G – Technical Drawings

Appendix G provides technical details on the various fabrication and installation techniques that form part of the Parks Canada signage system.

2

Legislative and Policy Requirements

Legislative and Policy Requirements

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2.6	Cultural Resource Management Considerations Relating to the Planning, Design, Location and Installation of Signs	9

2.1 Federal Identity Program

The framework established by the Federal Identity Program Policy is intended to maintain a coherent corporate identity for the Government of Canada as a whole, while at the same time allowing individual institutions to develop their own identity within this framework. Parks Canada is working closely with Treasury Board in developing its identity program to ensure that the identity program for Parks Canada complies with the spirit and intent of the Federal Identity Program.

The Federal Identity Program framework, as it relates to Parks Canada signage, consists of:

- Government of Canada corporate symbols *(Figure 2.1 – The "Canada" wordmark, Figures 2.2 and 2.3 – Parks Canada signature)*
- Visual equality of Canada's official languages
- Approved titles of institutions
- General rules on signature, typography, colours
- Design guidelines and standards

In view of Parks Canada's unique operational requirements, Treasury Board has granted Parks Canada some specific exceptions to the FIP standards for signage. Parks Canada is permitted to use green and white exterior signs *(Figure 2.4)*, rather than the standard FIP dark grey/light grey signs. As well, Parks Canada has received approval to incorporate the Parks Canada identifier *(Figure 2.5)*—Beaver symbol + "Canada" wordmark—on selected common-use and operational signs (trailblazers and boundary markers). This symbol must be used only on approved applications.

These standards and guidelines are derived from the Federal Identity Program requirements for signage but are tailored to reflect the Parks Canada Identity Program and Parks Canada's unique operational requirements. Every effort has been made to integrate all relevant standards and requirements from the Federal Identity Program into the Parks Canada standards and guidelines.

Figure 2.1 – The "Canada" wordmark

Figure 2.2 – Parks Canada signature used on PID signs

Figure 2.3 – Parks Canada signature used on regulatory signs

Figure 2.4 – Parks Canada's approved colours for signage

Figure 2.5 – Parks Canada identifier

2.2 Official Languages

As an agency of the Government of Canada, Parks Canada must meet all legislative and policy requirements as they relate to the official languages of Canada. All signs within the Parks Canada signage system are required to be bilingual and reflect the equality of status—linguistically and visually—of both official languages. The design layouts and specifications for Parks Canada signage in Section 4 have been developed in consideration of official languages requirements.

Official languages requirements as they relate to signage include:

- presenting the two official languages with equal prominence, i.e. in exactly the same colours and with identical style, size and weight of type (visual equality) *(Figure 2.6);*
- ensuring that the order of the languages is in accordance with the location in which the sign is situated (i.e. English followed by French in all provinces and territories except Quebec; French followed by English within the province of Quebec);
- ensuring that signs present both official languages in a side-by-side format (unilingual signs are not permitted unless the term being used is equivalent in both official languages, e.g. administration);
- ensuring that both official languages are presented using an over-under format only in cases where horizontal space is limited or when operational requirements warrant an over-under format. Situations might include insufficient horizontal space along a trail or roadway, signs required in confined areas, or to avoid unnecessarily wide signs. *See Figure 2.7 and Section 3.7.8—Official Languages: Over-under Format;*
- ensuring equal linguistic quality in the usage of the two official languages;
- ensuring that any use of a third language in the message area of the sign does not affect the status of both official languages and that it appears third under all circumstances (whether side-by-side or over-under) *(Figure 2.8).*

Figure 2.6 – Two official languages with equal visual prominence

Figure 2.7 – Exceptional circumstances for use of over-under format

Figure 2.8 – Placement of third language text on signage

2.3 Manual of Uniform Traffic Control Devices for Canada

MUTCD Signs and Symbols

To ensure maximum comprehensibility and safety across the Parks Canada system, regulatory (excluding parking control), warning and pedestrian crossing control signs, as well as pavement markings and signals, required on roadways and cycle paths within park or site boundaries must conform either to the Manual of Uniform Traffic Control Devices for Canada or provincial/territorial traffic control standards as appropriate. As not all provinces and territories have adopted the MUTCD, it is essential that each park and site be aware of which traffic standards to apply. The following is a list of relevant traffic manuals as of May 2004:

- Manual of Uniform Traffic Control Devices for Canada, 4th Edition
- Bikeway Traffic Control Guidelines for Canada, December 1998
- Supplemental Guide for Guide and Information Signage in Canada, November 2003

Province of Newfoundland and Labrador:

- The Department of Works, Services and Transportation. Specifications Book, March 2003 http://www.gov.nf.ca/wst/hwdesign/specbook/

Province of Nova Scotia

- Standard Specification Manual www.gov.ns.ca/tran/Publications/standard.pdf
- Motor Vehicle Act, Traffic Signs Regulations
- Schedule of Official Highway Signs www.gov.ns.ca/just/regulations

Province of New Brunswick:

- New Brunswick Department of Transportation. Standard Specifications, 2003. http://www.gnb.ca/0113/publications-e.asp

Province of Quebec:

- Transports Québec. Tome V – Signalisation routière Volumes 1 et 2, 2006.
- Transports Québec. Tome III – Ouvrages d'art, 2006.
- Transports Québec. Tome VII – Matériaux, 2006.
- Transports Québec, Tome V – Voies cyclables, 2007 (this book is in the revision process) http://www.publicationsduquebec.gouv.qc.ca

Province of Ontario:

- Ministry of Transportation, Ontario. Ontario Traffic Manuals (2001)

Province of Manitoba:

- Manitoba Infrastructure and Transportation. Traffic Signs Pamphlet, 2006. http://www.gov.mb.ca/tgs/trafeng/pdfs/signage.pdf

Province of Saskatchewan

- Saskatchewan Traffic Control Devices Manual, 1999
- Roadside Management Manual Part 2, 1999
- Saskatchewan Traffic Control Devices Manual for Work Zones, 2001

Province of Alberta:

- Alberta Transportation. Standard Specifications for Highway Construction, Edition 12, 2005. http://www.infratrans.gov.ab.ca/INFTRA_Content/docType245/Production/mns200.htm
- Alberta Sign Catalogue http://www.infratrans.gov.ab.ca/INFTRA_Content/docType252/Production/mns100.htm
- Highway Guide and Information Sign Manual, 2006 (available soon)

Province of British Columbia:

- Ministry of Transportation. Manual of Standard Traffic Signs & Pavement Markings, 2000. http://www.th.gov.bc.ca/publications/eng_publications/electrical/MOST_PM.pdf
- Ministry of Transportation. Traffic Sign Catalogue, 2001. http://www.th.gov.bc.ca/publications/eng_publications/electrical/Sign_Cat_2003.pdf

To provide a quick reference tool, Appendix C contains a list of signs that must conform to relevant provincial traffic control standards. All parks and sites are required to ensure signs posted on roadways within their jurisdiction comply with appropriate provincial standards and are encouraged to obtain copies of all relevant traffic control standards. Provincial departments of transport can identify which standards apply.

Figure 2.9 – MUTCD signs

2.4 Parks Canada Classification

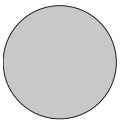
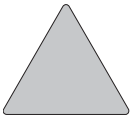
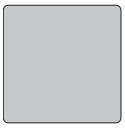
Parks Canada's new symbol set for signage is based on the National Standard of Canada CAN/CSA-Z321-96, *Signs and Symbols for the Workplace* (adopted by the Federal Identity Program). In view of Parks Canada's unique operational requirements, many additional symbols have been designed using the basic design principles as outlined in CAN/CSA-Z321-96.

The classification of symbols uses three specific shapes and six colours which permit users to distinguish between the six types of symbols and their respective function. In order to ensure proper due diligence in the adoption of these symbols, comprehensive evaluation and focus group testing has been conducted. Only those symbols meeting established evaluation criteria have been approved and released. All new symbol requests will be tested according to this evaluation criteria and released following successful testing results.

Appendix A includes a complete inventory of Parks Canada's approved symbol set. As further testing is conducted, Appendix A will be updated as required to address any additions. It is important to note that only the symbols found in Appendix A may be used on Parks Canada signage and that any new symbols required must be submitted for approval. In addition to signage, the Parks Canada symbol set must be used in all other media where use of symbols are required (e.g., web site, brochures etc.) to ensure proper due diligence and maximize comprehensibility.

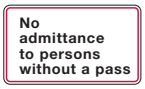
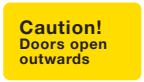
Appendix A provides a detailed breakdown of the evaluation approach and methodology. To submit a symbol request, or if you have any questions or comments, please contact the Parks Canada Identity Program Officer at pimpc-pcip@pc.gc.ca.

Table 2.1 – Federal Identity Program Classification of Graphic Symbols

Class and geometric shape	Type and colour	Function of symbols	
1 Regulatory signs 	**1.1 Prohibition** Red and black on white	To indicate an order forbidding an action.	
	1.2 Mandatory White on black	To indicate an order for obligatory action.	
2 Warning signs 	**2.1 Caution** Black on yellow	To indicate a potential hazard.	
	2.2 Danger White on red	To indicate a definite hazard.	
3 Information signs 	**3.1 Emergency** White on green	To provide information required in case of emergency.	
	3.2 Guidance and information White on blue	To provide general information.	

2.4 Parks Canada Classification (CONTINUED)

Table 2.2 – National Standard of Canada CAN/CSA-Z321-96, *Signs and Symbols for the Workplace* – classification and categories of signs

	Symbol signs	Symbol signs With text	Text signs
Regulatory signs			
1.1 Prohibition			
1.2 Mandatory			
Warning signs			
2.1 Caution			
2.2 Danger			
Information signs			
3.1 Emergency			
3.2 General Information			

2.4 Parks Canada Classification (CONTINUED)

Table 2.3 – Parks Canada classification and categories of signs, adapted from CAN/CSA-Z321-96

	Symbol signs Wall mounted	**Symbol signs** Post mounted	**Symbol signs** With text	**Text signs** Should be used only in cases where no appropriate symbol exists.
Regulatory signs				
1.0 Prohibition				
2.0 Mandatory				
Warning signs				
3.0 Caution		Note: caution signs must incorporate a text message		
4.0 Danger		Note: danger signs must incorporate a text message		
Information signs				
5.0 Emergency				
6.0 Guidance & Information				

2.5 Aids to Navigation and the Canada Shipping Act – Historic Canals

In addition to the Parks Canada regulatory, warning and information symbols and the MUTCD symbols, signs on the navigable waterways administered by Parks Canada must adhere to the Canadian Aids to Navigation System.

Aids to navigation on navigable waterways help the mariner determine position and course. They also warn of dangers or obstructions and indicate the best or preferred route.

Daybeacons

Port daybeacons are used at lock stations, bridge piers and as a channel marker on shorelines to indicate the port (left) side of the channel when proceeding upstream. Starboard daybeacons are used at lock stations, bridge piers and as a channel marker on shorelines to indicate the starboard (right) side of the channel when proceeding upstream.

Bifurcation or junction daybeacons mark a point where a channel divides and may be passed on either side. When proceeding upstream, the preferred route is to the left of a starboard bifurcation daybeacon. Similarly, the preferred route is to the right of a port bifurcation daybeacon. These daybeacons provide directional assistance when navigating across long reaches (e.g. Rideau Lakes), or at junctions where the main channel may not be clearly defined.

Figure 2.10 – Port hand daybeacon

Figure 2.11 – Starboard hand daybeacon

Figure 2.12 – Port bifurcation / Junction daybeacon

Figure 2.13 – Starboard bifurcation /Junction daybeacon

Boating Restriction Regulations

Other types of signs in use on navigable waterways adhere to the Boating Restriction Regulations, under the Canada Shipping Act. These regulations govern the safe operation of boats on most waterways in Canada. Sign examples shown include a speed limit sign, all vessels prohibited and waterskiing prohibited signs. (see fig 2.14 to 2.16)

Figure 2.14 – Speed limit

Figure 2.15 – No boats

Figure 2.16 – No water-skiing

Other Signage

Various areas along the Rideau Canal have been signed as "watch your wake" zones in an effort to protect private property and the natural environment. These signs can also be found along the Chambly, Saint-Ours, Lachine, Sainte-Anne-de-Bellevue and Carillon canals. Signs that indicate no anchoring areas are also used on the canals.

Figure 2.17 – Watch your wake

Figure 2.18 – No anchorage

Sign Specification Information

Sign specifications for Aids to Navigation signs or Canada Shipping Act signage may be obtained by contacting the Canadian Coast Guard, the Rideau Canal or the Trent– Severn Waterway.

2.6 Cultural Resource Management Considerations Relating to the Planning, Design, Location and Installation of Signs

In recognition of Parks Canada's conservation mandate, additional considerations relating to the installation of signs are required in order to minimize the impact on cultural resources and their historic values. Listed below are some general guidelines for the sensitive installation of signage to respect the historic values within a protected heritage area administered by Parks Canada:

1. Care should be taken to understand where historic value is located in a protected heritage area, i.e. in the architectural or aesthetic design of a building or landscape, in the archaeological remains, and/or in the significant natural resources. Neither the design, the materials, nor the location of signs for historic buildings, archaeological sites, cultural landscapes or heritage gardens should impair the historic value of these areas.

2. A cultural resource management specialist must be consulted when cultural resources may be affected by the installation of signs in order to minimize negative impacts on the historic value. In addition, an archaeologist must be consulted for proposed sign installations whenever installations will result in any disruptions below the existing grade.

3. Maintenance staff and representatives from groups or organizations who may be interested or implicated in the design, placement, materials or text of the sign should be involved early in the planning process and should be made aware of the historic values which may be affected by the sign.

4. Signs should be discrete and simple so as not to detract from the historic value of the heritage area. In most cases a minimalist approach to the design and mounting of signs is the most appropriate choice.

5. Locating signs toward the edges of exterior spaces is generally respectful of existing landscape character; however, this may not always be suitable depending on information/wayfinding requirements.

6. The installation of signs directly on the exterior of historic structures such as buildings or walls is strongly discouraged, as these structures can easily be impaired through the use of drilled holes and fasteners which could cause or accelerate damage due to weathering. When there appears to be no alternative to locating a sign on a historic structure, a cultural resource management specialist must be consulted to determine the location and method of mounting the sign that least interferes with the historic fabric or other historic values, while maximizing public viewing of the sign.

3

Sign Planning

Sign Planning

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3.1 Overview

Navigation within natural environments, cultural landscapes or buildings is a complex task that requires a certain degree of planning and consideration as to how the visitor navigates. Site sign planning, or wayfinding, is a means of organizing and defining a sequence of posted messages that help make buildings or spaces self-navigable. The primary objective of signage is to allow visitors to find a park or site and then to orient themselves within it as efficiently as possible.

Signs within a park or site must be considered as an integrated system. All signs interact with each other, whether they are directional, regulatory or informational. Their appearance, their messages and their placement all affect the communication value of the overall signage system and, as a secondary objective, make the visitor experience a more pleasant and memorable one while projecting a strong corporate image of Parks Canada as an organization.

In short, the goal of an effective Parks Canada signage system is to convey information in both official languages with the least number of signs while still answering the most essential questions. Visitors must arrive safely at their destination. Equally important, however, is that we must balance the need for signage with protecting the visual, environmental and cultural integrity of our national parks, national historic sites and national marine conservation areas.

Achieving an effective signage system within our national parks, national historic sites and national marine conservation areas is not a simple and straightforward task. The sign planning process requires considerable input and cooperation from various functional groups—from asset maintenance to visitor services and cultural resource staff. Each functional area within a park, site or marine conservation area plays an important role in helping the sign planning team develop the most efficient and effective sign plan possible. It is therefore essential that anyone involved in the sign planning process be intimately familiar with all the Parks Canada standards for signage, not simply those within this section.

3.2 Objectives

The objectives of this sign planning section are:

- To provide sign planning teams and Parks Canada staff with information, principles and tools to assist in the development of a comprehensive sign plan that is effective, efficient and integrated while adhering to the Parks Canada standards for signage.
- To ensure that signage systems within national parks, national historic sites and national marine conservation areas address visitor requirements, as well as the human and environmental factors that influence how a specific park, site or marine conservation area is navigated.
- To ensure that visitors can easily find their way to and within our national parks, national historic sites and national marine conservation areas, thereby helping to achieve a positive and memorable visitor experience.
- To ensure that the visual, environmental and cultural integrity of our national parks, national historic sites and national marine conservation areas is not compromised by eliminating excess signage, unnecessary and/or outdated messaging and complex navigation.
- To provide a standard signage system for all of Parks Canada that is consistent, effective and adds to the overall external image of Parks Canada.

3.3 Sign Classification

It is important for the Parks Canada sign planning team to become familiar with the Parks Canada sign classification system prior to beginning the sign planning process. The following section describes the various categories of signs and provides general information concerning their function and situations when they should be used. The sign planning team should be aware of what signs are potentially available as catalogue items (under development) to facilitate fabrication and avoid unnecessary duplication of sign design.

The sign categories are organized according to the visitor experience with each sign type having a unique sign code. There are six main sign categories within the Parks Canada signage system: Off-Site, Primary Identification, Special Purpose, Information, Regulatory and Warning.

Each sign category consists of multiple sign types depending on the function and elements of the sign. Detailed design and layout specifications by sign type are provided in Appendix E. To ensure the Parks Canada sign classification system reflects current operational requirements, information regarding any sign type not identified should be forwarded to the Parks Canada Identity Program Officer at pimpc-pcip@pc.gc.ca for further consideration.

The following tables provide an overview of the Parks Canada sign classification system by category and sign type and provides their unique sign code. All signs, site plans and sign schedules must reference these assigned sign codes.

Appendix D provides a detailed breakdown of all sign types with associated layout options. It is the responsibility of the sign planning team to identify the appropriate layout option for each sign.

Table 3.1 – Sign categories

Sign Category	Overall Function
Off-Site	This category encompasses signs that help visitors navigate to our national parks, national historic sites and national marine conservation areas. These signs are typically not located on Parks Canada property and are integral to wayfinding. Permission to install these signs must be secured by individual field units through negotiation with respective provincial/territorial/municipal authorities.
Primary Identification	The official identification signs for the Government of Canada, PID signs confirm visitors have arrived at a national park, national historic site, national marine conservation area or a facility located within these sites.
Special Purpose	Approved special purpose signs that have been developed to meet specific operational needs and deviate slightly from the standard approach to common use and operational signs. These approved signs consist of entry fee signs, welcome signs, partnership recognition signs, etc. and can be used only in specific situations. These standards and guidelines will be updated periodically to reflect any changes or additions that arise to the approved special purpose signs.
Information	Information signs provide essential navigational information to the visitor. This category of signs comprises Emergency signs, used to convey an emergency-related message, and Guidance and Information signs, used to identify locations, provide direction or convey non-regulatory information.
Regulatory	This category of signs comprises Prohibition signs, used to convey an order forbidding an action, Mandatory signs, used to convey an obligatory action, as well as parking control and boundary markers.
Warning	Warning signs communicate essential safety information to the visitor. This category of signs comprises Caution signs, used to identify a potential hazard and Danger signs, used to identify a definite hazard.

3.3 Sign Classification (CONTINUED)

Table 3.2 – Sign classifications

Off-Site

Tourism-Oriented Directional Sign patch	Highway advance	Trailblazer
A1	B1	B2 B3 B4 B5 Under review

Primary Identification

Primary identification sign
C1 C2

3.3 Sign Classification (CONTINUED)

Table 3.2 (continued) – Sign classifications

Special Purpose

Welcome sign

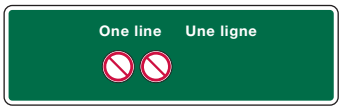
D1

D2

Day-use area sign

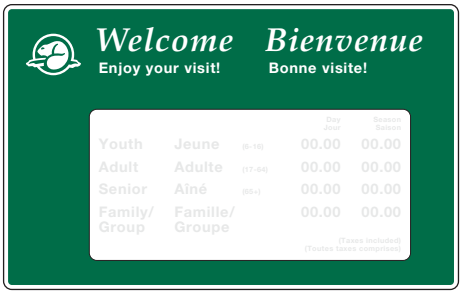
D3

D4



Fee sign

E1

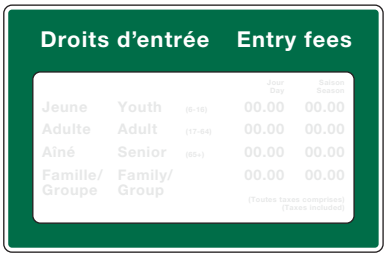


E2

E3

E4

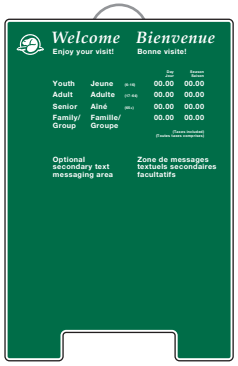
E5



E6

E7

E8



Street identification

F1

F2

Partnership recognition

G1

Fees at work

G2

3.3 Sign Classification (CONTINUED)

Table 3.2 (continued) – Sign classifications

Information

Emergency

H1

Campsite identification

J1

J2

Identification

K1

K2

K3

K4

K5

K6

Directional

L1

L2

L3

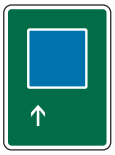
L4

L5

L6

L7

L8



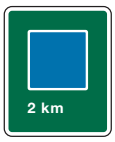
Distance

M1

M2

M3

M4



Distance indicator

M5

3.3 Sign Classification (CONTINUED)

Table 3.2 (continued) – Sign classifications

Regulatory

Prohibition

N1

Mandatory

P1

Prohibition and mandatory

Q1

Q2

Q3

Q4

Parking control

R1

R2

R3

R4

R5

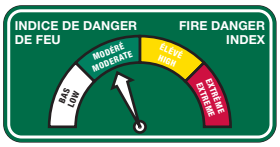
R6

Boundary marker

S1

3.3 Sign Classification (CONTINUED)

Table 3.2 (continued) – Sign classifications

Warning		
Caution	**Danger**	**Caution and danger**
T1 	U1 	V1
Wave hazard	**Fire index**	
W1 under development	W2  under development	

3.4.1 Warning Signs and Symbols – Duty of Care Requirements

Parks Canada must use reasonable care to ensure that visitors to Canada's national parks, national historic sites and national marine conservation areas are provided with a reasonably safe environment. The use of warning signs (caution and danger) is but one tool to communicate a potential hazard or danger to visitors. Simply erecting a caution or danger sign, however, does not ensure that Parks Canada has fulfilled its duty of care requirements with regard to providing a reasonably safe environment. Parks and sites should, depending on relevant circumstances, consider complementing their use of signs with other forms of communication to ensure visitors are made aware of potential hazards and dangers.

In an effort to ensure the safety of visitors, Parks Canada has taken several steps with respect to signage that will contribute to fulfilling Parks Canada's duty of care requirements for visitor safety. Parks Canada has:

1. Adopted National Standard of Canada CAN/CSA-Z321-96 *Signs and Symbols for the Workplace* colour coding for all warning signs (caution and danger).
2. In addition to the symbols prescribed by FIP, based the classification and design of its new symbol set on National Standard of Canada CAN/CSA-Z321-96 *Signs and Symbols for the Workplace* as well as the International Organization for Standardization ISO 7001, *Public Information Symbols*.
3. Conducted significant comprehensibility testing of the new Parks Canada symbol set based on the evaluation methodology prescribed by the International Standards Association. Parks Canada has established a minimum comprehensibility score of 80%, including text message, for all warning symbols.
4. Instituted the requirement that all warning symbols be accompanied by a supporting text message in all circumstances.

While these steps help fulfill Parks Canada's duty of care requirements and reduce the risk of liability to the Agency, it is the responsibility of each park and site to determine if, when, and where warning signs are required. More importantly, it is the responsibility of the park, site or marine conservation area to determine if a specific sign effectively communicates the potential/definite hazard or risk. In determining the need for a warning sign, parks, sites or marine conservation areas must address the following questions:

1. What is the best way to communicate the potential/definite hazard or danger?
2. What information must be conveyed to ensure visitors understand the hazard or danger?
3. Is the message being effectively communicated?
4. What do the words being used mean to the visitor and are they sufficient?
5. Is a sign enough to convey the hazard or danger effectively and will the visitor act upon this warning in an appropriate manner?
6. Is it in the right location?

In light of these questions, parks and sites should consider the effectiveness of a proposed warning sign and consider other/additional communication tools to help fulfill Parks Canada's duty of care towards visitors.

Warning signs – changeable conditions

In certain situations, a warning sign will require a changeable component to identify various levels of risk (for example, a wave or fire index warning sign). In addition to the above requirements for duty of care, Parks Canada must also ensure that the individuals charged with monitoring the risk and making decisions related to the level of risk are properly trained to assess the potential/definite hazard or danger. Furthermore, Parks Canada must ensure that the credentials of these individuals are well documented and that the criteria for assessing the risk are clearly established.

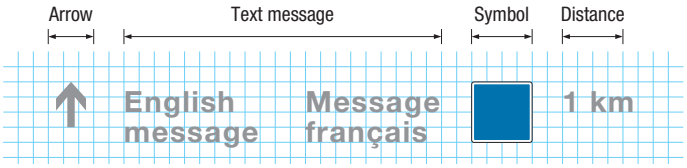
3.5 Message Elements

Parks Canada has established a consistent design and communication approach for all of its common-use and operational signs. Knowing how each message element "fits" on the sign will help facilitate the sign planning process.

The new Parks Canada signage system is aimed at maintaining a high aesthetic standard by reducing visual clutter and contributing to the consistent and orderly presentation of information. Ensuring that the net visual effect of a group of signs on the aesthetics of the environment does not detract from the visitor experience remains an important underlying objective of Parks Canada's signage program.

In addition, it is important to recognize that various factors must be considered when planning for and designing a sign, including determining the intended use of the sign (i.e. pedestrian, vehicular, cycling etc.) as well as the distance or traffic speed under which the sign will be used. Understanding how each variable impacts the sign will help the sign planning team develop an effective and comprehensive sign plan that will ensure a seamless transition from planning through to design and installation.

Figure 3.1 – Sign message elements



3.5.1 The Directional Arrow

The arrow is one of the most common elements in the Parks Canada signage system. The Parks Canada arrow is derived from FIP and conforms with ISO standard 7001. Human factors research has confirmed that this arrow is appropriate for all signage applications, including highway vehicular signs.

The arrow can be used to indicate eight different directions *(see Figure 3.2)*. In addition, four arrows have been developed to indicate an advance turn *(see Figure 3.3)*.

Arrow sequence – vehicular

When applied to vehicular signs, the arrow is used to convey three general directions—straight ahead, left or right. Human factors research and industry standards prescribe a set sequence in which multiple destinations should appear on a sign. They are *(see Figure 3.4)*:

1. Straight ahead. Through destinations apply to the majority of drivers. The down arrow must only be used to denote through destinations on overhead signs.
2. Left turn. Left turns require more driver preparation and must appear before a right arrow.
3. Right turn.

Arrow sequence – pedestrian

There is no prescribed sequence for pedestrian information and it is up to the sign planning team to determine the most appropriate sequence of destinations containing an arrow. As a rule, however, on pedestrian signs the most heavily used or important destination appears first. Alternately, the order of destinations can range from closest to furthest away.

Figure 3.2 – Eight different directions

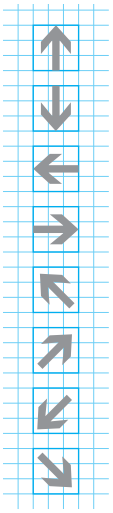


Figure 3.3 – Advance turn arrows

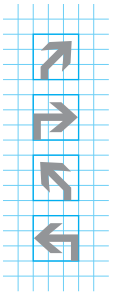


Figure 3.4 – Arrow sequence – vehicular
On vehicular signs, the straight ahead arrow appears first, followed by the left and right arrows.

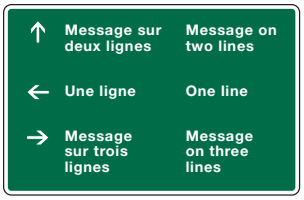
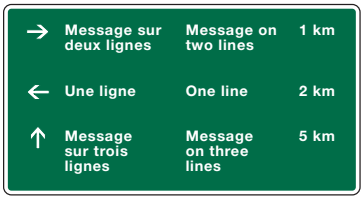


Figure 3.5 – Arrow sequence – pedestrian
On pedestrian signs the most frequently used destination typically appears first, or when a distance is indicated, destinations can be ordered from closest to farthest.



3.5.2 Text Messages

Most layouts are developed on the basis of two key factors: length of text and the size of the characters or symbols required. Determining the appropriate character and symbol size depends on a variety of factors, including whether or not the sign is intended for vehicular or pedestrian traffic, and what the viewing distance and/or traffic speed is. Based on human factors research and industry standards, Parks Canada has adopted a mandatory character and symbol size structure that will ensure maximum comprehension and legibility for all Parks Canada signs *(see Section 3.5.7 for more detail)*.

Decisions on character size may need to take into account a variety of human, environmental or heritage factors and could result in a smaller or larger size for a specific sign. Often, the length of the text message is the determining factor in the overall size of the sign. If using the required character size results in a sign that is too large for the intended space, the sign planning team should consider rephrasing the message, removing symbols or using two signs if appropriate.

3.5.3 Distance Indicator

The distance indicator is an important component of the Parks Canada signage system. The positioning of the distance indicator varies between sign types. However, for vehicular signs it must appear to the right of the destination in order to be consistent with the Manual of Uniform Traffic Control Devices for Canada (MUTCD).

3.5.4 Graphic Symbols

Parks Canada has based its symbol set on the National Standard of Canada CAN/CSA-Z321-96, *Signs and Symbols for the Workplace* as well as the International Organization for Standardization ISO 7001, *Public Information Symbols.* A general description of the symbol set is provided in Table 3.3.

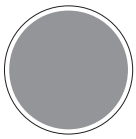
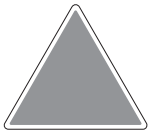
The Parks Canada signage system is based on the consistent use of three geometric shapes and specific colours. These shapes and colours function as a code, enabling users to readily distinguish between six types of symbols *(see Table 3.3)*.

Design standards prescribe the shape, colour and image content of the symbols. In addition, other aspects, such as the size relationship of the shapes, the proportion of the border, and the production sizes, have also been standardized.

The Parks Canada symbol set is designed to convey regulatory, warning and information messages to visitors at a glance. To ensure consistency across the Parks Canada system, over 300 symbols have been developed to communicate the vast range of activities and requirements within national parks, national historic sites, and national marine conservation areas.

The Parks Canada symbol set *(see Appendix A)* must be used on Parks Canada signage and will be incorporated into all other media where symbols are required (print, web, interpretive media). When preparing off-site signage, you must confirm with the respective provincial/municipal authorities which symbols are required, e.g. MUTCD.

Table 3.3 – Parks Canada symbol set

Class and geometric shape	Type and colour	Function of symbols	Subject classification	
Regulatory signs 	1 **Prohibition** Red and black on white	To indicate an order forbidding an action.	1-1 Access and activities	
	2 **Mandatory** White on black	To indicate an order for obligatory action.	2-1 Activities, protective apparel and equipment	
Warning signs 	3 **Caution** Black on yellow	To indicate a potential hazard.	3-1 Potential hazards	
	4 **Danger** White on red	To indicate a definite hazard.	4-1 Definite hazards	
Information signs	5 **Emergency** White on green	To provide information required in case of emergency.	5-1 Facilities and equipment	
	6 **Guidance and information** White on blue	To provide general information.	6-1 Public services 6-2 Concessions 6-3 Public transportation 6-4 Recreation	

Evaluation

Each symbol in the Parks Canada set has been tested for comprehension following an evaluation guideline developed by the International Standards Association, ISO 9186 *Graphical symbols—Test methods for judged comprehensibility and for comprehension*. Regulatory and warning symbols scoring a comprehension rate of 80%, including text message, or higher (80% of respondents understood the intended meaning) have been adopted for use.

In order to ensure comprehension and reduce the risk of liability, all warning symbols must be used with a supporting text message *(see Section 3.4.1 for additional information on warning signs).*

Information symbols scoring a comprehension rate of 60% or higher have been approved for use. In order to ensure comprehension and promote positive visitor experiences, it is recommended that information symbols scoring less than 80% be accompanied by a supportive text message.

Only Parks Canada approved symbols are permitted on Parks Canada signs. Any requirement for a symbol that is not part of the symbol set should be forwarded to the Parks Canada Identity Program Officer at pimpc-pcip@pc.gc.ca. If identified as a requirement, proper comprehensibility testing will be conducted prior to approval for use.

3.5.5 Sequence of Message Elements

Human factors research has demonstrated that the consistent application of message elements in a prescribed sequence, combined with limits on message load, provide for a high sign comprehension rate.

A prescribed sequence for message elements has been established based on human factors research and input. This sequence is modified slightly for vertical signs when the symbol is the primary message element.

Given that the arrow provides critical information for navigation, it always appears first (to the left on horizontal signs) or last (at the bottom of vertical signs).

For horizontal signs configured with a single symbol, the sequence is:

1. Directional Arrow
2. Bilingual Message
3. Symbol
4. Distance Indicator

(see Figure 3.6)

Figure 3.6 – Horizontal signs configured with a single symbol

Figure 3.7 – Horizontal vehicular signs with more than one symbol

For horizontal *vehicular* signs with more than one symbol, the sequence is:

1. Directional Arrow
2. Bilingual Message
3. Distance Indicator
4. Symbols (appear on second line)

(see Figure 3.7)

For horizontal *pedestrian* signs with more than one symbol, the sequence is:

1. Directional Arrow
2. Bilingual Message
3. Symbol (appears on second line)
4. Distance Indicator (appears on third line)

(see Figure 3.8)

For vertical signs formatted with the symbol as primary element, the sequence is:

1. Symbol
2. Bilingual Message
3. Distance Indicator
4. Directional Arrow

(see Figure 3.9)

Figure 3.8 – Horizontal pedestrian signs with more than one symbol

Figure 3.9 – Vertical signs formatted with the symbol as primary element

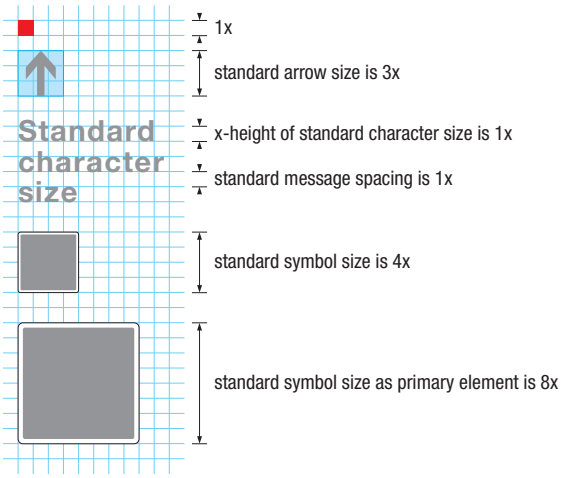
3.5.6 Message Elements – Standard Sizes

The unit "x"
All measurements pertaining to sign layout are based on the unit "x", which is equal to a single square cell in a layout grid *(see Figure 3.10)*. The size of "x" is variable to accommodate a wide range of viewing distance or traffic speed requirements.

Arrow Size
Human factors research has confirmed that a 3x arrow (3 times the value identified for x) is comprehensible from the same distance as 1x typography. An arrow size of 3x has been adopted as the standard size for all signs.

The directional arrow plays a key role in wayfinding and for this reason has been assigned the dominant position on a sign: far left for horizontal signs and bottom for vertical signs.

Figure 3.10 – Standard element sizes



Character Size
In order to ensure consistent legibility and comprehensibility of sign messages, especially for vehicular signs, all signs must use the mandatory character height sizes for signage. The unit "x" is equal to the height of the lower case "x" of the message. This typographical measurement is commonly referred to as the "x-height." For example, if the letter "x" in the word "example" had a height measurement of 10 mm, the "x-height" would be 10 mm.

Symbol Size
Human factors research has confirmed that a symbol-to-text ratio of 4:1 or 4x is appropriate for both vehicular and pedestrian signage. In situations, both vehicular and pedestrian, where the symbol is used as the primary message, the standard symbol size becomes 8x to provide an added level of legibility and detectablity to the symbol. This is particularly valuable when using a symbol sign as a trailblazing approach to a particular facility or activity.

3.5.7 Determining the Size of "x"

It is the responsibility of the planning team to identify the required dimension of "x" for each sign. Table 3.4 identifies the mandatory character size, symbol size and arrow size expressed in millimeters for a range of traffic speeds and viewing distances. Accurately identifying the proper viewing distance or traffic speed will ensure that signs provide the required level of detectability, legibility and comprehensibility, all of which are critical to the effectiveness of vehicular signs.

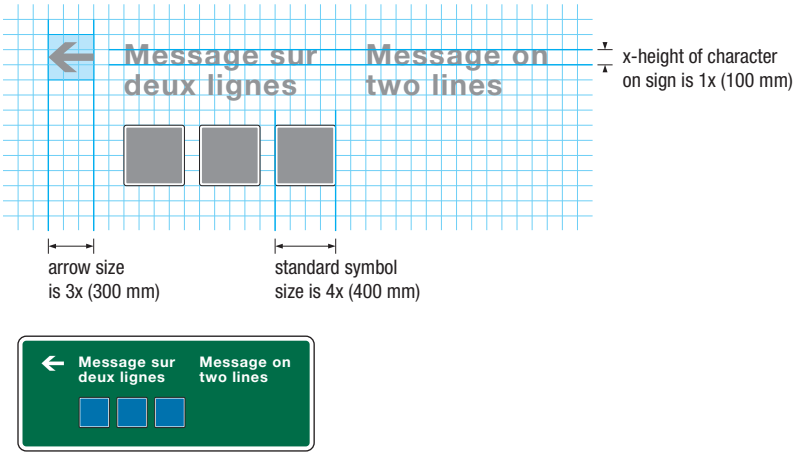
It should be noted that when a sign is used to convey a message to both vehicular and pedestrian traffic, the sign must be configured for vehicular traffic.

When determining the size of "x" there may be a need to compensate for a variety of human, environmental or heritage factors, which could result in a smaller or larger size for a specific sign. In addition to the size of "x" , message length is a determining factor in the overall size of the sign. If using the appropriate character size results in a sign that is too large for the intended space, consider rephrasing the message, removing symbols, using two signs, or in the case of pedestrian signs, selecting a different "x" size providing it does not compromise the intended function or impose a safety hazard when reading the sign.

Table 3.4 – Vehicular speed / pedestrian viewing distance and standard size of message elements

Vehicular speed	**—**	**—**	**—**	**0-15 km/h**	**20-30 km/h**	**35-40 km/h**	**45-60 km/h**	**65-75 km/h**	**80-90 km/h**	**95+ km/h**
Pedestrian viewing distance	**6 m**	**9 m**	**12 m**	**15 m**	**30 m**	**50 m**	**60 m**	**75 m**	**90 m**	**120 m**
The unit "x"	**10 mm**	**15 mm**	**20 mm**	**25 mm**	**50 mm**	**75 mm**	**100 mm**	**125 mm**	**150 mm**	**200+ mm**
Arrow size (3x)	30 mm	45 mm	60 mm	75 mm	150 mm	225 mm	300 mm	375 mm	450 mm	600 mm
Character size (1x)	10 mm	15 mm	20 mm	25 mm	50 mm	75 mm	100 mm	125 mm	150 mm	200 mm
Symbol size (4x)	40 mm	60 mm	80 mm	100 mm	200 mm	300 mm	400 mm	500 mm	600 mm	800 mm
Symbol size (8x)	80 mm	120 mm	160 mm	200 mm	400 mm	600 mm	800 mm	1000 mm	1200 mm	1600 mm
Distance size (1x)	10 mm	15 mm	20 mm	25 mm	50 mm	75 mm	100 mm	125 mm	150 mm	200 mm

Figure 3.11 – Sample sign with vehicular speed of 60 km/h
x = 100 mm



Posted Speed Limit Versus Operating Speed

Traffic speed can be expressed as either a posted speed limit—which can be an arbitrary value assigned to a roadway—or the actual operating speed of a vehicle. Most roadways are designed to standards that typically exceed the intended posted speed limit. This allows for drivers to exceed the speed limit with a margin of safety.

On Canada's streets and roadways, posted speed limits are established through legislation and often reflect public safety and community issues. Similar criteria are used by Parks Canada in determining posted speed limits for roads and highways in the heritage places that we manage. Parks Canada's added concern for the safety of wildlife and the aesthetic integrity of our natural and cultural resources further influences decisions regarding the establishment of speed limits. As a result, posted speed limits do not always reflect typical vehicle operating speeds or the design of the roadway.

Operating speeds are specific to a road or a section of road and reflect actual user tendencies. The posted speed limit is often exceeded, especially by drivers familiar with the road. Arbitrarily low speed limits usually result in poor compliance.

When deciding the size of a sign's message elements, it is critical to consider the impact a posted speed limit has on the operating speed for that location. Signs with undersized messages can cause a discrepancy in traffic speed as some drivers unfamiliar with a roadway slow down to read signs while more familiar drivers travel at higher speeds.

The character sizes in Table 3.4 are the minimum mandatory x-heights for posted speed limits. In some situations, it may be advisable to increase the size of the text, particularly in areas where the posted speed limit is often exceeded.

3.5.8 Message Load and Comprehension

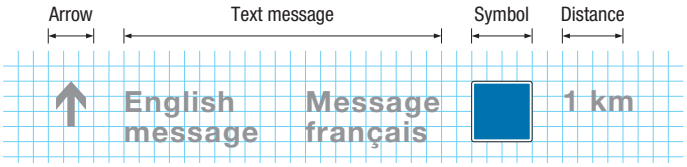
The comprehensibility of a sign is dependant upon several factors, including whether or not it is intended for vehicular or pedestrian traffic, viewing distance or traffic speed, driver age, and the number of message elements or message load.

To ensure that all signs, especially vehicular signs, offer the maximum level of detectability, legibility and comprehensibility, Parks Canada has developed its message load requirements in consultation with human factors specialists. The following section identifies the mandatory message load requirements for both vehicular and pedestrian signs and provides some of the rationale for the requirements. It is important that the sign planning team be familiar with Parks Canada's mandatory message load requirements and sizes prior to beginning the sign plan.

For the purposes of establishing message load, the following are considered elements:

- an arrow;
- the bilingual text message or destination name, including prepositions and names with multiple words;
- a symbol;
- a distance indicator or other numeric value, e.g. campsite indicator.

Figure 3.12 – Sign elements



3.5.9 Message Load Requirements

The comprehensibility of a sign, especially a vehicular sign, is dependant upon several factors, including: size of the message elements, viewing conditions, driver age, vehicle speed and the number of message elements, or message load. In addition to ensuring maximum legibility and comprehensibility, the message load requirements adopted by Parks Canada will also help to limit the overall size of the signs and address the issue of aesthetics, which remains one of Parks Canada's key objectives.

Vehicular Signs

The mandatory message size (x-height) values presented in Table 3.4 were established to provide an average minimum of 4.0 seconds of viewing time for the range of speeds from 0 to 90+ km/h. Human factors research has confirmed that an average of ½ to 1 second of reading time is required per major word, number or symbol. At this rate it is reasonable to expect a driver to be able to comfortably comprehend a sign containing between 4 and 8 message elements.

A study undertaken with the Ministry of Transportation in Ontario confirmed that for signs with unilingual messages, drivers cannot be expected to retain the entire message if it exceeds 4 major words. Signs with 6 or more words yielded sign reading performance rates of 70% and less. Human factors research, commissioned by Parks Canada, has determined that 3 destinations per sign and 4 symbols per sign is the maximum message load that can be retained at an acceptable sign reading performance rate.

To ensure comprehensibility of vehicular signs, the following restrictions on message load have been established:

- 3 destinations per sign, maximum;
- 9 elements per sign, maximum;
- 4 symbols per sign, maximum.

If there is a requirement to post more information than the recommended message load, it is advisable to fabricate and install a second sign beyond the first, according to the appropriate separation distances identified in Section 3.9. This will allow drivers a comfortable amount of reading time for the second sign.

Pedestrian Signs

Due to the approachable nature of pedestrian signs there is no set limit to the number of message elements on these signs, other than exercising effective communication and creating concise messages. It is recommended that pedestrian signs not exceed 6 destinations with a maximum of 6 symbols per destination.

Along with comprehension, environmental and aesthetic concerns should be part of the criteria that are considered in determining the size, content and location of a pedestrian sign.

Figure 3.13 – Maximum of 3 destinations and/or 9 elements per vehicular sign
Nine elements in example include 3 arrows, 3 text messages and 3 distance indicators.

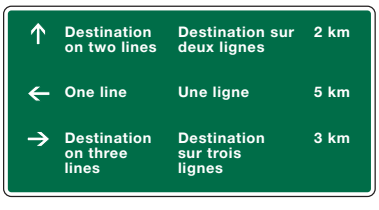


Figure 3.14 – Maximum of 4 symbols per vehicular sign

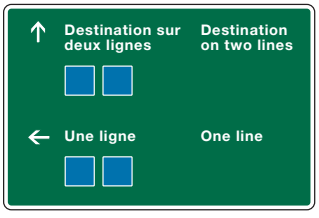
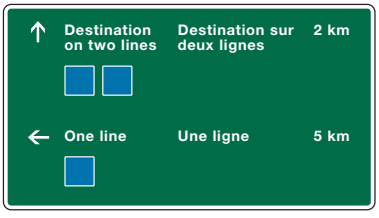


Figure 3.15 – Maximum of 9 elements per vehicular sign



3.6 Cycling and Cross-country Skiing Signs

While specific guidelines for cycling and cross-country skiing signs have not been developed, the standards for cycling and cross-country skiing signs can be borrowed from the vehicular or pedestrian elements of the Parks Canada standards for signage.

Cycling and skiing trails are often shared with other users. Cross-country skiing trails in winter become hiking trails in summer. Pedestrians often use cycling trails. With the exception of controlled access highways, bicycles are allowed on most roads designed for motor vehicles. The user must be considered when determining the size and content for these signs.

While there is no national standard speed limit for bikeways, 15 to 20 km/h is a commonly adopted posted maximum speed. For dedicated cycling signs it is recommended that the message load standards for vehicular signs be adopted with a suggested layout grid size of x = 25 mm.

For cross-country skiing applications the pedestrian message load signs will be best. It is recommended that the pedestrian viewing distance scale in table 3.4 be consulted to determine the size of the layout grid and the message elements. The location of these signs should also be considered as a factor influencing content and comprehension.

3.7 Sign Messaging

The messaging objectives of the federal government for signage as stipulated in FIP are:

- to facilitate access to federal programs and services;
- to promote consistency and clarity in the writing and presentation of both official languages;
- to promote the functional identification of government services; and
- to assist effective communication in applications where messages must be concise.

Messaging on Parks Canada signs must meet the needs of the user but it must also conform to various policy and legislative requirements such as the Official Languages Act. Messages must be clear, concise, functional and not bureaucratic or superfluous.

One of the principle considerations for messages on signs is that the message must be plain and simple—i.e., it must be in plain language, it must clearly convey its purpose and it must be immediately understood. In addition, the principle of official languages equality must be communicated via the messaging on signs. Balancing this requirement with the need to be concise presents a significant challenge to the sign planning team and underscores the importance of ensuring that only essential information is communicated on a sign.

3.7.1 Sign Communication

Sign communication is very distinct from most other media. It is important that the sign planning team put themselves in the place of the visitor when determining a message. What is clear to Parks Canada staff may not be clear to a first-time visitor to a national park, national historic site or national marine conservation area. The following principles should be applied when determining a message:

- Brevity
- Clarity despite brevity
- Familiarity of words (short familiar words help comprehension).
- Equivalence in meaning of texts in both official languages
- Context—much of the meaning of a sign is derived from the environment, and therefore self-explanatory
- Logic—in the sequence of the message elements
- Distinction between primary and secondary messages
- Consistency in messaging within a signage system
- Visual impact of a sign on the environment.

To facilitate the development of consistent messages, Appendix B provides a list of standard messages with appropriate translation.

3.7.2 Message Editing and Design

Editing and design both give meaning and form to a message. The transition from the first draft or layout to the final message is a process of arranging, organizing, and in many cases rephrasing. The choice of words, of spatial relationships and of expressive graphic elements results in meaningful and purposeful communication. An objective review should take place during the process of drafting and designing sign messages.

3.7.3 Choosing the Line Breaks

Messages that are to be presented on more than one line should be broken into readable phrases. Articles and prepositions at the end of a line should be avoided. In addition, an attempt should be made to provide a visual balance between the two language columns. Although a visual balance is generally achieved by using the same number of lines for each language, compromises may have to be made to avoid one language column of three or more lines, each consisting of one word only.

3.7.4 The Process: Verbal and Visual

Communicating a message effectively requires methodology. This includes:

- Skilful choice of words;
- Organization of the words in a logical sequence; and
- Presentation of the words in typographic, spatial terms.

The first and second aspects are verbal and the responsibility of the park/site. The last is visual and involves advice from a designer. All three are inter-related and the thought processes and goals are identical: to give meaning to a message, to strengthen its significance, to make the important stand out against the unimportant. A message presented in "tone-of-voice" typography reflects the relative importance of verbally expressed thoughts; it gives impact to visual communication.

3.7.5 Message Elements Common to Both Languages

Many messages include an element that is equivalent in both official languages, such as a geographical name or street name that may not be translated, a street number, an expression of time, a distance, or a directional arrow (for example, since the word "Administration" is equivalent in both official languages it would not necessarily need to be repeated on a sign).

Depending on content within the total sign message, a common element may be presented in several ways:

- Where a common element forms the primary message (or an important part of the total message), it should be emphasized in relation to the remaining text. In such cases, a repeat of the common element in the second column may be redundant. Also, if the equivalent message is the only message on a sign, it would not need to be repeated.
- Where an element is common in both official languages but does not require emphasis in the context of the message, the common element should appear in each language column of the sign if it would result in a void on the sign face.

Figure 3.16 – Choosing the line breaks

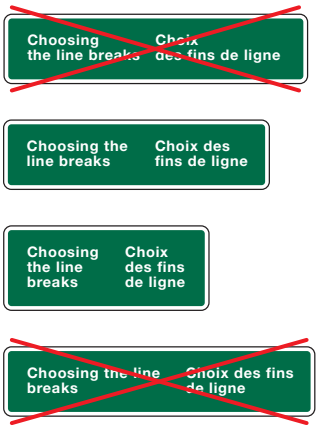
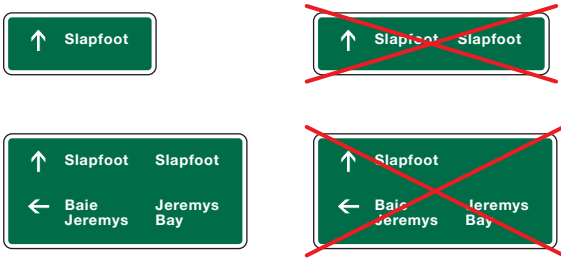


Figure 3.17 – Message elements common to both languages



3.7.6 Official Languages: Side-by-side Format

It is the policy of the Government of Canada to project itself as an institution in which English and French enjoy equal status in all respects. Therefore, the two official languages shall be presented with equal prominence, i.e. in exactly the same colours and with identical style, size and weight of type.

To project visual equality, it is preferred that the official languages appear in a side-by-side bilingual format. Operational signs may present situations where horizontal space is limited, or operational requirements do not lend themselves to side-by-side format (for example, backcountry trail signs that would have to be installed on two posts if side-by-side format requires a horizontal sign greater than 600 mm). In these cases, an over-under approach is permitted. Section 3.7.8 and Section 4.10 provide more detailed direction on the use of the over-under format. It is essential that conditions warranting an over-under format be documented in the message schedule of the sign plan.

3.7.7 Equivalence

The messages on Parks Canada signage must be equally complete in both official languages and correspond in all respects. To achieve equivalence in both content and number of lines, a revision of the original text may be required. This could include segmenting the text into primary and secondary messages, or rephrasing.

To facilitate the development of consistent messages, Appendix B provides a list of standard messages with appropriate translations.

3.7.8 Official Languages: Over-under Format

Ensure that both official languages are presented using an over-under format only in cases where horizontal space is limited or when operational requirements warrant an over-under format. Situations might include insufficient horizontal space along a trail or roadway, signs required in confined areas, or the need to avoid unnecessarily large signs.

When presenting both official languages in an over-under format, the language appearing on top is the language that would appear to the left in the side-by-side format.

Figure 3.18 – No line break
This sign has a horizontal dimension of 5800 mm.

Figure 3.19 – Line break
Horizontal dimension is reduced by 1800 mm, over 30% narrower than figure 3.18.

Figure 3.20 – Line break with stacked languages
Horizontal dimension is reduced further, over 50% narrower than the example in figure 3.18.

Figures 3.18, 3.19 and 3.20 are configured for vehicular traffic with a posted speed limit of 60 km/h (x=100 mm)

Figure 3.21 – 60 km/h roadway sign without line breaks

Figure 3.22 – 60 km/h roadway sign with line breaks and stacked languages

Figures 3.21 and 3.22 are scaled drawings based on posted speed limit of 60 km/h.

Laying out a sign without line breaks can result in an unnecessarily long sign. Attempting to install such a sign along a roadway or other confined space can result in the message being obscured.

Introducing a line break and stacking the official languages results in a sign over 50% narrower. Additional cost savings will be realized with this format by reducing the number of posts required for installation.

3.7.9 Creating Message Hierarchy

Creating a message hierarchy by contrasting the size of select message elements is an effective way to highlight or give emphasis to a particular element. To accomplish this, the primary message elements are increased in size relative to the secondary message elements.

In most situations, creating a message hierarchy improves the performance of a sign. This is particularly true when dealing with warning signs (caution and danger). Creating a message hierarchy by increasing the size of the words "Attention" or "Danger" helps grab the attention of visitors and provide them with essential safety information. Note the difference in legibility in the examples in Figure 3.23 below.

When creating a message hierarchy, the size of the primary message element is increased in increments of "x":

- 1.5x,
- 2x, or
- 3x

It is important to note that when creating a message hierarchy for vehicular signs, the smallest element on the sign must equal the mandatory "x" value identified in Table 3.4.

Section 4.14 provides more detail on designing a sign using message hierarchy.

Figure 3.23 – Creating a message hierarchy improves the communication performance of a sign. The larger title (right) commands the reader's attention and introduces the core message, which is formatted in a smaller size.

3.7.10 Multiple Languages

In situations where another language in addition to the two official languages is required, the additional language must appear third in all cases whether the format be side-by-side or over-under. Care must be taken to ensure that the additional language is properly reviewed and edited and that line breaks are confirmed to be appropriate.

Figure 3.24 – Placement of third language text on sign

3.8 Considerations and Guiding Principles

The following considerations and principles will help guide sign planning teams within parks and sites to develop a comprehensive sign plan. As it is essential to understand how people interact with an unknown environment when creating a signage system, parks, sites and marine conservation areas are required to develop a professional sign plan using human factors specialists or sign programming professionals. The sign planning team appointed for each park/site/marine conservation area will facilitate the work of the sign planner and provide the site-specific context required to develop a sign plan.

3.8.1 Considerations

- Signage in a park or site must be planned and adapted to the specific features of a site so as to facilitate optimal use of the facilities.
- Signage in a park must be planned and adapted as a function of the experience that we want to offer visitors.
- Signage must be tailored to the categories of visitors and their mode of transportation: motorists, cyclists, hikers, walkers, paddlers, skiers, etc.
- Signage is part of the matrix of media that can inform and direct visitors. Visitors can also be informed or guided by reception staff, interpretation staff and publications, such as maps, brochures or pamphlets.
- Signage has an impact on the visitor's experience. Signage influences the quality of the landscape as perceived by the visitor. The quantity, dimensions and locations of signs can generate negative impacts and impair enjoyment of the landscape or cultural resource.
- In wayfinding, confirmation is the most basic and important principle. Visitors must be provided with feedback that they are making, or have made, the correct decision. It is important that all directional signs be followed by an identification sign that uses consistent nomenclature and delivery (verbal or non-verbal). For example, a direction to a campsite using a name and a symbol should be followed by the same name and symbol upon arrival at the destination.
- Providing universal access without compromising cultural resources should be an underlying goal in determining the location and methods for installing signs. Whenever possible, signs should be located in a manner accessible to persons with disabilities. Factors such as viewing distances, angles of sign faces and heights of signs should be addressed in the planning stages with professional input in order to achieve universal accessibility.
- Signage comprises a large variety of signs of diverse shapes and dimensions attached to various types of supports (posts and base) spread throughout a park/site/marine conservation area. It is important that a project lead be identified to manage all activities related to sign planning and implementation.
- Parks, sites and marine conservation areas offer many services and opportunities for engaging in a wide variety of activities. It is very difficult, and in fact impossible, to include all the available services and activities on signage, so choices must be made.

3.8.2 Principles

- Avoid over signing and sign congestion.
- Use a comprehensive planning approach that proceeds from the general to the specific.
- Use phases, inventories and analysis, develop options, evaluate options and make recommendations for the optimal solution.
- Direct visitors from the outside towards the main entrances and then towards the reception areas.
- Identify the main areas and buildings to help orient visitors.
- Always use the correct names or toponyms and ensure consistency with other communications media to enhance visitor understanding.

- Suggest changes to the facilities if a directional problem cannot be solved by signage.
- Position directional information sufficiently far in advance of an intersection so that visitors can read it and decide which direction to take.
- Use approved symbols to reduce the quantity of text on a sign.
- Avoid complex messages on signs—consider alternate delivery mediums.
- For pedestrians, position the most important or the most heavily used option on top; alternatively, the closest option can also be on top, if appropriate.
- If several trails leave from the same point, position the shortest one first.
- Use simple language to reduce the length of the text and the size of the signs.
- As a general rule, avoid using signage to publicize regulations, specifically prohibitions, where possible. Appropriate locations are fee collection booths and notice boards.
- Use information or welcome modules to confirm arrival in an area and to communicate information on activities and regulations.
- Use welcome modules to display location maps to help orient visitors.
- Use publications and maps with legends to enhance visitors' understanding of the symbols.
- Simple, clear language and short, familiar words make for the most effective signs. The sign message should be consistent, as short as possible and stated positively.
- Signs intended to be used back-to-back on the same posts should use the same panel size
- Where appropriate, work towards standard panel sizes for common areas such as trails, campgrounds, etc.

3.9 Sign Placement

3.9.1 Sign Placement

The posting of vehicular signs at appropriate locations is critical to ensuring visitors can find their way safely and contributes to a positive experience. Competing information and stimuli internal and external to the vehicle can place significant demands on a driver's attention, limiting time available to read signs. When searching for destination and directional information that cannot be readily found, drivers may perform dangerous manoeuvers. Sign placement must take into consideration a driver's need to glance at roadway signs and determine rapidly whether the information is important and requires action.

Three factors must be considered when determining where to locate vehicular signs: visibility distance, advance placement distance and separation distance.

Advance signs, signs advising drivers of exits, must be sited sufficiently in advance of intersections to allow drivers an adequate amount of time to detect and read the sign (visibility distance), and time to decelerate to an appropriate speed to make a comfortable turn (advance placement distance). Signs posted consecutively along a roadway must be separated by a sufficient distance (separation distance) allowing drivers enough time to detect and read the signs while safely operating their vehicles.

Speed, distance and time are the three factors that are considered in the process of calculating sign placement distances. Using these factors, distances are sometimes referred to in terms of time, e.g. signs must be separated a distance equivalent to 4 seconds at the posted speed limit.

Sign placement information on the following pages is based on human factors research using signs built to Parks Canada's specifications and on industry standards gleaned from MUTCD and the Transportation Association of Canada's *Supplemental Guide for Guide and Information Signage in Canada*.

3.9.2 Visibility Distance

Visibility distance refers to the distance at which the sign can be seen, and in the context of sign placement includes the period of time a driver needs to prepare to read and the time required to read the sign. Visibility distance will be affected by terrain and roadway design. Signs should be sited to ensure hills, curves and other features do not affect the required visibility distance.

A distance equal to 8–10 seconds at the posted speed limit is the recommended visibility distance.

3.9.3 Separation Distance

Separation distance is the distance between signs installed in succession along a roadway. A minimum separation distance will provide drivers with time to detect and read signs posted consecutively. A separation distance equivalent to 4 seconds at the posted speed limit is recommended.

3.9.4 Advance Placement Distance

Advance placement distance refers to the distance between a sign and the intersection or exit the sign is marking. Advance placement distances must be sufficient to allow drivers time to comfortably decelerate to make turns safely.

Calculations to determine advance placement distances consider the posted speed limit, vehicle deceleration rate, exit or turning speed, and legibility time. Parks Canada's advance placement distances have been developed through human factors research, testing drivers' response times, deceleration rates and turning speeds.

Advance placement distances for signs posted along multi-lane divided highways will need to be increased by a distance equal to 8 seconds at the posted speed limit to allow drivers time to make lane changes.

Advance placement distances longer than those cited in Table 3.5 may be required in situations where high volumes of traffic impede lane changes, and where drivers must decelerate to very slow speeds to make ninety degree turns or to turn onto a narrow road.

3.9.5 Confirmation Sign

A sign containing distance or directional information confirming that drivers have made the correct navigational decision should be posted at the visibility distance immediately following the intersection or exit from which the driver changed course.

Table 3.5 – Sign placement distances

Speed Zone (km/h)	Visibility distance (m)	Separation distance (m)	Advance placement distance (m)
50	100–150	50	25
60	125–175	65	50
70	150–200	75	75
80	175–225	90	125
90	200–250	100	150
100	225–275	110	200
110	250–300	125	250
90 multi-lane divided [1]	200–250	100	350
100 multi-lane divided [1]	225–275	110	425
110 multi-lane divided [1]	250–300	125	500

[1] Multi-lane divided highways require an addional 8 seconds of deceleration time to allow drivers to make a lane change.

3.9 Sign Placement (CONTINUED)

Figure 3.25 – Sign placement strategy
When siting signs, hills, curves and roadway design must be considered to ensure the required visibility distance of 8-10 seconds at the posted speed is maintained.

A distance of 4 seconds at the posted speed is recommended to separate signs installed in succession.

Advance placement distance calculations are influenced by legibility time, posted speed limit, deceleration rate and turning speed. Advance placement distances must be sufficient to allow drivers ample time to decelerate and make turns safely.

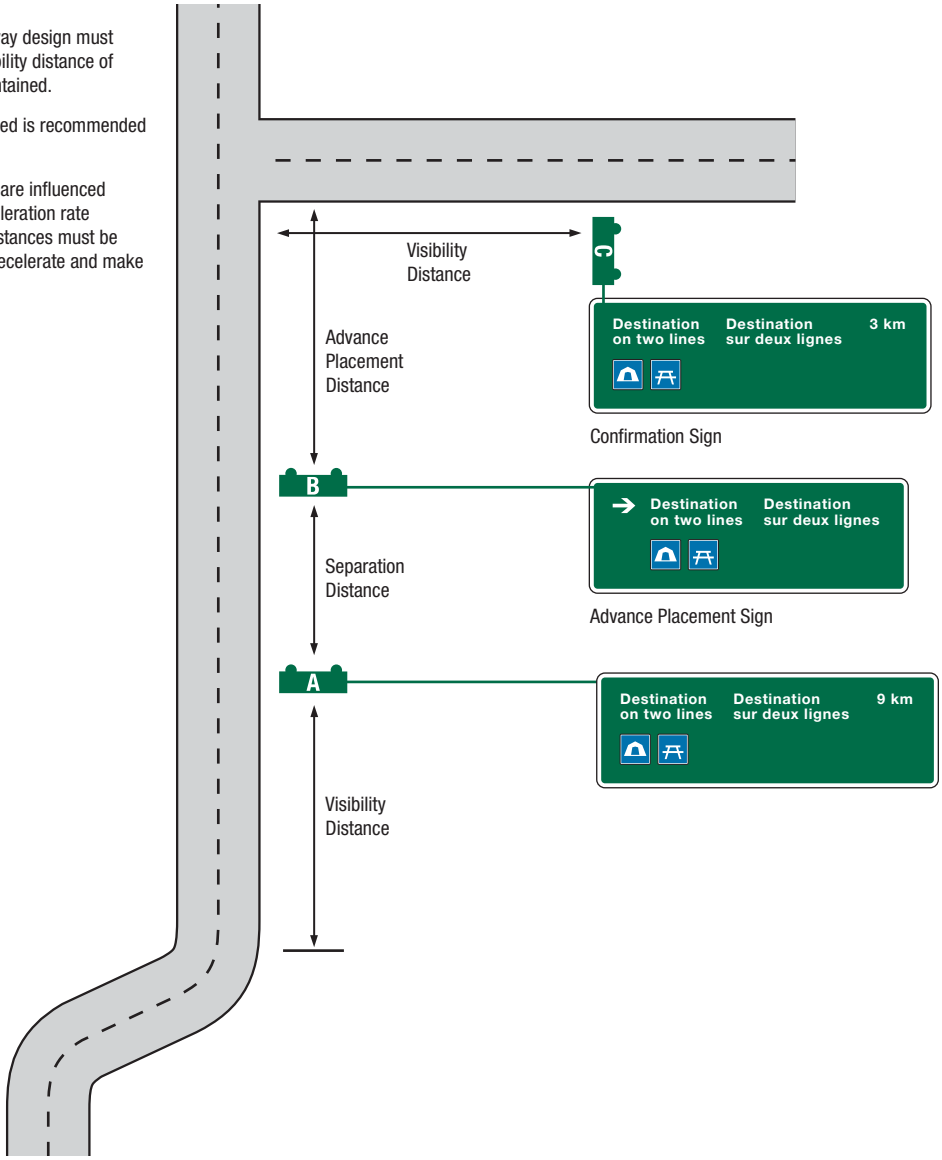
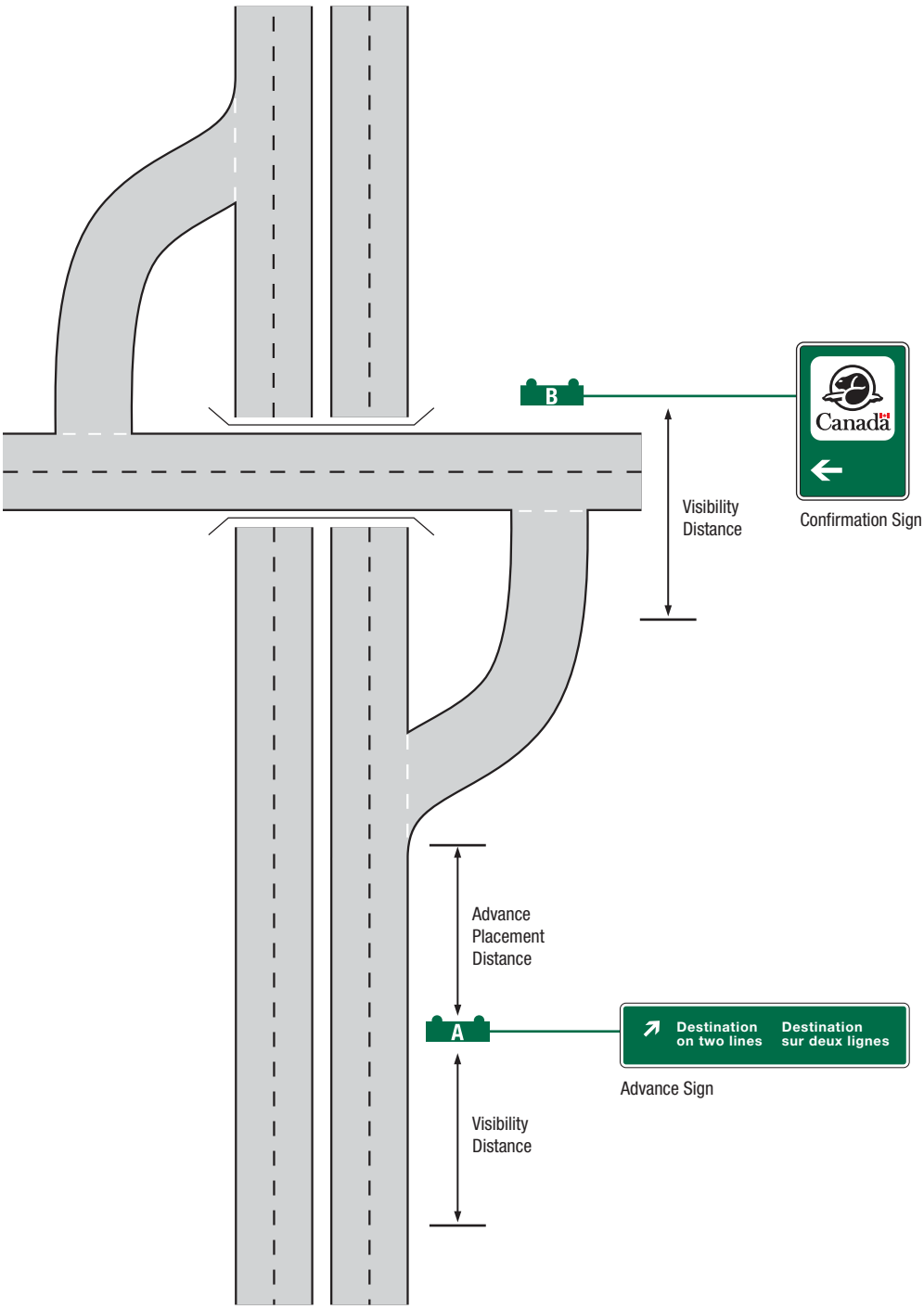


Figure 3.26 – Freeway exit sign placement
The advance sign (A), advising drivers of the exit, must be located at the appropriate advance placement distance before the exit. This should be followed by a confirmation sign (B) posted at the next decision point. A visibility distance appropriate for the posted speed for the respective signs should be maintained.



All national parks, national historic sites and national marine conservation areas require an approved sign plan prior to fabrication. An approved sign plan will consist of the following:

- site plan
- sign schedule
- general assembly drawings (GAD).

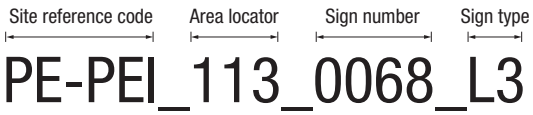
3.10.1 Sign Identification

Essential to developing a sign plan is the ability to identify individual signs. A sign identification code (sign ID) allows the park, site, or marine conservation area to track signs from siting and message development through construction and maintenance. Each sign is identified by a unique alphanumeric construct, composed of three to four references. In order, these codes are: site reference code, area locator (if applicable), sign number, and sign type. Each sign's unique code must be applied consistently to the site plan, sign schedule, general assembly drawing number and electronic file name to ensure accurate data management. *See Figures 3.27–3.28*.

Site Reference Code

All parks, sites and marine conservation areas in the Parks Canada system have been assigned a unique *site reference code*. All sign identification codes start with this abbreviation as a prefix.

Figure 3.27 – Sample sign ID composed of site reference code, area locator, sign number and sign type



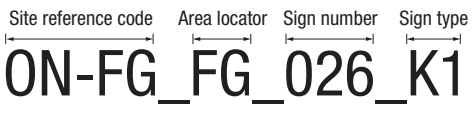
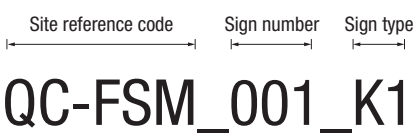


Figure 3.28 – Sample sign ID composed of site reference code, sign number and sign type



Area Locator

When developing a site plan, a park, site or marine conservation area may need to be subdivided into smaller sections *(Figures 3.30–3.31)*, in order to accommodate the number of signs that need to be represented on a single drawing *(Figures 3.35–3.36)*. These sections are cited in the sign identification as *area locators*. Area locators may be identified with an alpha or numeric code, and should be between two and four characters in length. Figures 3.30 and 3.31 illustrate two examples of typical sectioning. Although most scenarios will require sectioning, it is not necessary for all site plans.

Sign Number

The *sign number* is a unique identifier assigned to each sign in the system. Depending upon the magnitude of the system, it could be two to four digits. Sign numbers should be assigned sequentially and then be reset for each different area locator throughout the site. Resetting the numerical sequence allows for the future addition of new signs without large gaps in sign numbers.

For example, signs in the Fort George sector of Fort George NHSC are numbered ON-FG_FG_001 to ON-FG_FG_033 while signs in the Parking Lot sector of Fort George are numbered ON-FG_PL_001 to ON-FG_PL_012.

Sign Type

Depending upon use and content, each type of sign has a unique code, based on the Parks Canada sign classification system (*Section 3.3*). S*ign type* is a two-character alphanumeric code.

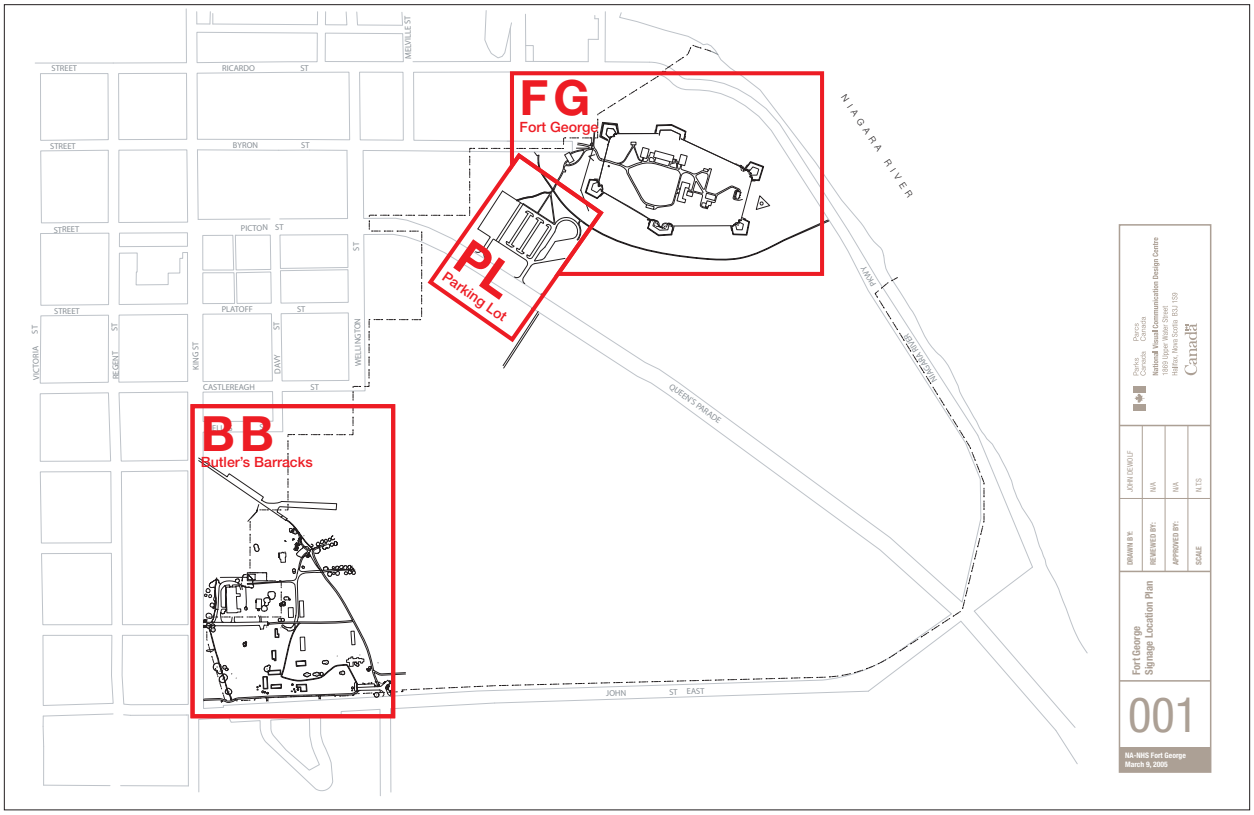
Figure 3.29 – Sample area locator prefixes

Campground	CG## or C###
Day Use	DU## or D###
Gateway	GT## or G###
Highway	HW## or H###
Trailhead	TH## or T###
Visitor Centre	VC## or V###

3.10 Sign Planning: Components (CONTINUED)

Figure 3.30 – Sample index of area locators for a national park

Figure 3.31 – Sample index of area locators for a national historic site



3.10.2 Site Plan

The development of a conceptual and schematic site plan involves plotting the location of each individual sign on a base map or survey drawing based on the wayfinding requirements and specific issues of the site. This site plan captures pertinent information associated with the siting of signs, including but not limited to sign identification, sign location, panel direction, and number of sign panel faces. The initial stage in the development of the plan may invlove notes and hand-drawn maps which eventually evolve into electronic files. *See Figure 3.39*. The site reference code is often not included with each sign location mark-up on the site plan as this is redundant information.

For ease of review, site plans for smaller sites may additionally include thumbnail illustrations of individual signs and their content. *See Figure 3.36*.

Figure 3.35 provides an example of a typical site plan for a national park, while Figure 3.36 provides an example of a typical site plan for a national historic site.

Figure 3.32 – Sign siting examples

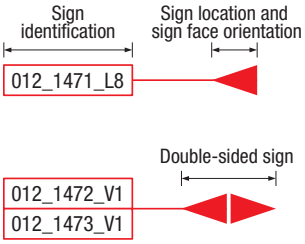
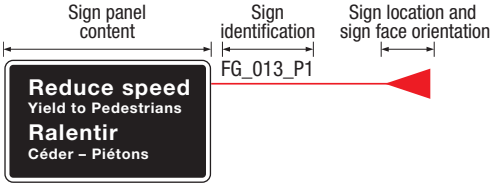


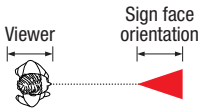
Figure 3.33 – Sign siting example



Figures 3.32 and 3.33 show examples of two typical sign siting mark-ups. In Figure 3.32, the mark-up includes sign identification and location. As shown in Figure 3.33, in addition to indicating sign identification and location, the mark-up may also illustrate the content. In this example, the sign has been drawn as to the standards in this manual; however, it is not necessary that the content be drawn to this level of detail.

The direction of the sign panel face is indicated by the arrow head, which faces the viewer, as illustrated in Figure 3.34.

Figure 3.34 – Sign face orientation example



3.10 Sign Planning: Components (CONTINUED)

Figure 3.35 – Sample site plan (national park)

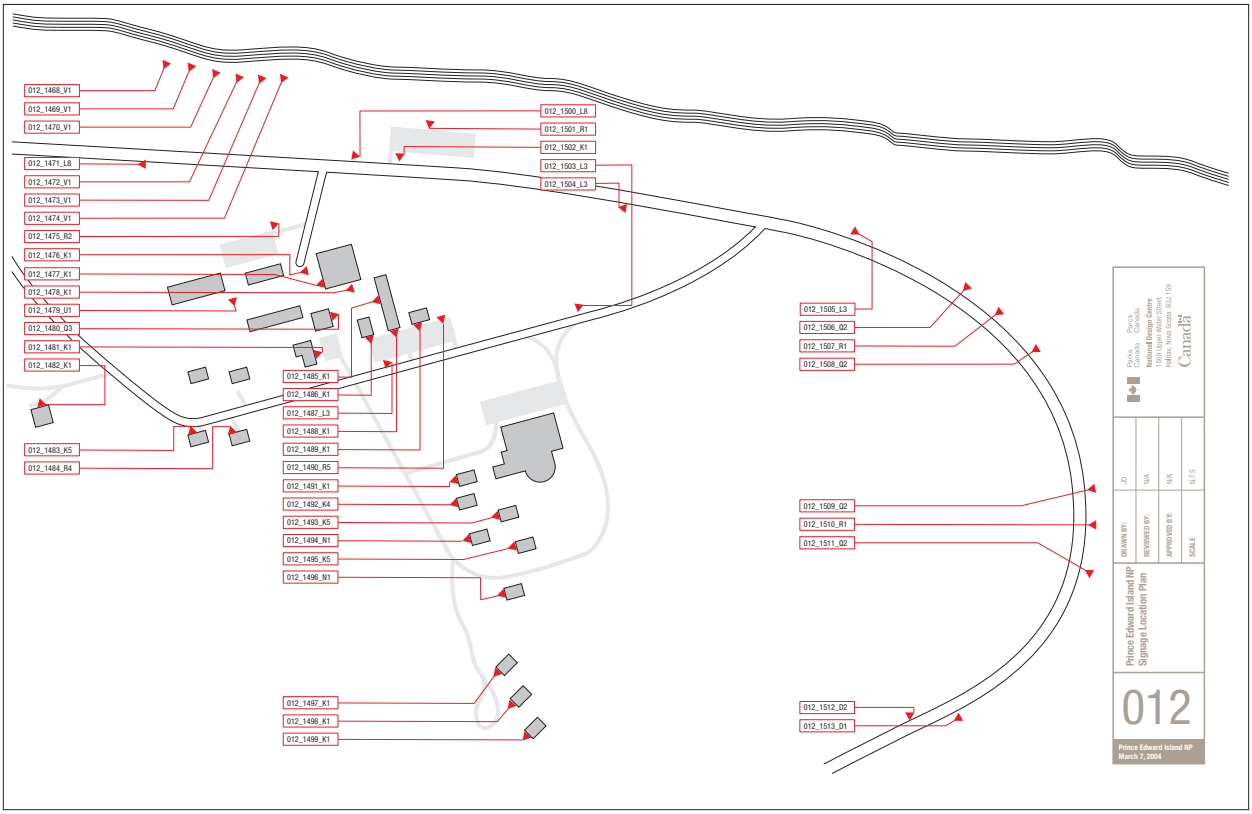
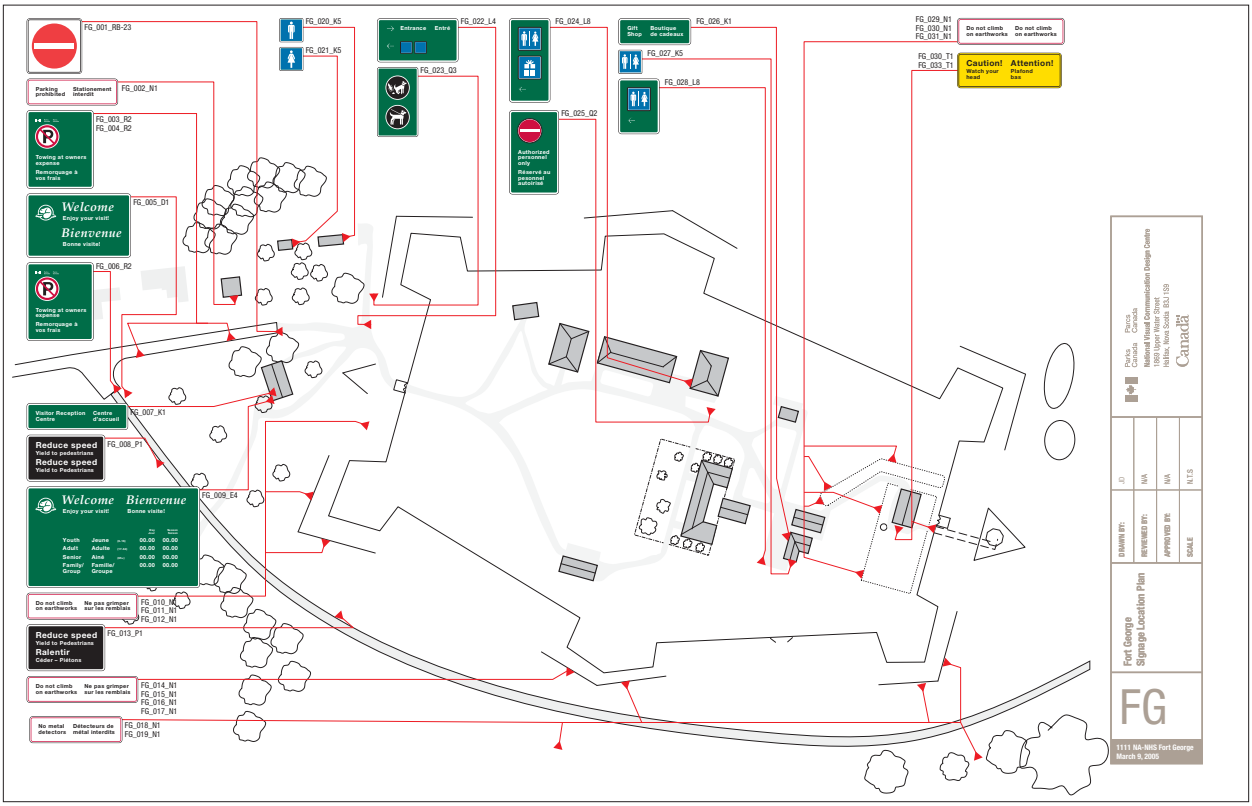


Figure 3.36 – Sample site plan (national historic site)



3.10.3 Sign Schedule

This is a "spreadsheet" type report/database that lists all the sign entries in a sign plan and captures all of the relevant information associated with the sign, including but not limited to sign messaging, type, post type, installation requirements/issues, vehicle speed, character height, size, etc. There are core fields of information that must be captured when developing a sign schedule (see Table 3.6). Site-specific fields of information may be added to address individual site requirements.

Figure 3.37 – Sign schedule form

Basic information							
Drawing number	Area locator	Sign number	Sign type	Layout option	Vehicular speed (km/h)	Pedestrian viewing distance (m)	x-height (mm)
NN-NNN_YOW_0001_L4	YOW	0001	L4				
NN-NNN_YOW_0002_L1	YOW	0002	L1				
NN-NNN_YOW_0003_P1	YOW	0003	P1				
NN-NNN_YOW_0004_L1	YOW	0004	L1				
NN-NNN_YOW_0005_R1	YOW	0005	R1				

Legend/Message				
Arrow (N,E,S,W)	English message	French message	Symbol identification	Distance indicator

Installation										
Post type	Post finish	Post qty	Post length (mm)	Installation height (mm) (baseline)	Installation height (mm) (centre line)	Installation depth (mm)	Installation deviation (mm)	Panel height (mm)	Panel width (mm)	Substrate
			3800	1200		1200	150	1200	2400	
			4300	1200		1200	150	1750	2175	
			3400	1200		1200	200	750	1500	
			2800	1200		1000	400	200	100	
			5000	1200		1200	300	2250	1625	

Installation (continued)				Notes			
Alternate installation (flag,wall,etc.)	Back-to-back with sign #	Easting	Northing	Field notes	Site comments	Design notes	Image reference no.
							PA310060.JPG

3.10 Sign Planning: Components (CONTINUED)

Table 3.6 – Sign schedule information fields
The following fields of information are required for various phases of the sign planning process. Some information will not be available until a certain phase is completed (ex. installation height can be determined only following the completion of the GAD).

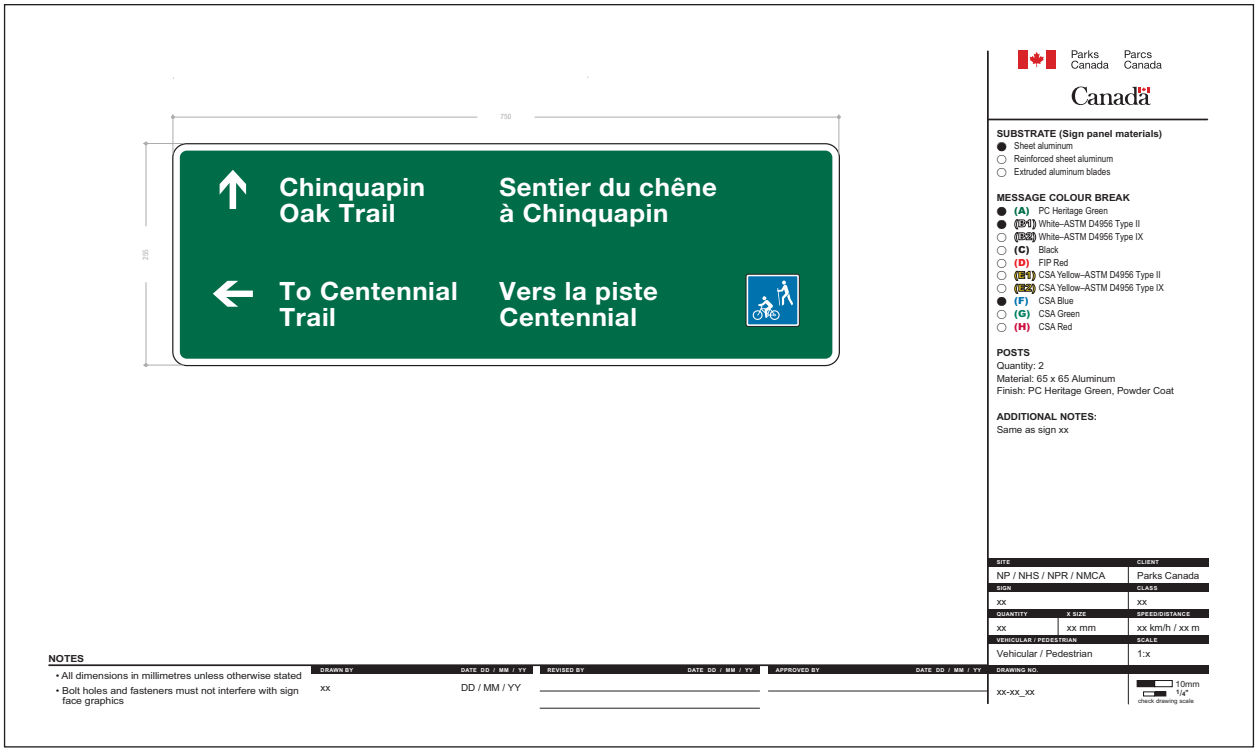
Information field	Description
Drawing number	The drawing number is a combination of the site reference code, area locator, sign number and sign type. The drawing number helps facilitate the management of multiple signage files.
Site reference code	Is an abbreviation of a park, site or marine conservation area name used as a prefix.
Area locator	Area locators are a means of subdividing large parks, sites or marine conservation areas into smaller sections. An area locater is identified with an alpha or numeric code and should be between two and four characters (ex. Highway 10 = HW10).
Sign number	The sign number is the unique identifier that is assigned to each sign and ranges from two to four digits. Sign numbers are assigned sequentially and reset for each area locator.
Sign type	The sign type is a unique two-digit alphanumeric code based on the Parks Canada sign classification system.
Layout option	Refers to the various layout options available for each sign type. Appendix D provides a complete breakdown of layout options by sign type.
Vehicular speed (km/h)	Refers to the posted speed limit of the road where the sign will be sited.
Pedestrian viewing distance (m)	Applies to pedestrian signs only and refers to the intended distance the sign should be viewed at. Consideration should be given to identifying a consistent viewing distance for signs within a common geographic area.
x-height (mm)	Determined by either the posted speed limit of the roadway or required pedestrian viewing distance. X-height refers to the size of the lower case characters or the dimension of each cell of the layout grid.
Arrow	Can be used to represent 8 different directions and 4 advance turn requirements.
English message	English text message
French message	French text message
Symbol identification	The numeric coding that identifies each symbol.
Distance indicator	The distance value required for a destination expressed in kilometres or metres.
Post type	Specifies the type of post to be used. See Section 5 for approved post options.
Post finish	Specific to aluminum posts, specifies if post is to be powder-coated Parks Canada Heritage Green or Black.
Post quantity	Number of posts required depending on sign size.
Post length (mm)	The length of the post is determined by adding the installation depth, installation height (centre line or baseline), and panel height. To allow for greater cost savings, posts should be matched to the closest 100 mm increment.
Installation height (mm) (baseline)	Refers to the installation height to the bottom of the sign. Appropriate installation heights based on sign type are identified in Section 6.3.
Installation height (mm) (centre line)	Refers to the installation height to the centre of the sign. Appropriate installation heights based on sign type are identified in Section 6.3.
Installation depth (mm)	The depth at which sign posts should be installed in various geographic areas across the country. Individual parks and sites are responsible for identifying required installation depths for sign posts in their area. It is also an opportunity to provide general notes on installation conditions.
Installation deviation (mm)	Refers to installation situations where one post may be installed at a different depth (ex. up or downslope) and requires that one add or subtract (+/-) for install depth.
Panel width (mm)	Refers to the width of the sign and is determined after the general assembly drawing is approved. This information is essential for determining the number of posts needed to secure the sign.
Panel height (mm)	Refers to the height of the sign and is determined after the general assembly drawing is approved. Panel height is essential for calculating post length.
Substrate	Refers to aluminum, aluminum reinforced or extruded aluminum blades.
Alternate installation (flag, wall, etc.)	Information related to a non-standard installation type that is site-specific.
Back-to-back with sign #	Information on signs that are back-to-back and would require design adjustments to ensure equal panel width and height.
Easting	Refers to GPS coordinates (Easting).
Northing	Refers to GPS coordinates (Northing).
Field notes	To provide any additional information pertinent to the sign.
Site comments	Site input/comments/change requests.
Design notes	Information to site on design issues, approaches or adjustments.
Image reference no.	Refers to photograph image file name in support of a specific sign.

3.10.4 General Assembly Drawings (GAD)

The general assembly drawing phase involves the development of detailed technical drawings for each of the signs identified in the sign plan.

The GAD will incorporate limited information required for fabrication and represents the final approval process for messaging, size, substrate, colour break and post specifications prior to fabrication. Detailed material, fabrication and installation specifications can be found in Sections 5 and 6. See Figure 3.38 for an example of a general assembly drawing.

Figure 3.38 – Example of a general assembly drawing



3.11 Sign Planning: Approach and Methodology

The following resources are required to facilitate development of a comprehensive Sign Plan. It is the responsibility of each park/site/marine conservation area to secure all site-specific resources prior to beginning the sign plan.

1. Current digital versions of area maps that include all vehicular routes (public and service roads), pedestrian trails, campsite boundaries, campsite facilities, building footprints (including entrances), etc. in an acceptable digital format (DFX, DWG, etc.). Recommended scales are 1:25,000 or 1:50,000 for vehicular routes and 1:5,000 or 1:10,000 for day-use, campground areas, visitor reception areas and pedestrian trails.
2. A full set of maps printed at the above scales for working documents.
3. Maps at a scale of 1:100,000 to 1:1,000,000 to document desired positioning of off-site signage (to be coordinated with appropriate provincial/municipal authorities).
4. Sites are encouraged to use the sign survey form provided in Section 3.13 to develop their specific sign schedule. This form is to be used as a field tool to collect preliminary information that will be revised and refined during the development of the electronic sign schedule. It is important that the fields of information identified be completed and that the appropriate coding developed for the signage program be used.
5. Current visitor surveys, publications, guide maps, and other visitor service products that are provided as part of the visitor experience. This will ensure proper terminology is being supplied consistently.

The following section outlines the recommended approach, resources required and deliverables for each phase of the sign planning process.

3.11.1 Phase I – Pre-planning and Orientation

1. ***Establishment of the sign planning team***
   Each park and site will be required to identify a sign planning team at the outset of the process and clearly define what their roles and responsibilities will be related to the project. The sign planning team will be responsible for assisting the sign planner in the development of the sign plan and be available as resources to answer questions regarding the park or site. Depending on the size of the park or site, the sign planning team should consist of:
   - Sign planner (specialist in wayfinding, human factors, sign programming, etc.)
   - Project lead – Appointed by FUS/Site Superintendent to be the management lead throughout the life of the project
   - Representatives from asset maintenance (including GIS specialists), visitor services, communications and resource conservation – to ensure all areas of concern with signage within the park or site are appropriately represented

2. ***Resource collection and orientation***
   The sign planning team will be required to collect all base maps and auxiliary information related to the park or site that will assist in the sign planning exercise. This includes securing available maps from the park or site, reviewing visitor guides and other communications products related to the site (Web site) and any existing relevant visitor surveys that identify specific issues with respect to signage and navigation within the park or site

3. ***Pre-sign planning meeting***
   Prior to the first on-site visit, the sign planning team will confer via teleconference to discuss the sign planning process, address any questions/issues related to the on-site visit and establish clear, site-specific objectives for the overall sign planning process.

3.11.2 Phase II – On-site Planning

1. ***Familiarization with park or site***
   The sign planning team will meet to address issues raised during the pre-sign planning meeting. Depending on the size of the park or site, a preliminary tour of the park or site may be required in order to determine the best approach to completing the sign plan within the allotted time. The sign planning team will confirm if the existing base maps reflect the current situation of the park or site.

2. ***Documentation of existing conditions***
   The sign planning team will document existing conditions and digitally photograph existing signs where required. Images will provide contextual information and serve as a reference throughout the sign planning process.

3. ***Development of preliminary site plan and sign schedule***
   The sign planning team will identify the approximate location of the proposed new signs on the existing base map and capture all essential information, including sign type, viewing distance, vehicular speed, installation information and message content etc. Initial GPS coordinates should be captured for larger parks and sites. During the development of the preliminary site plan and sign schedule, multiple tours of the park or site may be required to address complex issues.

   Also, in determining the approximate location of vehicular signs, it is important that the sign planner be familiar with Parks Canada advanced sign placement requirements. See Section 3.9 for more information.

4. ***Data confirmation***
   Following the preliminary site plan and data collection, the sign planning team will meet to confirm the information captured for each new sign. This should take place prior to the departure of the sign planner to allow time to address any issues while on site.

3.11.3 Phase III – Sign Schedule Development

1. ***Development of electronic sign schedule***
   The sign planner will take all of the information gathered during the on-site planning process and record it in digital format. The electronic sign schedule document will become the master approval document during the sign planning phase. The sign planning team will use the document to record changes during the life of the sign planning process as well as all essential information related to each sign. Figure 3.37 provides an example of an electronic sign schedule. It is the responsibility of the sign planning team to identify the preferred sign layout option *(see Appendix D)* prior to proceeding with the development of the general assembly drawings.

2. ***Translation***
   Following confirmation of the information and digitization of the sign schedule, the park or site will arrange for the translation, vetting and approval of the content for each sign.

3. ***Sign schedule approval***
   Following translation of the sign schedule, the park or site will approve the completed sign schedule and return it to the sign planner. The approved sign schedule must include all information related to both fabrication and installation *(note: installation height can be identified only following the development of the GADS and must be added to the sign schedule once determined)*.

3.11.4 Phase IV - Preparation of General Assembly Drawings

1. ***Development of detailed general assembly drawings***
   The sign planner will take the approved sign schedule and arrange for the development of the detailed general assembly drawings. The sign planner will review and approve each GAD in accordance with the Parks Canada Standards and Guidelines for Exterior Signage prior to sending to the park or site for final approval. In order to manage the numerous signs associated with a sign plan, a file-naming convention has been developed to standardize and facilitate the approval process. See *Section 3.10 – Sign Planning: Components* for more information.

2. ***Review and approval of detailed GADS***
   The entire sign planning team should review and approve each general assembly drawing from a content, fabrication and installation perspective. The approval of the GAD constitutes the final approval prior to fabrication of the signs. All required changes must be submitted to the sign planner.

3.11.5 Phase V – Finalization of Sign Plan

1. ***Site plan***
   Following the approval of the sign schedule and accompanying general assembly drawings, each individual sign must be accurately plotted to the base map to finalize the new site plan. Large parks and sites may use GPS to accurately identify the location of each new sign. The park and site must then approve the site plan prior to fabrication.

2. ***Sign schedule***
   Following the approval of the GADS, the park or site is responsible for ensuring that the information contained in the sign schedule accurately reflects the information on the GAD. Any discrepancies or omissions must be addressed and clarified with the sign planner prior to fabrication.

   Following the approval of the site plan, sign schedule and general assembly drawings, the approved signs are ready for fabrication.

   Following the completion of a comprehensive sign planning exercise, regular review and updating will be required. Parks and sites should review their approved sign plans on a regular basis and make adjustments as required. As signage is an essential and integrated wayfinding tool for the visitor, any addition or removal of signs must be considered in the context of the overall sign plan. As such, signs cannot simply be erected. Each addition or removal of a sign must be approved by the sign planning team and be incorporated into the approved sign plan. This requires a revision to the sign schedule and development and approval of the GAD.

3.12 Sign Planning: Reviewing, Adding or Deleting Signs

Following the completion of a comprehensive sign planning exercise, regular review and updating will be required. Parks and sites should review their approved sign plans on a regular bases and make adjustment as required. As the sign program is an essential and integrated wayfinding tool for the visitor, any addition or removal of signs must be considered in the context of the overall sign plan. As such, signs cannot simply be erected. Additions or removal of signs must be approved by the sign planning team and be incorporated into the approved sign plan. This requires a revision to the sign schedule and development and approval of the GAD.

3.13 Sign Survey Form

Figure 3.39 - Sign survey form

Area loc.	Sign no.	Sign type
HII4	001	
Easting		
Northing		
Grade +/-		
Post		
Photo ID		

km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+
m	6	9	12	15	30	50	60	75	90	120
"x"	10	15	20	25	50	75	100	125	150	200

Notes

AT BRIDGEHEAD
4x4 WOOD

100-0068

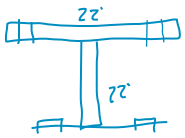
Arrow	Message	Symbol	Distance
	UPPER		
	SALMON		
	RIVER		

Area loc.	Sign no.	Sign type
HII4	002	
Easting		
Northing		
Grade +/-		
Post		
Photo ID		

km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+
m	6	9	12	15	30	50	60	75	90	120
"x"	10	15	20	25	50	75	100	125	150	200

Notes

ON EXISTING ALUMINUM
OVERHEAD STRUCTURE
LEFT LANE
100-0069 TO 0072



Arrow	Message	Symbol	Distance
↓	THROUGH TRAFFIC		
	HWY 114 WEST	HWY 114	
	SUSSEX		
	ST. JOHN		
✱	TRACY TO CONFIRM		

Area loc.	Sign no.	Sign type
HII4	003	
Easting		
Northing		
Grade +/-		
Post		
Photo ID		

km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+
m	6	9	12	15	30	50	60	75	90	120
"x"	10	15	20	25	50	75	100	125	150	200

Notes

ON EXISTING ALUMINUM
OVERHEAD STRUCTURE
RIGHT LANE
100-0069 TO 0072

Arrow	Message	Symbol	Distance
↓	PARK PERMITS		
	PURCHASE		
	R PARK ENTRY		
	PERMITS		
✱	TRACY TO CONFIRM		

Area loc.	Sign no.	Sign type
HII4	004	
Easting		
Northing		
Grade +/-		
Post		
Photo ID		

km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+
m	6	9	12	15	30	50	60	75	90	120
"x"	10	15	20	25	50	75	100	125	150	200

Notes

MOUNT TO KIOSK BELOW
WINDOW IN RIGHT LANE
WALL = 38"x84"
100-0073, 0074

Arrow	Message	Symbol	Distance
✱	SEE PHOTO		

Area loc.	Sign no.	Sign type
HII4	005	
Easting		
Northing		
Grade +/-		
Post		
Photo ID		

km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+
m	6	9	12	15	30	50	60	75	90	120
"x"	10	15	20	25	50	75	100	125	150	200

Notes

AT FIRST MEDIAN ISLAND
4x4 WOOD

Arrow	Message	Symbol	Distance
	NO CAMPING		
	NO OVERNIGHT PARKING		

Area loc.	Sign no.	Sign type
HII4	006	
Easting		
Northing		
Grade +/-		
Post		
Photo ID		

km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+
m	6	9	12	15	30	50	60	75	90	120
"x"	10	15	20	25	50	75	100	125	150	200

Notes

4x4 WOOD

100-0075

Arrow	Message	Symbol	Distance
→	ALMA	LOOKOUT	
	BEACH		

3.13 Sign Survey Form (CONTINUED)

Figure 3.40 - Sign survey form

Area loc.	Sign no.	Sign type	km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+	Arrow	Message	Symbol	Distance
			m	6	9	12	15	30	50	60	75	90	120				
			"x"	10	15	20	25	50	75	100	125	150	200				
Easting			Notes														
Northing																	
Grade +/-																	
Post																	
Photo ID																	

Area loc.	Sign no.	Sign type	km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+	Arrow	Message	Symbol	Distance
			m	6	9	12	15	30	50	60	75	90	120				
			"x"	10	15	20	25	50	75	100	125	150	200				
Easting			Notes														
Northing																	
Grade +/-																	
Post																	
Photo ID																	

Area loc.	Sign no.	Sign type	km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+	Arrow	Message	Symbol	Distance
			m	6	9	12	15	30	50	60	75	90	120				
			"x"	10	15	20	25	50	75	100	125	150	200				
Easting			Notes														
Northing																	
Grade +/-																	
Post																	
Photo ID																	

Area loc.	Sign no.	Sign type	km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+	Arrow	Message	Symbol	Distance
			m	6	9	12	15	30	50	60	75	90	120				
			"x"	10	15	20	25	50	75	100	125	150	200				
Easting			Notes														
Northing																	
Grade +/-																	
Post																	
Photo ID																	

Area loc.	Sign no.	Sign type	km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+	Arrow	Message	Symbol	Distance
			m	6	9	12	15	30	50	60	75	90	120				
			"x"	10	15	20	25	50	75	100	125	150	200				
Easting			Notes														
Northing																	
Grade +/-																	
Post																	
Photo ID																	

Area loc.	Sign no.	Sign type	km/h	—	—	—	0-15	20-30	35-40	45-60	65-75	80-90	95+	Arrow	Message	Symbol	Distance
			m	6	9	12	15	30	50	60	75	90	120				
			"x"	10	15	20	25	50	75	100	125	150	200				
Easting			Notes														
Northing																	
Grade +/-																	
Post																	
Photo ID																	

4

Layout System

Layout System

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4.1 Overview

4.1.1 Introduction

Section 4 provides an overview of the layout system and the principles used to determine how the message elements—arrow, text message, symbol and distance—along with the sign border and the colour palette are applied to a sign face. This section should be used in conjunction with Appendices D and E. *Appendix D–Sign Type Configuration* details sign type (classification), layout options, sign function and sign application. *Appendix E – Layout Specifications* provides detailed sign layout specifications for each sign type. Instructions to assist in developing a sign layout and specifications for the preparation of general assembly drawings are also provided.

The following process will assist navigation through the manual and the location of pertinent information when preparing a sign layout:

1. Determine the sign type (classification) provided on the sign schedule, e.g. L3.
2. Consult *Appendix D – Sign Type Configuration* to confirm the desired layout for sign type L3 (e.g. 01).
3. Look up the corresponding layout (e.g. 01) in *Appendix E – Layout Specifications* to obtain precise layout instructions and specifications.

4.1.2 Sign Size

Most layouts are developed on the basis of two key factors: length of text and the size of the characters or symbols required. Determining the appropriate character and symbol size depends on a variety of factors, including whether or not the sign is intended for vehicular or pedestrian traffic and what the intended viewing distance or traffic speed is.

When producing sign layouts the following principles should be followed:

- As a general rule, signs are designed from the inside out; that is, content and application determine sign size.
- The mandatory character and symbol size, based on the viewing distance or traffic speed, must always be used.
- Spacing standards between message elements must be followed.
- Smaller signs are preferred but safety and legibility must always be the overarching factors in sign design.

4.1.3 Layout Adjustments

Adjustments to layout may be required to improve the fit and finish of a sign structure. For example, when signs are installed back-to-back, on shared posts and content differences yield different-sized panels, the layout of one or both panels may be adjusted to achieve equivalent panel sizes. Signs designed for fabrication on extruded aluminium blades may benefit from a layout adjustment to optimize the number of blades required for a sign.

In all cases, adjustments to sign layouts that require deviations from the recommended sizes of the message elements should be made only if the legibility of a sign is not compromised and the modifications do not create a safety hazard. When adjustments are required, input should be obtained from a qualified sign designer to help achieve the desired effect. See *Section 4.12 – Modifying Sign Margins* for more information.

Ensuring the sign fits the environment prior to fabrication is key to supporting the organization's ecological and cultural integrity values. Evaluating the size of a draft sign relative to its proposed installed location during the layout stage will assist in determining an appropriate size.

Maintaining high aesthetic standards by reducing visual clutter and contributing to the consistent and orderly presentation of information is an important underlying objective of Parks Canada's signage program.

4.1.4 Research and Specifications

Section 3 – Sign Planning contains background information and research findings leading to decisions on message sequence, message load, message size and viewing times.

4.2 Message Elements

4.2.1 Message Elements

Parks Canada's operational signs will contain a combination of up to four message elements—directional arrow, text message (English and French), symbol and numeric element (distance or identification number)—collectively referred to as the "message unit". These message elements are arranged in a predetermined sequence on a layout grid composed of square cells.

4.2.2 Sign Border

The perimeter of each sign shall contain a border, the function of which is to increase detection and visibility (conspicuity) of the sign, especially during the hours of darkness.

4.2.3 Layout Grid

Although invisible on the finished sign, the layout grid is an integral component of the sign layout system. It provides the framework on which the message elements and border are arranged. The grid brings regularity and order to the layout and ensures consistency throughout the sign system, a fundamental tenet of the Parks Canada Identity Program.

Figure 4.1 – The message elements

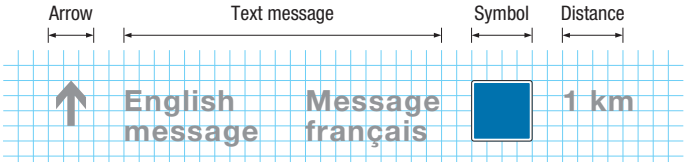


Figure 4.2 – Sign borders

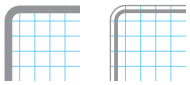
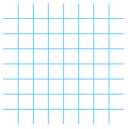


Figure 4.3 – The layout grid



4.3 The Directional Arrow

4.3.1 Applying the Arrow

The directional arrow always functions in conjunction with a message (text and/or symbol) and is integrated into the sign layout. To assist in determining the position of an arrow in a layout, the arrow is fitted with a square non-printing key-line. The key-line is used as a guide when scaling and positioning the arrow on the layout grid.

Approved digital artwork for each directional arrow is available.

Section 3.5.1 – The Directional Arrow provides further information on the sequence or order in which multiple destinations should appear on a directional sign.

Figure 4.4 – Eight different directions

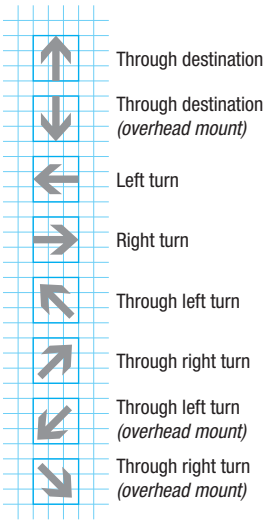


Figure 4.5 – Advance turn arrows

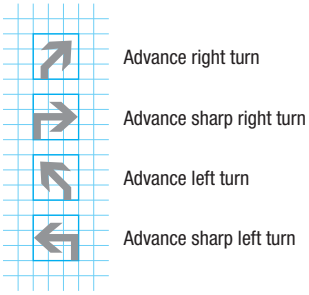


Figure 4.6 – The arrow with square non-printing keyline

Figure 4.7 – On vehicular signs, the straight ahead arrow appears first, followed by the left and right arrows.

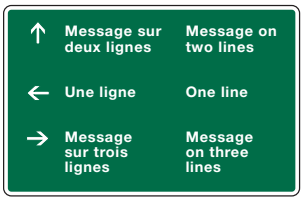
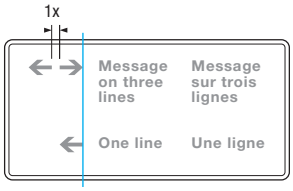


Figure 4.8 – When signing multiple directions, left and right, for a single destination, the left and right arrow are used with a 1x space between. If a second destination appears on the same sign, the arrows are right aligned as shown.



4.4 Typeface

The presentation of typography in a consistent format plays a fundamental role in corporate identity. In the 1980s the FIP adopted Helvetica medium as the signage typeface, to provide a graphic standard for the Government's signage system and to achieve consistency in the production of signs.

The signage typeface includes all characters commonly used in sign messages, including letters, numerals, typographic symbols and punctuation marks.

Due to the variations in the design of Helvetica and the difficulty in ensuring compatibility among suppliers and fabricators, Helvetica Neue 75© has been adopted as the official typeface for all Parks Canada signs. All contracts for the design and fabrication of signs must specify Helvetica Neue 75©. Substitutions with any other font are not acceptable.

Helvetica Neue© is regarded by typographers and designers as the premium version of Helvetica. This contemporary design is versatile and highly legible.

Recent human factors testing has confirmed that Helvetica Neue 75©, when configured following the layout, fabrication and installation specifications described in this manual, meets acceptable legibility standards for vehicular signage.

Copyright for Helvetica Neue© is held by Linotype® and is available under licence from Adobe®. Specifying Helvetica Neue 75© for Parks Canada signage will ensure a common look on a system-wide level and consistency when revising or updating messages locally.

This font is available in PostScript and TrueType formats for both Macintosh® and Windows®. The font can be purchased online from Linotype®, Adobe® and the FontShop®. It is the responsibility of all contractors to purchase the required fonts and hold a valid user licence agreement for the font software in question when performing design and fabrication work on any Parks Canada sign.

Figure 4.9 – Helvetica Neue 75© is used for the Parks Canada signature and sign messages

ABCDEFGHIJKLMNOPQRSTUVWXYZ ÉÇ`ˆ¨ŒÆ
abcdefghijklmnopqrstuvwxyz éç`ˆ¨œæ
1234567890. , : ; “ ” « » ? ! / ( ) $

The History of Helvetica

Helvetica was originally designed by Max Miedinger in Switzerland in the late 1950s. The typeface was heavily promoted by the Linotype Foundry and grew in popularity in the 1960s and 1970s. During the pre-postscript/desktop publishing age of phototypesetting, many type foundries and manufacturers of phototypesetting equipment produced their own versions of Helvetica.

This led to the proliferation of many versions of Helvetica with unique differences and idiosyncrasies; the same weight was often referred to by different names, the stroke thickness, x-height, and numerous other design features varied from one version to another.

In 1983, Linotype remedied this situation by redrawing the entire Helvetica family. Differences in alignment were corrected, subtle features were made consistent from one face to another, and all the weights and widths were designed to work together as one family. This new drawing is called Neue Helvetica (German for New Helvetica), and incorporates an easy-to-use numbering system to accurately identify various styles and weights.

Palatino Bold Italic© is the secondary typeface for signage and has been adopted for limited use only on special-purpose signs not subject to FIP signage typeface requirements.

The design of this typeface originates from the Renaissance and contrasts the modern style of Helvetica. A very legible face, Palatino Bold Italic© is elegant, warm and inviting. As part of the Parks Canada signage system, it is used exclusively on APPROVED special-purpose signs aimed at providing a welcome message.

Copyright for Palatino Bold Italic© is held by Linotype® and is available under licence from Adobe®. This font is available in PostScript and TrueType formats for both Macintosh® and Windows®. The font can be purchased online from Linotype®, Adobe® and the FontShop®. It is the responsibility of all contractors to purchase the required fonts and hold a valid user licence agreement for the font software in question when performing design and fabrication work on any Parks Canada sign.

Figure 4.10 – Palatino Bold Italic© is used for the welcome message on special purpose signs.

ABCDEFGHIJKLMNOPQRSTUVWXYZ ÉÇ`ˆ¨ŒÆ
abcdefghijklmnopqrstuvwxyz éç`ˆ¨œæ
1234567890.,:;“”« »?!/()$

4.5 Letterspacing and Tracking

The spacing between characters and words affects the legibility of a typographic message. Because each character has a unique shape, the combination of specific letters, numerals and punctuation may yield irregular gaps and impede legibility, or disrupt the visual integrity of a word.

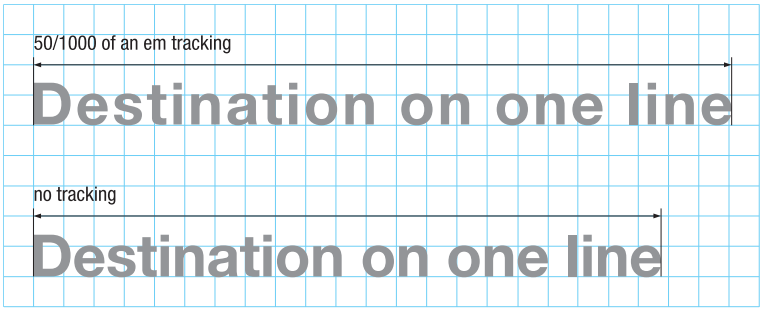
In signage, where the need for large character sizes is determined by traffic speed, viewing distance and viewing time, the tendency is to increase the amount of space between characters and words.

To ensure detection, legibility and comprehensibility during the hours of darkness, Parks Canada signs use retro-reflective materials for messaging, applied to an opaque, non-reflective dark background. When viewed under nighttime conditions with an artificial light source—vehicle headlights—these signs can produce a blurring effect called halation, in which a halo can appear around the edge of the retro-reflective message.

Increased letterspacing improves legibility on these signs by reducing the effect of halation. The extra space between characters serves as a buffer, providing a negative area on which the halo is absorbed.

Tracking on all Parks Canada signs is increased the equivalent of 50/1000 of an em.

Figure 4.11 – Comparison between 50/1000 of an em tracking and no tracking



4.6 Graphic Symbols

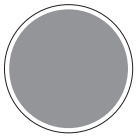
4.6.1 Classification of Symbols

The Parks Canada symbol set, based on that of the Canadian Standards Association and adopted by FIP, is designed to convey regulatory, warning and information messages to visitors at a glance. To ensure consistency across the Parks Canada system, over 300 symbols have been developed to communicate the vast range of activities and requirements within national parks, national historic sites and national marine conservation areas. An overview of the symbol set classification is provided in the table below.

Each symbol in the set has been tested for comprehensibility following the International Standards Organization methodology, ISO 9186. The Parks Canada symbol set has been approved for use based on each symbol's comprehensibility score. To preserve the integrity of the symbols and maintain maximum comprehensibility, symbols must not be modified in any way.

Only Parks Canada approved symbols are permitted on Parks Canada signs. Any requirement for a symbol not part of the symbol set should be forwarded to the Parks Canada Identity Program Officer. If the symbol is identified as a requirement, proper comprehensibility testing will be conducted prior to release.

Figure 4.1 – Parks Canada symbol set

Class and geometric shape	Type and colour	Function of symbols	Subject classification	
Regulatory signs 	1 **Prohibition** Red and black on white	To indicate an order forbidding an action.	1-1 Access and activities	
	2 **Mandatory** White on black	To indicate an order for obligatory action.	2-1 Activities, protective apparel and equipment	
Warning signs	3 **Caution** Black on yellow	To indicate a potential hazard.	3-1 Potential hazards	
	4 **Danger** White on red	To indicate a definite hazard.	4-1 Definite hazards	
Information signs	5 **Emergency** White on green	To provide information required in case of emergency.	5-1 Facilities and equipment	
	6 **Guidance and information** White on blue	To provide general information.	6-1 Public services 6-2 Concessions 6-3 Public transportation 6-4 Recreation	

4.6.2 Size Relationship of Shapes

A specific size relationship between the three symbol shapes has been established. It is based on the need to compensate for the differences in the surface area and apparent size of the circle, triangle and square.

To establish a range of sizes for the symbols, all sizes have been based on the height of the square.

The relationship is as follows:

- Diameter of the circle = 1.1
- Height of the equilateral triangle = 1.1
- Height of the square = 1.0

To assist in determining the appropriate size and position of the symbols in a sign layout, each circular and triangular symbol is fitted with a square non-printing key-line. The key-line is used as a guide when scaling and positioning the symbol on the layout grid. Square symbols do not require a key-line as the perimeter of the square is used to adjust size and position.

A layout should not be attempted with a symbol that does not contain a key-line as errors in size and position could result.

Figure 4.12 – Relative size of symbol shapes

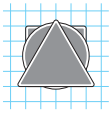


Figure 4.13 – Relationship of square keyline to symbol shape

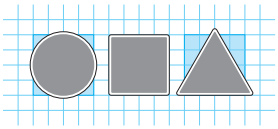
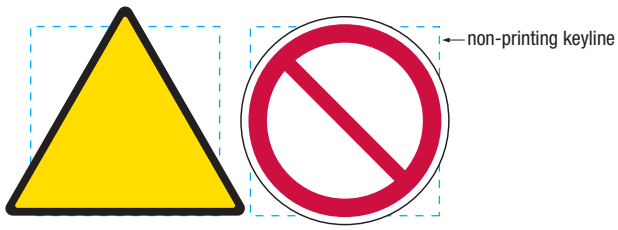


Figure 4.14 – A non-printing keyline is used to assist in scaling and positioning the symbols on the layout grid.



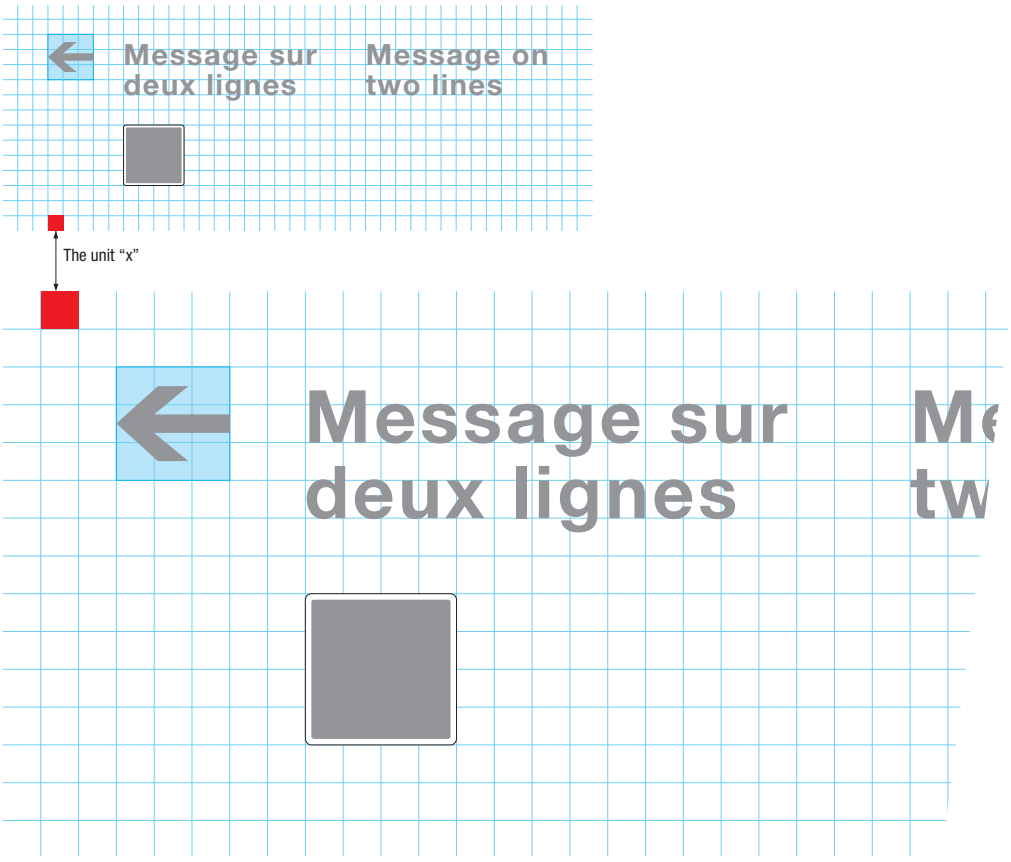
4.7 Proportional Measurement System

4.7.1 Layout Grid

The Parks Canada sign layout system employs a layout grid which has been developed to provide guidance for the consistent positioning and scaling of the message unit: directional arrow, text message (English and French), symbol and numeric element (distance or identification number) and the border. The layout grid is composed of uniform square cells. The size of each cell is referred to as "x". All measurements pertaining to sign layout are expressed in "x", which is measured in millimeters.

The size of the grid is flexible, and can be scaled proportionately to accommodate a range of pedestrian or vehicular applications. The size of the arrows, text, symbols and border has been predetermined by performing legibility distance index calculations. This process considers vehicle speed, viewing distance and reading time in determining the appropriate size of the message elements.

Figure 4.15 – Both examples below are at the same scale. The top example uses a 25 mm grid, the bottom example a 50 mm grid.



4.7.2 Arrow Size

The directional arrow plays a key role in wayfinding and for this reason has been assigned the dominant left position on a sign. Human factors testing has confirmed that a 3x arrow is comprehensible from the same distance as 1x typography. The 3x arrow has therefore been adopted as the standard size for directional signs.

4.7.3 Character Size

The unit "x" is equal to the height of the lower case characters of the text message. This typographic measurement is commonly referred to as the "x-height".

The majority of sign messages will use a character size of "1x". In some situations it may be desirable to create a visual hierarchy by enlarging a key message element—"Notice", "Attention", "Danger"—and contrasting this against a secondary message configured in a smaller character size *(see Section 4.14 – Creating Message Hierarchy)*.

4.7.4 Symbol Size

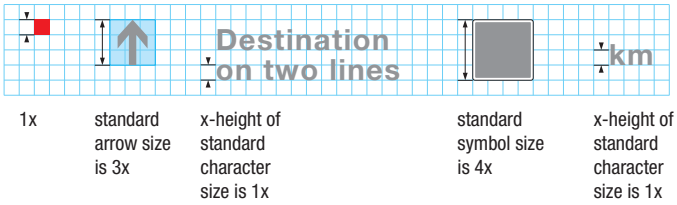
A symbol size of 4x has been adopted as the standard for signs on which the symbol is secondary to the text message.

To improve the performance of the symbol, and reinforce its role as primary message element, the symbol is increased to 8x when used on a symbol sign.

4.7.5 Size Specifications

Table 4.2 provides size specifications for the layout grid (the unit "x") and the message elements, for a specified range of vehicle speeds and viewing distances.

Figure 4.16 – Element sizes



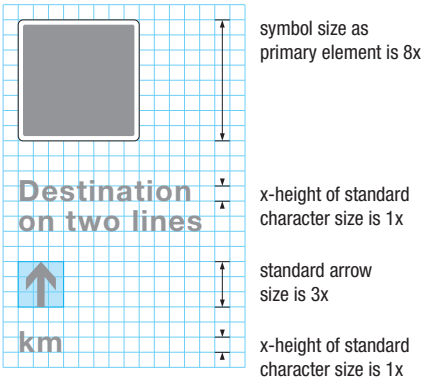


Table 4.2 – Vehicular speed / Pedestrian viewing distance and standard size of message elements

Vehicular speed	**—**	**—**	**—**	**0-15 km/h**	**20-30 km/h**	**35-40 km/h**	**45-60 km/h**	**65-75 km/h**	**80-90 km/h**	**95+ km/h**
Pedestrian viewing distance	**6 m**	**9 m**	**12 m**	**15 m**	**30 m**	**50 m**	**60 m**	**75 m**	**90 m**	**120 m**
The unit "x"	**10 mm**	**15 mm**	**20 mm**	**25 mm**	**50 mm**	**75 mm**	**100 mm**	**125 mm**	**150 mm**	**200+ mm**
Arrow size (3x)	30 mm	45 mm	60 mm	75 mm	150 mm	225 mm	300 mm	375 mm	450 mm	600 mm
Character size (1x)	10 mm	15 mm	20 mm	25 mm	50 mm	75 mm	100 mm	125 mm	150 mm	200 mm
Symbol size (4x)	40 mm	60 mm	80 mm	100 mm	200 mm	300 mm	400 mm	500 mm	600 mm	800 mm
Symbol size (8x)	80 mm	120 mm	160 mm	200 mm	400 mm	600 mm	800 mm	1000 mm	1200 mm	1600 mm
Distance size (1x)	10 mm	15 mm	20 mm	25 mm	50 mm	75 mm	100 mm	125 mm	150 mm	200 mm

4.8 Border Size

Retroreflective borders shall be used on all Parks Canada signs. This improves conspicuity and ensures that all sign's edges are clearly visible during the hours of darkness. Borders will be flush with the edges of the sign.

Figure 4.17 – Standard white 1/2x sign border for emergency, mandatory and danger text signs

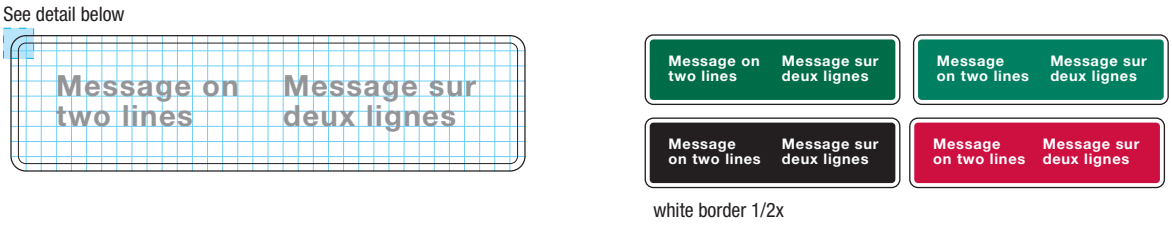


Figure 4.18 – Sign border detail

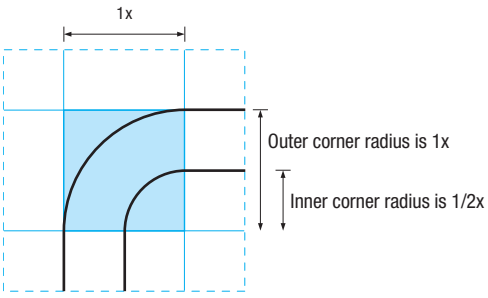


Figure 4.19 – Sign border detail for blade signs
Blade signs have square corners. The triangular corner area outside the border is the same colour as the sign background.

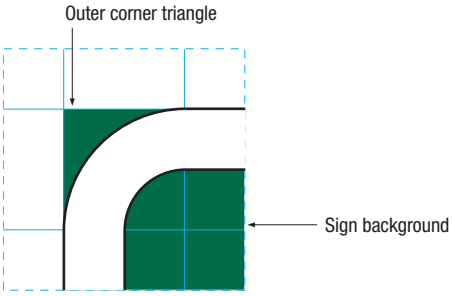
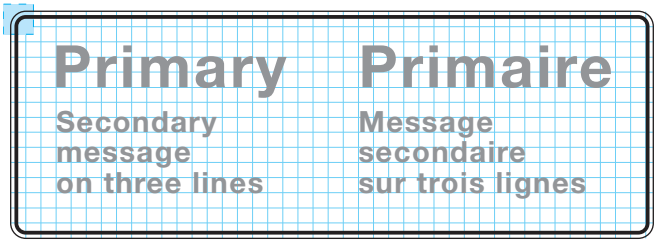


Figure 4.20 – Black border on yellow for caution text signs

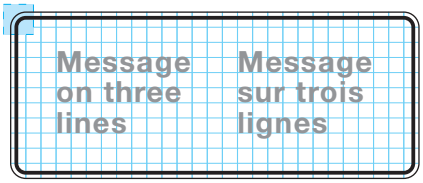
See detail below



yellow exterior border 1/4x
black interior border 1/4x

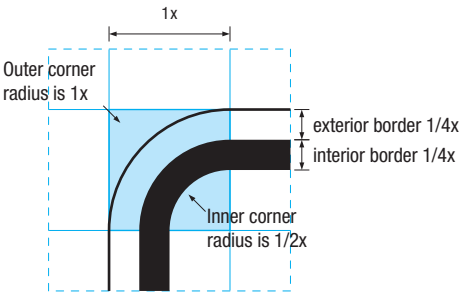
Figure 4.21 – Red border on white for prohibition text signs

See detail below



white exterior border 1/4x
red interior border 1/4x

Figure 4.22 – Caution and prohibition sign border detail



4.9 Alignment of Message Elements

The examples below illustrate the standard alignments used on most signs. Refer to Appendix E for alignment instructions specific to each sign type.

A Top of arrow (key line) and/or top of symbol is placed 1x above x-height of first line of text message.

B Baseline of numeric element is aligned with baseline of first line of text message.

C Elements (arrow, text, symbol, distance) in separate information units are left aligned with each other.

Figure 4.23 – Example of layout 1

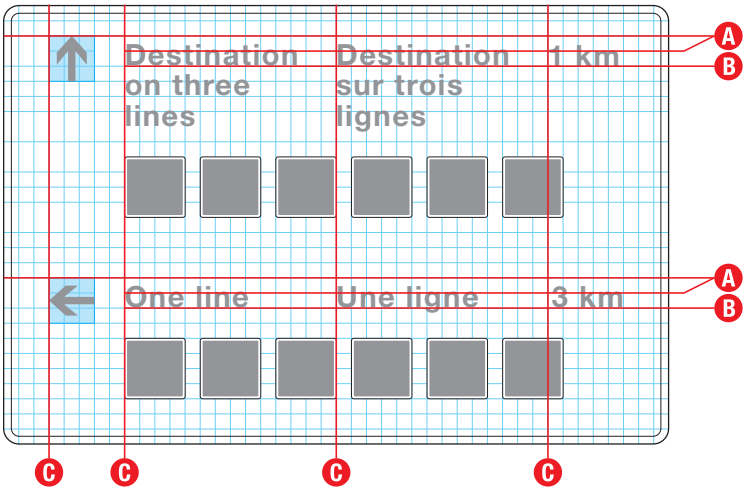
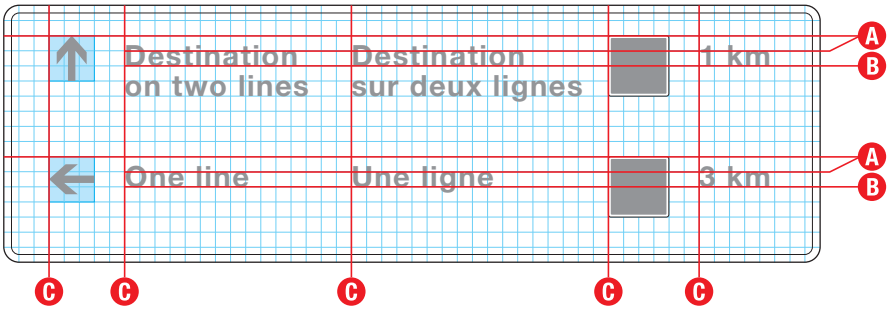


Figure 4.24 – Example of layout 3



4.9 Alignment of Message Elements (CONTINUED)

Figure 4.25 – Example of layout 2

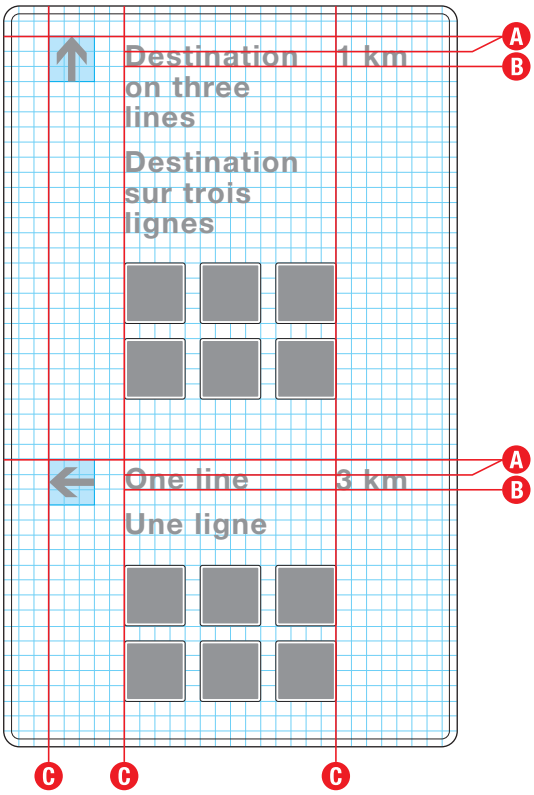


Figure 4.26 – Example of layout 4

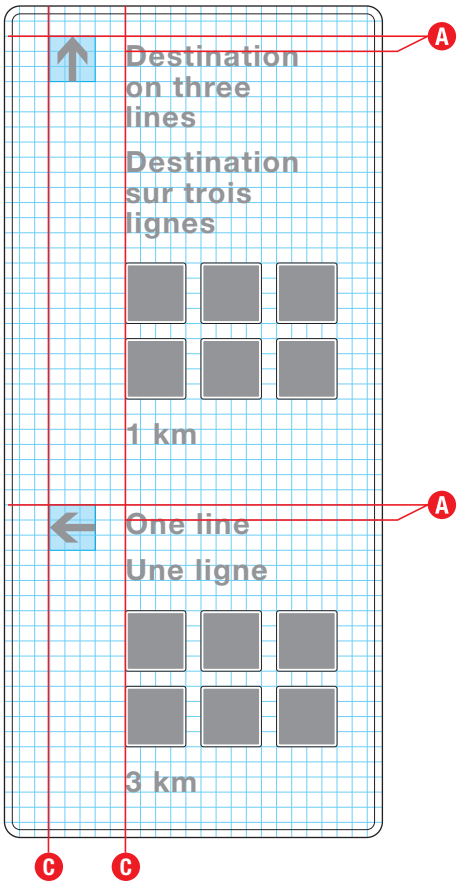
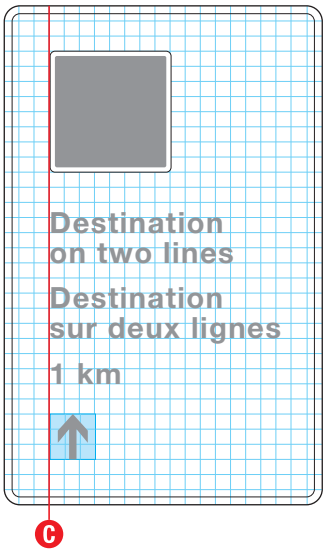


Figure 4.27 – Example of layout 6



4.10 Spacing Between Message Elements

4.10.1 Standard Spacing

The examples below illustrate the standard spacing between message elements. Exceptions to these standards are found most often when designing off-site signs, primary identification signs and special-purpose signs. Refer to Appendix E for spacing instructions specific to each sign layout option.

Each message element—arrow, text, symbol—requires a predetermined space surrounding it whether used alone or with other elements. All layout specifications reference the grid and are expressed in terms of "x".

Figure 4.28 – Example of layout 1

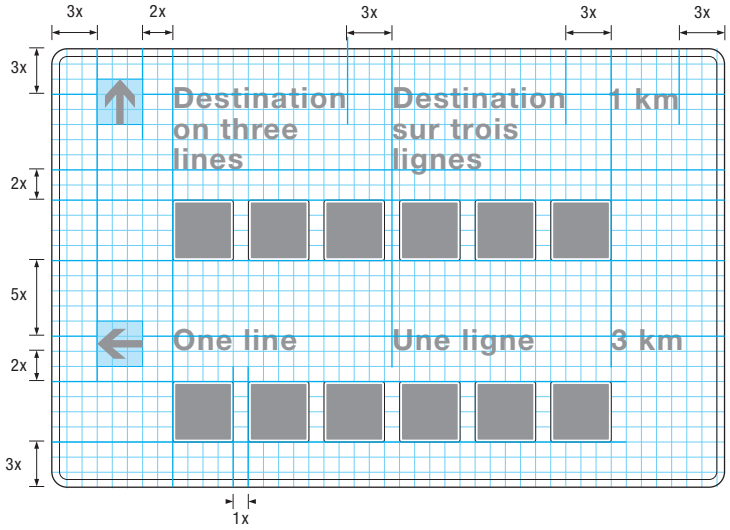
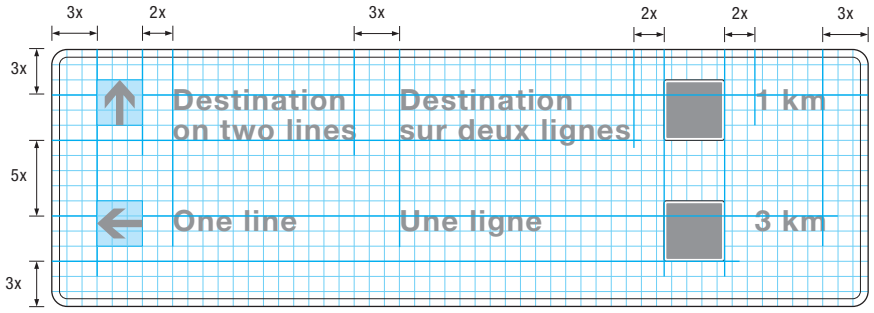


Figure 4.29 – Example of layout 3



4.10 Spacing Between Message Elements (CONTINUED)

Figure 4.30 – Example of layout 2, vehicular

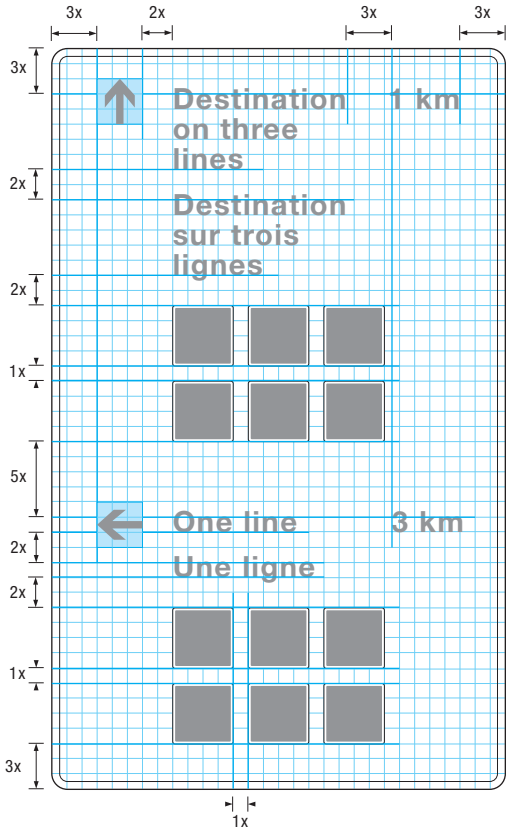


Figure 4.31 – Example of layout 4, pedestrian

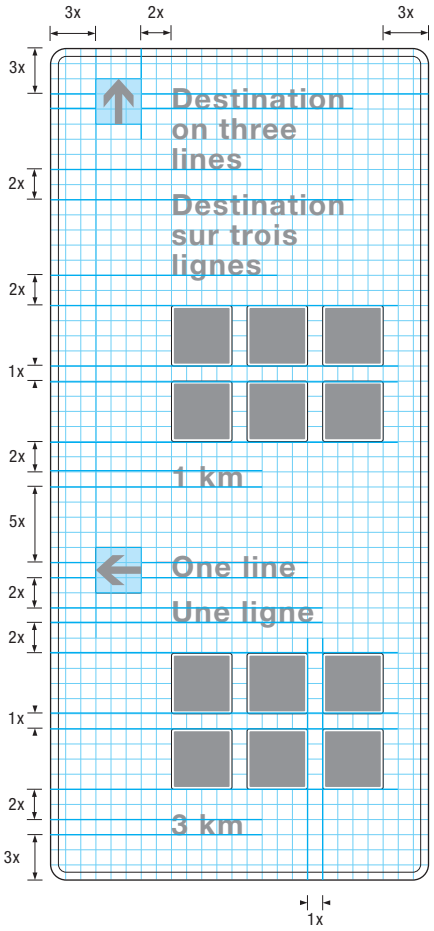


Figure 4.32 – Example of layout 6

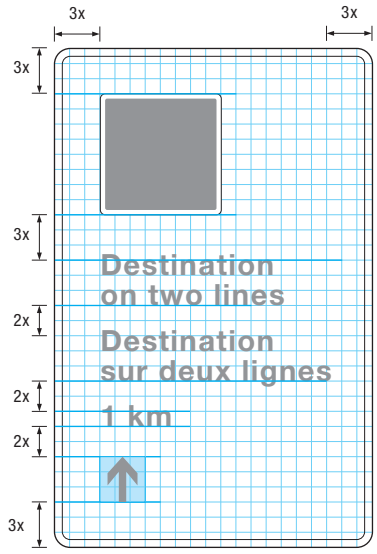
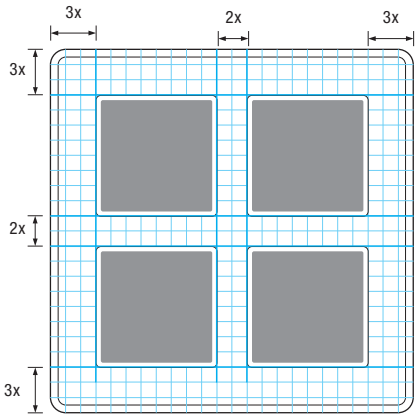


Figure 4.33 – Example of layout 7



4.10.2 Spacing Adjustments

Current releases of drawing software provide for objects (text, graphics, etc.) to snap to the layout grid. Respecting the precise spacing specification between text columns or between text and symbols may result in objects falling off the layout grid.

Minor adjustments (1/2x or less) to the spacing may be implemented to facilitate all elements snapping to the grid.

Figure 4.34 – Precise 3x spacing between text

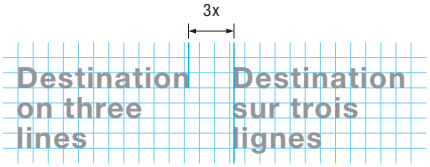
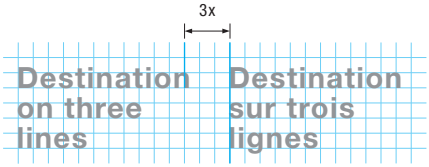


Figure 4.35 – 3x spacing between text adjusted so text snaps to layout grid



The rag of a multi-line message may yield an excess of negative space between languages. The net visual effect of the negative space allows for the alignment of the second language to the grid, without appearing too tight.

4.11 Margins

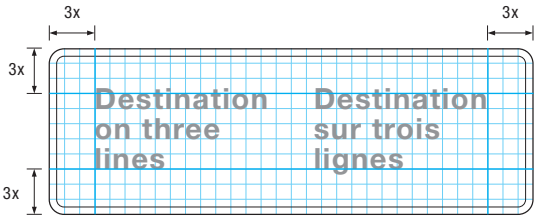
4.11.1 Standard Margins

A standard perimeter margin of 3x is maintained between the message elements and the sign edge.

Section 4.10 – Spacing Between Message Elements provides detailed specifications for margins as applied to a variety of layouts and messaging combinations.

Signs fabricated with extruded aluminium blades may benefit economically from an adjustment to the top and bottom margins. The following pages provide an explanation of this gain.

Figure 4.36 – Standard margins



4.11.2 Margins on Extruded Aluminium Blade Signs

Extruded aluminium blades are available in a standard height of 305 mm. Extruded blade sign substrates are created by assembling the required number of blades to incorporate the message, including the top and bottom margins and the border. The height of blade signs is therefore a multiple of 305 mm.

Parks Canada's layout standards specify all margins—top, left, right and bottom—as 3x. Signs are typically laid out from top to bottom. Given the fixed blade height and prescribed layout specifications, there will be situations where the bottom margin exceeds 3x.

If the bottom margin is excessive, a reduction can be made to the top and bottom margins, in order to delete one blade from the sign assembly.

When performing this layout adjustment, the measure for the top margin is 2.5x. The bottom margin must not be less that 2.5x; however, it may exceed 3x.

If reducing the margins to 2.5x does not provide a sufficient reduction in height to eliminate a blade, the sign must be fabricated using the standard top margin specification of 3x and a variable bottom margin, with no reduction in the number of blades. In all situations, the bottom margin must be equal to or greater than the top margin.

Table 4.3 identifies the sign heights by "x" value that are candidates for a blade reduction.

Figure 4.37 – Excessive bottom margin
Comparison of sign layout with standard 3x top and bottom margins (left) to 3x top margin and bottom margin expanded when configured for blades (right).
x = 125 mm

Sign layout with standard 3x top and bottom margins.

Sign layout with standard 3x top margin, when configured for blades yields excessive bottom margin.

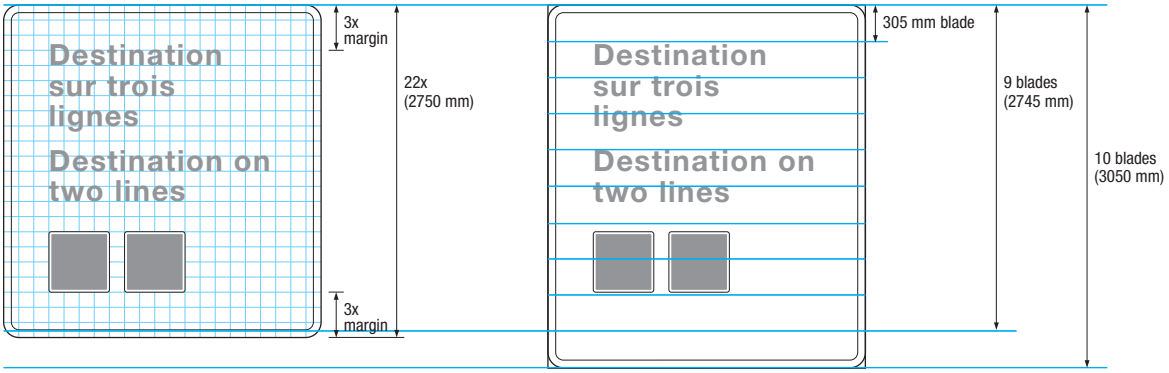


Figure 4.38 – Layout adjustment
Same layout as above with 2.5x top and bottom margins (left). The example on the right shows this layout adjusted for blades. The top margin remains 2.5x, while the bottom margin is slighty greater than 2.5x. One less blade is required to build the sign.
x = 125 mm

Sign layout with 2.5x top and bottom margins.

Sign layout with 2.5x top margin configured for blades.

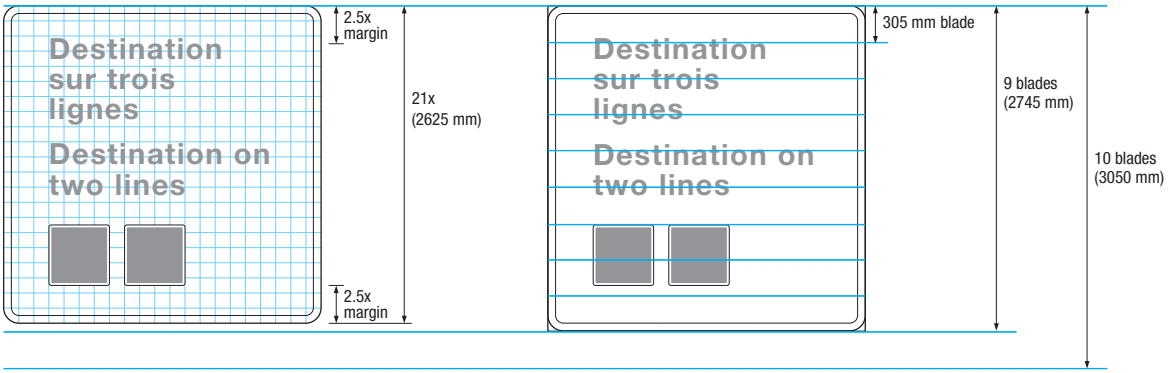


Figure 4.39 – Expanded bottom margin
Comparison of sign layout with standard 3x top and bottom margins (left) to 3x top margin and bottom margin expanded when configured for blades (right).
x = 125 mm

Sign layout with standard 3x top and bottom margins.

Sign layout with standard 3x top margin and expanded bottom margin when configured for blades.

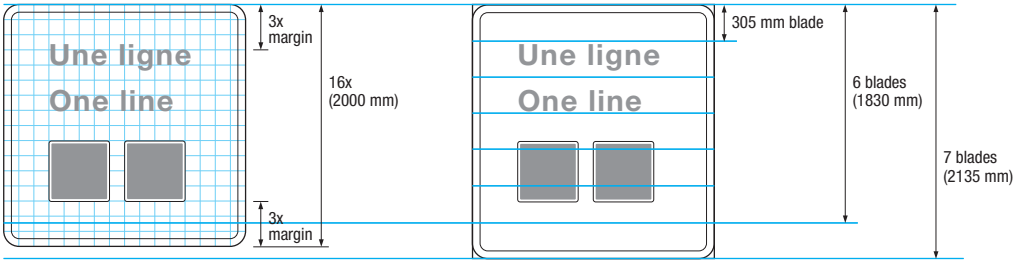
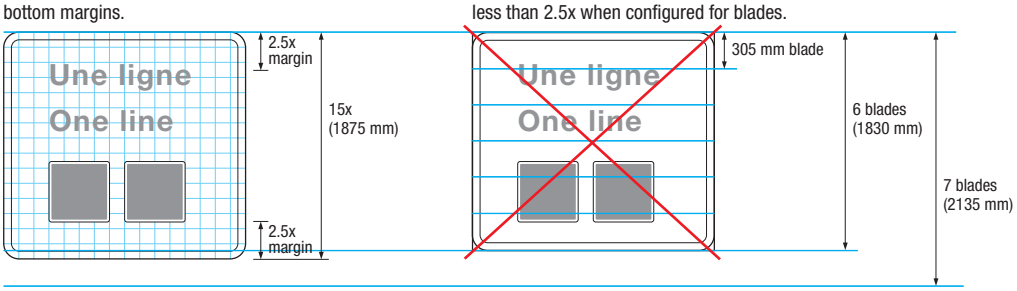


Figure 4.40 – Bottom margin less than 2.5x
Same layout as above with 2.5x top and bottom margins (left). The example on the right shows this layout adjusted for blades. The top margin remains 2.5x. The resulting bottom margin is less than 2.5x, therefore the number of blades is not reduced. A 3x top margin must be used with 7 blades as shown in Figure 4.39 above.
x = 125 mm

Sign layout with 2.5x top and bottom margins.

Sign layout with 2.5x top margin yields bottom margin less than 2.5x when configured for blades.



4.11 Margins (CONTINUED)

Table 4.3 – Reducing the number of blades on a sign
The following table identifies sign heights by "x" value which are candidates for a reduction in the number of blades required on a sign when the top margin is adjusted to 2.5x. If the sign height is equal to one of the heights listed below, the top margin may be reduced to 2.5x. The resulting bottom margin must be a minimum of 2.5x. Signs with these heights may be fabricated with one less blade.

For example, a sign with an "x" value of 125 mm, laid out with the standard 3x top and bottom margins, depending on message load, could yield a height of 2250 mm. This sign would require 8 extruded blades to fabricate. If the top and bottom margins are adjusted to 2.5x, the resulting sign will be reduced in height by 1x, to 2125 mm, and fit comfortably on a substrate composed of 7 blades.

Sign height with 3x top and bottom margins						**Revised sign height**
x = 50 mm	**x = 75 mm**	**x = 100 mm**	**x = 125 mm**	**x = 150 mm**	**x = 200 mm**	**x = 50, 75, 100, 125, 150 & 200 mm**
—	675 mm	700 mm	—	—	—	610 mm (2 blades)
—	975 mm	1000 mm	1000 mm	1050 mm	—	915 mm (3 blades)
1250 mm	1275 mm	1300 mm	1250 mm	1350 mm	1400 mm	1220 mm (4 blades)
1550 mm	1575 mm	1600 mm	1625 mm	1650 mm	1600 mm	1525 mm (5 blades)
1850 mm	1875 mm	1900 mm	1875 mm	1950 mm	2000 mm	1830 mm (6 blades)
—	2175 mm	2200 mm	2250 mm	2250 mm	2200 mm	2135 mm (7 blades)
—	2475 mm	2500 mm	2500 mm	2550 mm	2600 mm	2440 mm (8 blades)
—	2775 mm	2800 mm	2750 mm	2850 mm	2800 mm	2745 mm (9 blades)
—	—	3100 mm	3125 mm	3150 mm	3200 mm	3050 mm (10 blades)
—	—	3400 mm	3375 mm	3450 mm	3400 mm	3355 mm (11 blades)
—	—	3700 mm	3750 mm	3750 mm	3800 mm	3660 mm (12 blades)
—	—	—	4000 mm	4050 mm	4000 mm	3965 mm (13 blades)
—	—	—	4375 mm	4350 mm	4400 mm	4270 mm (14 blades)
—	—	—	4625 mm	4650 mm	4600 mm	4575 mm (15 blades)
—	—	—	—	4950 mm	5000 mm	4880 mm (16 blades)
—	—	—	—	5250 mm	5200 mm	5185 mm (17 blades)
—	—	—	—	5550 mm	5600 mm	5490 mm (18 blades)
—	—	—	—	—	5800 mm	5795 mm (19 blades)
—	—	—	—	—	6200 mm	6100 mm (20 blades)
—	—	—	—	—	6600 mm	6405 mm (21 blades)
—	—	—	—	—	6800 mm	6710 mm (22 blades)
—	—	—	—	—	7200 mm	7015 mm (23 blades)
—	—	—	—	—	7400 mm	7320 mm (24 blades)

4.12 Modifying Sign Margins

When using bolts installed through the sign face to mount sheet aluminium signs, the margins between the sign border and message content must be adjusted, if necessary, to ensure that the fastener head does not overlap the border or any of the sign message elements.

This issue is more prevalent on small signs that employ x-heights of 25 mm and less.

Be prepared to allow a margin of 20 mm to 25 mm depending on the size of the fastener head.

Figure 4.41 – Verify that fasteners do not interfere with message unit

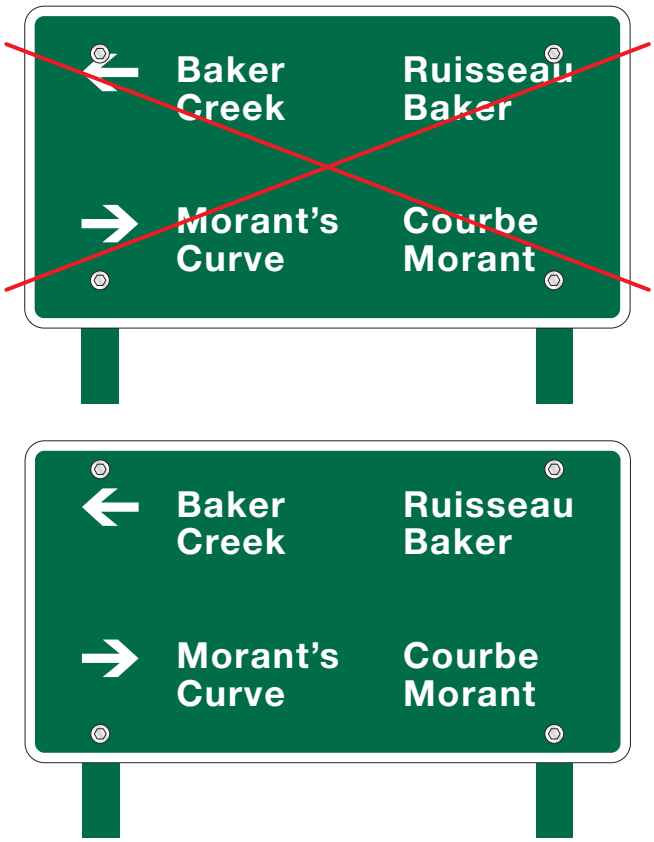


Figure 4.42 – Top and bottom margins are adjusted to eliminate fastener overlap with border and message.

4.13 Colour Specifications

4.13.1 Colour

Colour plays an important role in Parks Canada's Identity Program. The consistent application of the Agency's corporate colours helps the public recognize Parks Canada, its programs and service offer. The colour palette also serves to distinguish between sign types.

Parks Canada's palette for signage consists of eight colours. Heritage Green is the primary colour, used as the background for most signs. White and black are used primarily for text messages and symbol graphics.

The CSA colours: yellow, blue, green, and red along with black and white, are used to help distinguish the classifications of symbols and of text signs.

Appendix D – Sign Type Configurations and *Appendix E – Layout Specifications* provide detailed specifications on the application of colour by sign type.

4.13.2 Colour Palette

The following are the official Parks Canada colours for signage:

- Parks Canada Heritage Green
- White
- Black
- FIP Red
- CSA Yellow
- CSA Red
- CSA Blue
- CSA Green
- Boundary Marker Yellow

Figure 4.43 – Sign face colour, sign back colour and post finish colour

4.13 Colour Specifications (CONTINUED)

4.13.3 Colour Conversions

The following table provides Pantone® and process ink—cyan, magenta, yellow and black—equivalents for the colour palette.

Table 5.2 – Applied Graphics Sheeting Schedule in Section 5 provides a breakdown of sign sheeting material by colour. Heritage Green is not specified in this table as this colour will be applied using a powder coating system.

Table 4.4 – Colour specifications

	Pantone® (PMS)	CMYK
PC Heritage Green	PMS 553C	C = 60% M = 0% Y = 55% K = 80%
White	N/A	N/A
Black	PMS process black	C = 0 % M = 0% Y = 0% K = 100%
FIP Red	PMS 032C	C = 0% M = 100% Y = 100% K = 0%
CSA Yellow	PMS 109C	C = 0% M = 10% Y = 100% K = 0%
CSA Red	PMS 193C	C = 0% M = 100% Y = 65% K = 15%
CSA Blue	PMS 3015C	C = 100% M = 30% Y = 0% K = 20%
CSA Green	PMS 341C	C = 100% M = 0% Y = 70% K = 30%
Boundary Marker Yellow	PMS 012C	C = 0% M = 5% Y = 100% K = 0%

4.14 Creating Message Hierarchy

4.14.1 Message Hierarchy

Message hierarchy is a technique used to highlight or give emphasis to a sign element. A hierarchy is created by enlarging a key text element—"Notice", "Attention", "Danger"—and contrasting this with a secondary text message configured in a smaller character size.

In most situations a message hierarchy improves the legibility of a sign. When employed on pedestrian signs this technique will reduce the overall size of the sign.

If a hierarchy is required for a vehicular sign, it is critical that the secondary message be configured with a character height that is specified for the posted speed limit. The primary message will be enlarged to create the hierarchy, and will be legible in advance of the secondary message. This approach will ensure that the sign enjoys an appropriate viewing time and that all text is legible. An increase in sign size will result *(see Appendix F – Text Message Hierarchy).*

4.14.2 Text Sign with Message Hierarchy

The examples below compare signs with and without text hierarchy. The result is a more legible sign when hierarchy is applied. The larger title commands the reader's attention and introduces the core message which is formatted in a smaller size.

Figure 4.44 – Text sign with one character size, no hierarchy

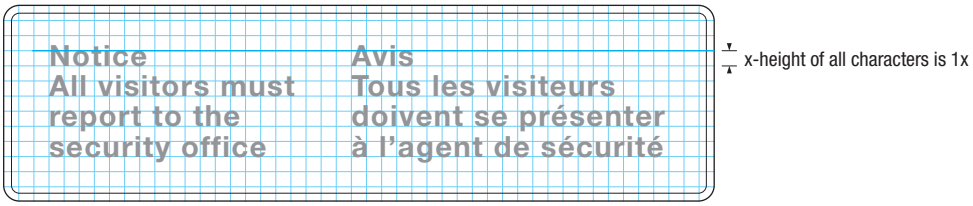


Figure 4.45 – Text sign with message hierarchy
Primary message formatted at 2x, contrasting the secondary message formatted at 1x.

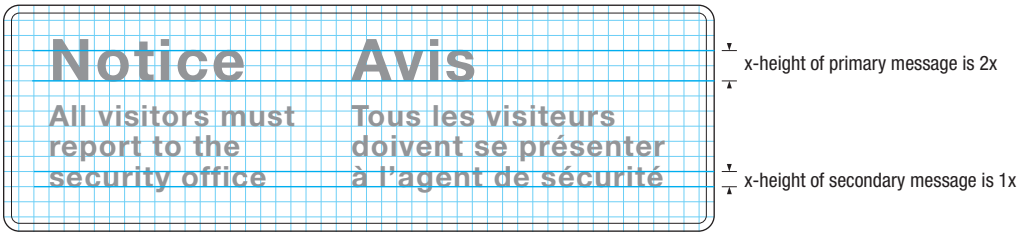
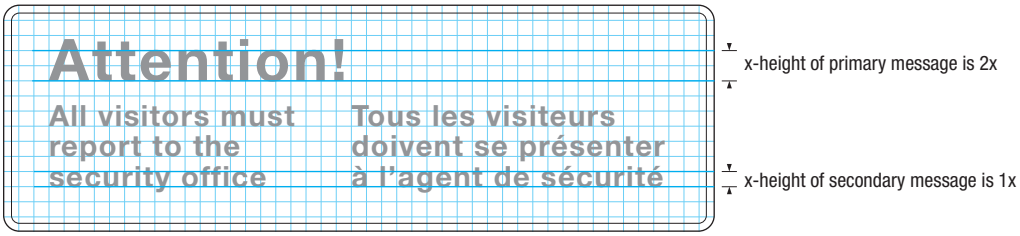


Figure 4.46 – Equivalent titles
In situations where the primary message is equivalent in both spelling and meaning in English and French, Attention or Danger for example, that term may be presented once, in a larger size, introducing the secondary message in both languages.



4.14 Creating Message Hierarchy (CONTINUED)

4.14.3 Message Hierarchy on Pedestrian Signs

Pedestrian signs are considered approachable media and do not have the viewing time restrictions associated with vehicular signs.

When creating a message hierarchy on a pedestrian sign, the option exists to reduce the size of the seconday message while retaining the primary message at 1x. The viewing distance associated with the smaller secondary message will be reduced, requiring the reader to approach the sign.

This strategy will reduce the overall size of the sign. Margins and border size continue to be calculated using the standard measure of "x".

Figure 4.47 – Pedestrian sign with standard hierarchy
Pedestrian sign hierarchy created by increasing the size of the primary message to 2x.

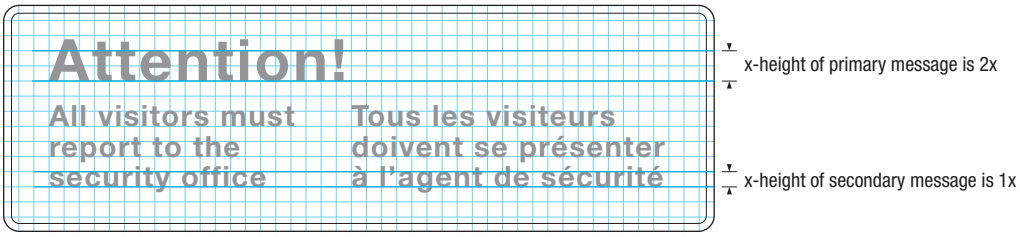
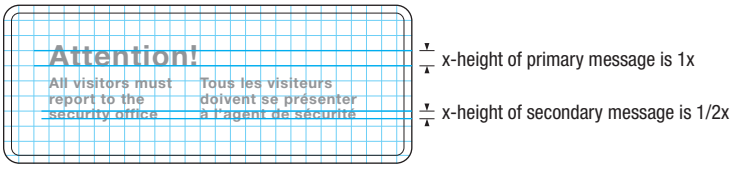


Figure 4.48 – Pedestrian sign with non-standard hierarchy
Pedestrian sign hierarchy created by decreasing the size of the secondary message to 1/2x. Primary message size remains at 1x. This sign is approximatly 50% smaller than the example above.



4.14.4 Symbol as Primary Message Element

Creating a message hierarchy can also be applied to symbol signs. Symbols may be used as the primary message with an arrow, text message or distance indicator as supporting secondary elements. When a symbol is used as a primary message element, a vertical sign format is applied; the symbol occupies the dominant position on the sign, appearing above the text or arrow. Bilingual text is stacked one language above the other. The language appearing on top is the language that would appear to the left in the side by-side-format.

4.14.5 Vertical Symbol Signs

Symbols, by their nature, vary significantly in form and legibility. Some—like the parking symbol—are simple, containing a single large typographic element. Others—No fishing, for example—are more complex, containing multiple illustrated elements with finer detail. Because symbols vary significantly, so do the viewing distances at which they are comprehensible.

The performance of all symbols will improve with increased size. The recommended size for a symbol used as the primary message element is 8x. The smallest character size of a typographic message remains 1x.

Figure 4.49 – Horizontal text and symbol sign with no hierarchy

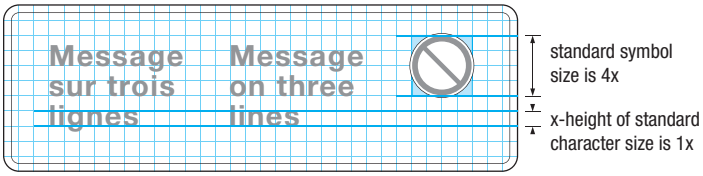


Figure 4.50 – Vertical symbol sign

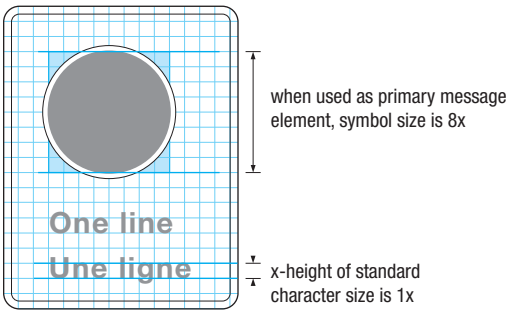


Figure 4.51 – Symbols vary in complexity and so do the distances at which they are comprehensible.

5

Material and Fabrication Specifications

Material and Fabrication Specifications

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5.1 Overview

These standards of fabrication seek to fulfill three guiding principles:

1. To provide consistent presentation of Parks Canada's identity.
2. To create a consistent and universal medium of messaging for an enhanced visitor experience.
3. To maintain a signage system that is sustainable and environmentally conscious.

These specifications will provide for a high-quality, long-lasting signage system. For example, the painted finishes—Parks Canada Heritage Green and black—are the new architectural grade fluoropolymer powder coatings that yield a 20+ year life with marginal degradation through weathering, UV, salt and abrasion. Additionally, Parks Canada's signage standards are based on environmentally conscious material and fabrication techniques, including:

Powder coating
- zero volatile organic compounds (VOCs) ,
- non chrome,
- UV, mar, abrasion and salt resistant,
- anti graffiti properties.

Powder coating pre-treatment
- zero VOCs,
- non chrome,
- salt resistant.

Aluminum
- high recycled content and recyclable.

Western red cedar
- naturally rot and rodent resistant,
- renewable, sustainable.

Stainless steel fasteners and anchors
- zero corrosion
- chrome free.

These specifications set out the requirements for the fabrication of all signs under Parks Canada's jurisdiction.

All exterior signs must conform to the following material, fabrication and supply specifications (excluding MUTCD signs, boundary markers and temporary signs). It is the responsibility of each individual park or site to ensure that sign manufacturers adhere to these standards of fabrication.

Sign manufacturers are responsible for securing the specified materials required for Parks Canada's signs.

Specification sections are provided for substrates, finishes, sign face materials and sign support structures. Unless specifically stated otherwise, the most current edition of the reference standard shall always be used.

Where no tolerances are specified, the standard of workmanship shall be in accordance with normally accepted standards of practice.

5.2.1 Sheet Aluminum Substrate

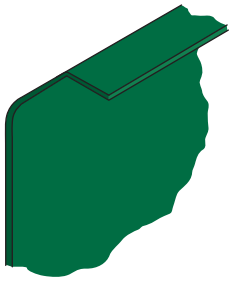
All sign faces less than 1800 mm wide are to be fabricated with sheet aluminum.

Sheet aluminum shall be flat-sheet tension-levelled, sign grade aluminum, alloy 5052-H32, conforming to the requirements of ASTM B209M, *Specification for Aluminum and Aluminum-Alloy Sheet and Plate*. Nominal thickness for sheet-faced signs is 3.0 mm (0.125").

Installation conditions will vary and consultation with the sign designer and/or an engineer will be required to determine the appropriate gauge for non-standard applications.

To ensure maximum tolerancing and best appearance, sheet aluminum sign panels shall be cut using a numerically controlled device such as water jet or laser cutting system. Alternatively, sheet aluminum substrates may be sheared to size, corner-punched and detailed, provided that the dimensions and corner radii exactly match the supplied sign artwork. All edges shall be broken, de-burred and made smooth.

Figure 5.1 – Sheet aluminum substrate



5.2.2 Sheet Aluminum Substrate, Reinforced

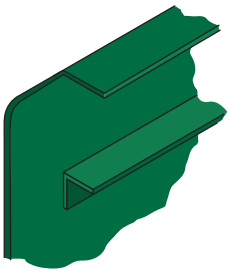
Post-mounted sheet aluminum signs 1800 mm–3000 mm wide must be reinforced from behind with aluminum angle bracing affixed to the back of the sign face. Refer to *Section 5.6 – Sign Bracing* for more information.

Sheet aluminum shall be flat-sheet tension-levelled, sign grade aluminum, alloy 5052-H32, conforming to the requirements of ASTM B209M, *Specification for Aluminum and Aluminum-Alloy Sheet and Plate*. Nominal thickness for reinforced sheet-faced signs is 3.0 mm (0.125").

Installation conditions will vary and consultation with the sign designer and/or an engineer will be required to determine the appropriate gauge for non-standard applications.

To ensure maximum tolerancing and best appearance, sheet aluminum sign panels shall be cut using a numerically controlled device such as water jet or laser cutting system. Alternatively, sheet aluminum substrates may be sheared to size, corner-punched and detailed, provided that the dimensions and corner radii exactly match the supplied sign artwork. All edges shall be broken, de-burred and made smooth.

Figure 5.2 – Sheet aluminum substrate, reinforced



5.2.3 Extruded Aluminum Substrate – Channel Shape, Highway Standard Blade System

Extrusion-based sign faces shall be constructed using 305 mm (12") standard highway extruded blades (shape # 73247) using aluminum alloy 6061-T6 or 6063-T5. Extruded blades are to be mill finished with no exposure to any silicone-borne products. Extrusion-based signs must be supplied with all mounting hardware as required for proper assembly and installation. For more information and typical detail, please refer to the following sources:

- Alberta T&U drawing TEB 1.95 (http://www.tu.gov.ab.ca/Content/doctype233/production/signage.pdf)
- British Columbia Standard Specification for Highway Construction, 2004 (http://www.th.gov.bc.ca/Publications/const_maint/contract_serv/standardspecs.htm)
- Ministère des Transports du Québec (http://www.publicationsduquebec.gouv.qc.ca/produits/ouvrage_routier.fr.html)

5.2.4 Extruded Aluminum Substrate – Rectangular Shape, FIP Standard

Extrusion-based sign faces shall be constructed to follow Canadian General Standards Board (CGSB) specifications. The aluminum must be alloy 6061-T6 or 6063-T5 and mill finished with no exposure to any silicone-borne products. Extrusion-based signs must be supplied with all mounting hardware including nuts, bolts, washers, clips, channel and posts as required for proper assembly and mounting. For more information and typical detail, please refer to the following source:

- *Federal Identity Program Manual – 4.2 Primary Identification Signs* http://www.tbs-sct.gc.ca/fip-pcim/man_4_2_e.asp#Top)

5.2.5 Tolerancing

The sign face blank shall be cut within 1.5± mm of the specified dimension (L x W) and flat within a maximum allowable deflection of .005 (d), where (d) is the maximum dimension of the sign blank in either dimension. The manufacturer shall provide signs conforming in quality and accuracy of detail to the dimensional and tolerance requirements of the specification.

Figure 5.3 – Extruded aluminum substrate – channel shape, highway standard blade system

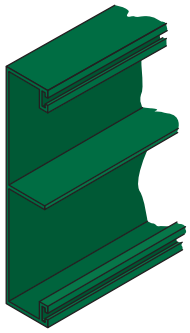
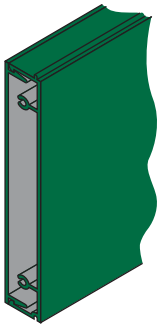


Figure 5.4 – Extruded aluminum substrate – rectangular shape, FIP standard

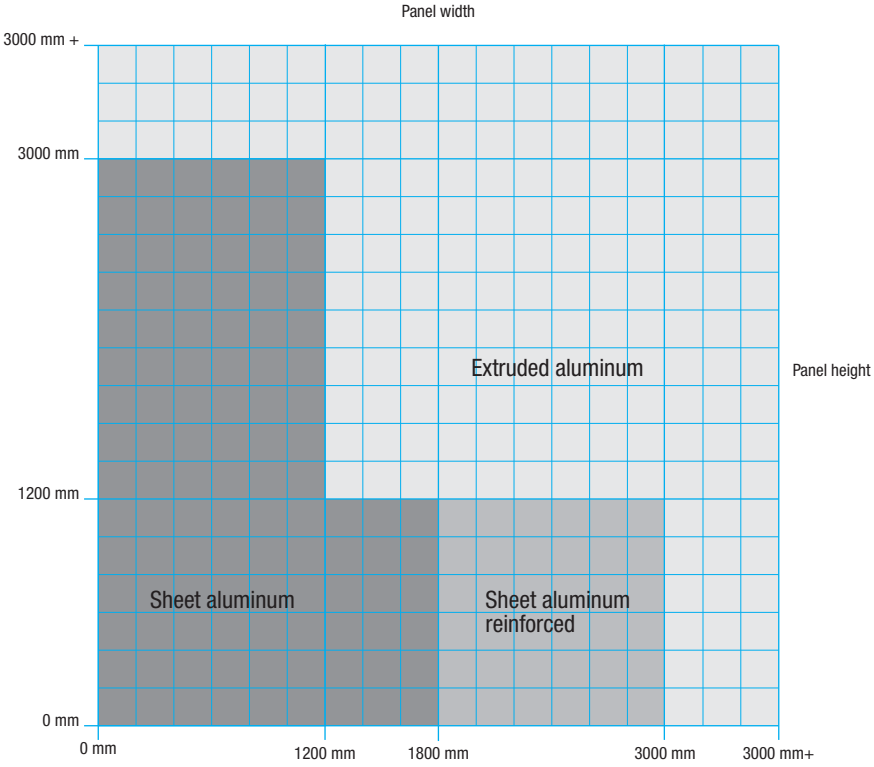


5.2 Sign Substrates (CONTINUED)

Table 5.1 – Size/substrate matrix – pedestrian and vehicular signs

	Sheet Aluminum 5052-H32	Sheet Aluminum 5052-H32, Reinforced	Extruded Aluminum 6061-T6 or 6063-T5
Under 1800 mm wide/tall	3 mm		
1800 mm to 3000 mm wide/tall		3 mm aluminium panel with bracing	
Over 3000 mm wide			Channel Shape, Highway Standard Blade System—Alcan Shape #73247 or Equal
Primary Identification Signs (PIDs) - Parks Canada Sign Type C (Federal Identity Program, FIP)			Rectangular Shape, FIP Blade System—Refer to Federal Identity Program Standards Manual

Figure 5.5 – Substrate material size matrix



5.3 Sign and Post Finishes

All surfaces and edges of all sign faces—sheet aluminum, sheet aluminum reinforced, extruded aluminum and related aluminum components—as well as aluminum posts are to receive a cleaning, pre-treatment (conversion coating) and a graphics-ready finish (powder coating) prior to the application of graphics/legends. It is the responsibility of the manufacturer to meet the following minimum standards for both the pre-treatment and powder finish. It is the responsibility of the manufacturer to ensure that the cleaning processes to be employed for the mill finish aluminum prior to pre-treatment are environmentally sound and:

- are VOC and chrome free;
- present no failure after 4000 hours of salt spray in accordance with ASTM B117;
- is 100% inter-compatible with aluminum substrate, pre-treatment and powder coat;
- is 100% compatible with powder coat and applied graphics materials;
- meet the requirements of the pre-treatment and powder-coating manufacturer.

5.3.1 Preparations and Cleaning

All sign faces—sheet aluminum, sheet aluminum reinforced, extruded aluminum and related aluminum components—and aluminum posts must be:

- predrilled with suspension holes at the edge in the required locations (number of holes dependent on sign face size), to facilitate suspension of the components for cleaning, pre-treatment and powder coating process. The manufacturer is to ensure that the locations of all holes are discrete and captured within the applied graphic border of sheet-faced signs, tab of sheet face and/or the web of extrusion based signs. The manufacturer is to coordinate size and location requirements with the powder-coater.
- finished to ensure all exposed edges and corners are de-burred and made smooth.
- cleaned prior to pre-treatment to ensure that the aluminum is degreased, etched and made free of white rust with a multi-stage cleaning system that is environmentally friendly, compatible and approved by the pre-treatment manufacturer.
  The cleaning processes are to be chrome and phosphate free.

5.3.2 Pre-treatment

All sign faces—sheet aluminum, sheet aluminum reinforced, extruded aluminum and related aluminum components—and aluminum posts must be pre-treated according to the pre-treatment manufacturer's specifications. The pre-treatment is to be a non-chrome conversion coating that:

- meets or exceeds AAMA 2605-05 (American Architectural Manufacturers Association).

Product example: E-Clps by Bulk Chemical.

5.3.3 Powder Coating

Pre-treated sign faces—sheet aluminum, sheet aluminum reinforced, extruded aluminum and related aluminum components—and posts must be powder-coated according to the powder coating manufacturer's specifications and the powder coating is to be applied through electrostatic application. Finished powder coating is to provide 100% coverage with a film thickness of 3 to 3.5 mil with 30 degrees of gloss and be free of mechanical defects. The powder coating is to be a high performance architectural grade fluoropolymer based powder system that:

- meets or exceeds AAMA 2605-02;
- has a minimum warranty of 10 years (10 Year Florida);
- has a maximum change of 5 Hunter units of Colour Integrity ASTM D2244;
- has a minimum Gloss Retention of 50% ASTM D523.

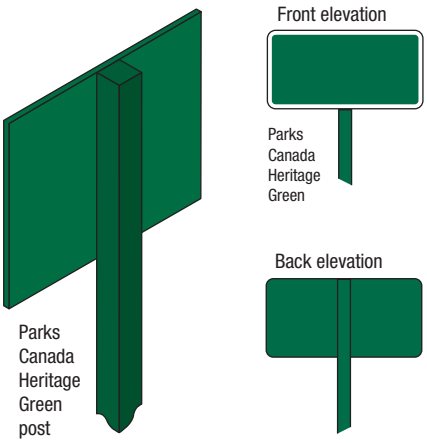
Product examples: Protech Solar, Tiger Dry lac 75 Series, Akzo Nobel Fluoromax 3000

All sign types, all surfaces and edges of all sign faces—sheet aluminum, sheet aluminum reinforced, extruded aluminum (including sign types C1/C2 PID)— shall have a final finish to match Parks Canada Heritage Green, Pantone® 553C, with a gloss level of 30 degrees.

Most aluminum posts will be powder-coated to match the Parks Canada Heritage Green. The following sign types, however, require black powder-coated posts following the same requirements for preparation, pre-treatment and powder-coating as specified above:

- H1 (Emergency)
- N1 (Prohibition)
- P1 (Mandatory)
- S1 (Boundary Marker)
- T1 (Caution)
- U1 (Danger)

Figure 5.6 – Standard colour powder-coat finish (sign and post)



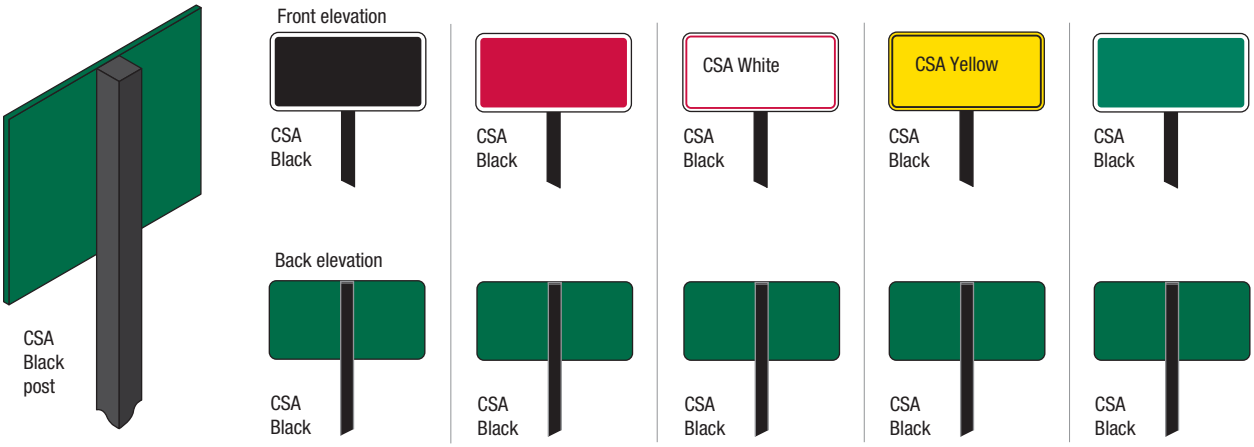
To ensure quality and consistency in production, the sign manufacturer is to:

- ensure that all powder and pre-treatment manufacturers' recommendations and guidelines for use and preparation are followed;
- document the complete finishing process, from mill finish to graphic-ready, and obtain written approval, with warranties, from both the pre-treatment and powder system vendors and applicators, plus the applied graphics manufacturer, for review and approval by Parks Canada prior to order/purchase/application of pre-treatment and powder systems;
- provide technical performance details and product information for each step;
- provide finish samples – i.e. powder coating, 500 x 300 x 3 mm aluminum, all colours;
- provide a flow chart of the entire finishing and applied graphics process dated and signed by respective manufacturers for compatibility and confirmation of warranties;
- provide sign manufacturers' warranty—refer to *Section – 5.11 Warranty;*
- provide control charts, samples and quality control reports at intervals throughout the finishing program/run.

5.3.4 Powder coating Touch-up Kits

The manufacturer is to provide the park/site with a sufficient quantity of paint touch-up kits appropriate for field application (proportional to program size of the park or historic site) of the Parks Canada Heritage Green and Black (only if black posts are part of program) powder paints. The touch-up kits with detailed, illustrative instructions are to be approved by the powder manufacturer for use with the powder paint system. The touch-up kits are to include a corresponding supply of all necessary preparation materials or cleaning wipes packaged for field use. The kits are to be packaged in an appropriately sized durable waterproof pack or pouch. A single touch-up pen or delivery device and respective wipes is considered a kit. Each pen or delivery device is to be clearly labelled with an expiration date that reflects the expected shelf life under primary and normal conditions of heated indoor storage. The manufacturer will issue replacement kits to the park or site on these anniversary dates for a period of 10 years from first issue.

Figure 5.7 – Exceptions; non-standard colour powder-coat finish (sign and post)



5.4 Sign Sheeting – Applied Graphics

Sign sheeting material (applied graphics) shall be correctly applied in accordance with the sheeting manufacturer's recommendations and industry-accepted quality practices. The sheeting material must be applied so that it does not contain air pockets or trapped debris. The sheeting shall not have holes, tears, scrapes, compressed cells or patches. Any joints must be sealed in accordance with the sheeting manufacturer's recommendations. Processes and materials shall be recommended by the sheeting manufacturer, shall be cross-compatible and shall include, but are not limited to, protective clear liquids, over-laminates and electronically cut imaging films.

5.4.1 Retroreflective Sheeting

The specifications for colours of all retroreflective sign sheeting are established in ASTM D4956 *Standard Specification for Retroreflective Sheeting for Traffic Control*. Where required, all retroreflective sheeting used on Parks Canada roadway signs must conform to this specification.

To ensure maximum tolerancing and best appearance, all adhesive sheeting shall be cut using a computer-controlled cutting system.

Text message, arrow, border and symbol backgrounds will be white retroreflective Type II or Type IX sheeting as defined by the applied graphics sheeting schedule *(Table 5.2)*.

5.4.2 Transparent and Opaque Films

For signs or symbols using CSA colours other than yellow, the method of lay-up is to apply transparent film of the required colour over retroreflective sheeting (Type II or Type IX).

For signs or symbols requiring the use of CSA yellow, black opaque film is to be applied over the CSA yellow retroreflective sheeting (Type II or Type IX).

Notes:

1. Double warranty numbers imply that the first (lower) number is for materials and labour and the second (higher) number is for materials only. Refer to sheeting manufacturers' written warranties for details of coverage for material and labour.
2. Overlay assumes warranty of base product, i.e. transparent acrylic overlay applied to T-9500 yields a 12-year warranty; transparent overlay applied to T-2500 yields a 10-year warranty.
3. Products listed in Table 5.2 are examples of products meeting or exceeding Parks Canada's standards. Equivalent products may be used provided they meet or exceed Parks Canada's standards for exterior signage. Written confirmation from the sheeting manufacturer is required prior to fabrication and must stipulate that the equivalent product meets or exceeds Parks Canada's requirements for exterior signage use.

Table 5.2 – Applied graphics sheeting schedule

Generic name	ASTM reference	Colour	Minimum required warranty (years), Note 3	Product example
Retroreflective sheeting – roadway speed under 50 km/h				
Enclosed lens	Type II	White	7 and 10 years, Note 1	Avery T-2500
		CSA Yellow	7 and 10 years, Note 1	Avery T-2501
Retroreflective sheeting – roadway speed 50 km/h and greater				
Prismatic lens	Type IX	White	7 and 12 years, Note 1	Avery T-9500
		CSA Yellow	7 and 12 years, Note 1	Avery T-9501
Transparent film – CSA				
Apply over retroreflective sheeting		CSA Red	10 years, Note 2	Avery OL-2002; 3M Electrocut 1172
Apply over retroreflective sheeting		CSA Blue	10 years, Note 2	Avery OL-2005; 3M Electrocut 1175
Apply over retroreflective sheeting		CSA Green	10 years, Note 2	Avery OL-2007; 3M Electrocut 1177
Opaque sheeting (applicable sign type in parentheses)				
Apply over retroreflective sheeting (B1–B4)		Black	9 years, Note 2	Avery A-9090-0; 3M Electrocut 1178
Apply over opaque sheeting (C1–C2)		Black	9 years, Note 2	Avery A-9090-0; 3M Electrocut 7725
Apply over retroreflective or opaque sheeting (B1–B4)		FIP Red	9 years, Note 2	Avery A-9325-0; 3M 7725-13
Apply over powder coated substrate (C1–C2)		White	9 years	Avery A-9001-0; 3M 180-10
Protective over-laminate				
Anti graffiti		Clear	No warranty	Avery OL-1000; 3M 1160 Over-laminate

5.4.3 Protective Over-laminates

The park/site may elect to apply a transparent protective over-laminate, anti graffiti, on sign faces in areas where signs are exposed to a high risk of vandalism or interaction with wildlife. The over-laminate must be 100% compatible with all applied graphics and base finishes.

5.4.4 Surface Preparation

Substrate must be prepared according to the applied graphics manufacturer's specifications.

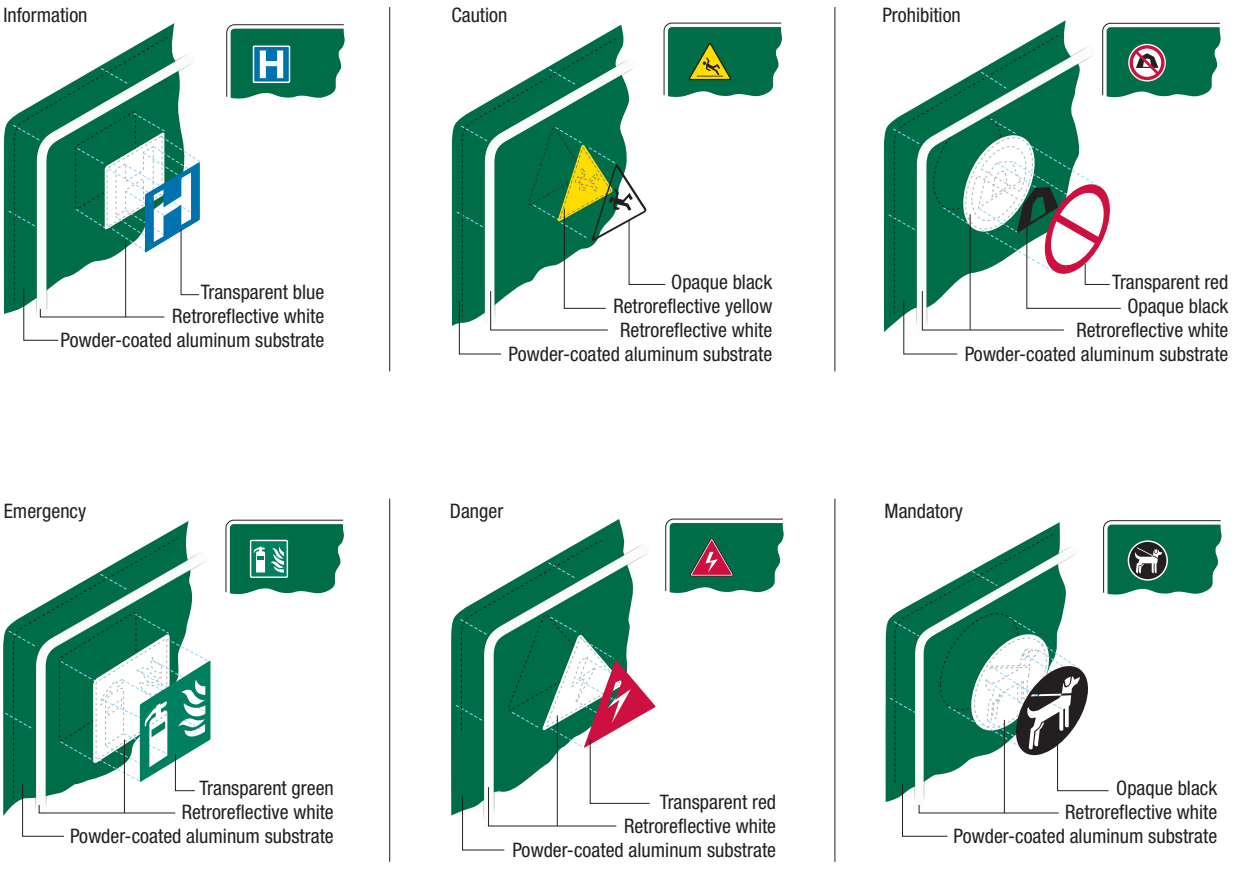
5.4.5 Certified Manufacturer

Sign fabricators must be approved as certified manufacturers by the sheeting manufacturer pursuant to the following terms:

- Sign fabricators shall undergo regular certification by the sheeting manufacturer.
- Certification shall demonstrate that the manufacturer possesses the expertise, equipment, processes and materials to produce a high-quality sign meeting or exceeding client specifications and expectations.

Additionally, the certified manufacturer shall warrant the materials and workmanship for all signs produced, as per *Section 5.11 – Warranty*.

Figure 5.8 – Exploded isometric and front elevation views of symbols on substrate.



5.4 Sign Sheeting – Applied Graphics (CONTINUED)

Figure 5.9 – Exploded isometric and front elevation views of text on substrate.

5.5 Sign Identification

All signs are to be clearly labelled with a meaningful identification (ID) code relating to an asset management database. The coding should appear on the back corner of the sign closest to the roadway/pathway. The base material of the label is to be digitally printed with a sheeting manufacturer-approved system. The text of the ID code is to be Helvetica Neue Condensed Light with an "x" dimension of 5 mm (14 points) in black. The manufacturer will submit samples for review and approval prior to proceeding.

In addition to the labelling, the manufacturer will compose a database containing all points of information and will submit digital copies to the park or site. The manufacturer will include the following points and any additional information that may be required, e.g. asset management code. The manufacturer is to coordinate the following with the park/site/marine conservation area:

- sign number, corresponding to Parks Canada sign and site plans,
- manufacturer's name,
- month and year of manufacture,
- month and year of installation.

The following points are to be included in the corresponding database:

- Coded brand and grade of powder coating (include batch number, if applicable).
- Coded brand and grade of primary retroreflective legend/border sheeting (include batch/run number, if applicable).
- Coded brand and grade of secondary translucent symbol film (include batch/run number, if applicable).

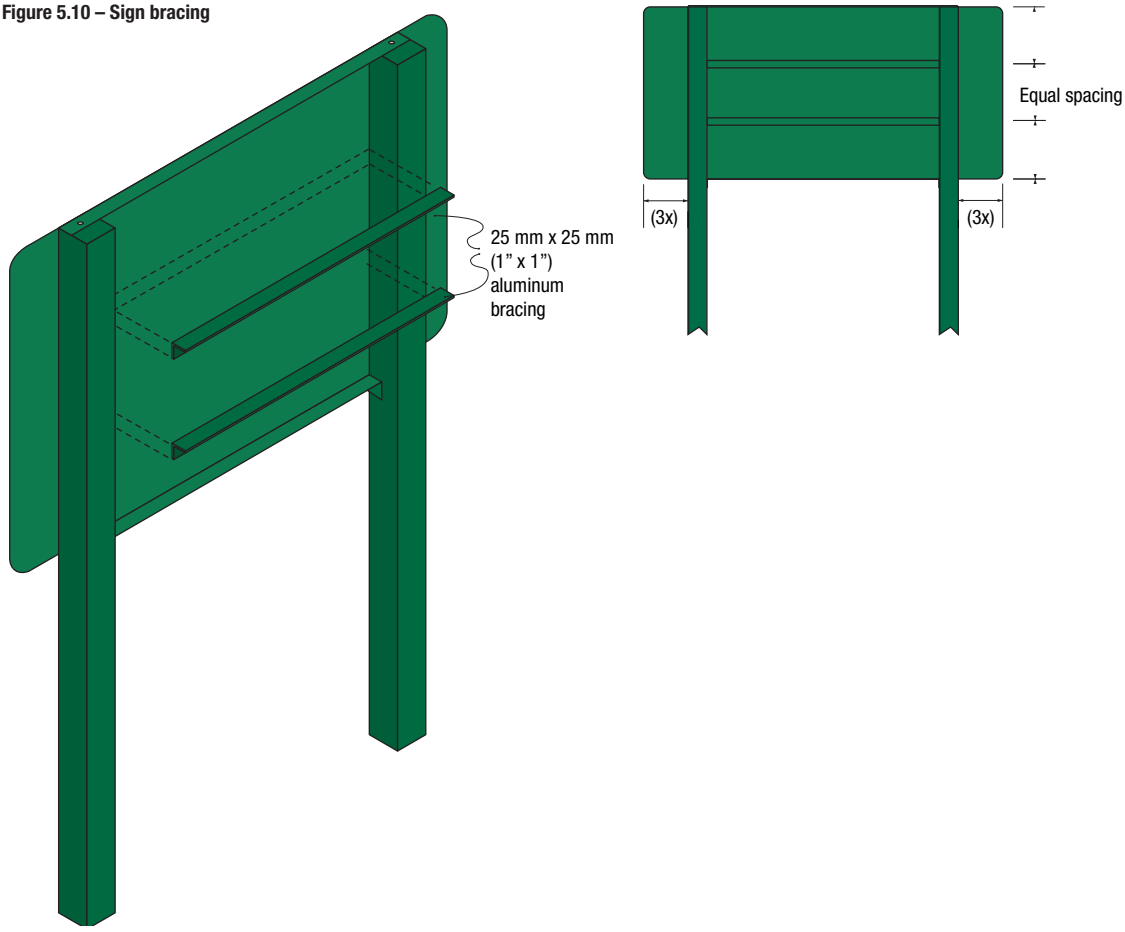
5.6 Sign Bracing

Sheet aluminum reinforced signs will be braced to eliminate flex and distortion due to sign size and local environmental conditions. In general, all post-mounted sheet aluminum signs 1800 mm to 3000 mm wide shall be reinforced from behind using 25 mm x 25 mm (1.0" x 1.0") structural aluminum angles with a minimum leg thickness of 3 mm (0.125”). Bracing shall run the width between the posts and be aligned parallel with the top and bottom edges of the sign face at equal increments. The sign bracing must be powder-coated, as outlined in *Section 5.3 – Sign and Post Finishes,* prior to attachment.

The bracing assemblies are to be affixed to the back of the sign face with a double-faced very high-bond tape system. The tape system is to be specifically designed for exterior use for the permanent adhesion of powder-coated surfaces (low-energy surfaces). The sign fabricator must meet or exceed the tape manufacturer's recommendations for preparation prior to application. In addition, parks and sites may elect to have the bracing spot welded for increased rigidity.

High-bond tape system product example: 3M 5952, 5962

Figure 5.10 – Sign bracing



5.7 Sign Supports

The sign supports define the overall aesthetic character of the sign, given that all sign faces for all mounting types are prescribed by a set standard. The mounting types are composed of four styles: post, surface, bracket and freestanding. Posts are defined by three styles or aesthetics: colour-finished metal, unfinished wood, or treated metal. The colour-finished metal posts will be either Parks Canada Heritage Green or black, depending on sign type. Wood posts, typically western red cedar, are to remain unfinished, and will acquire a natural patina of silver. Treated metal posts are either anodized clear structural aluminum shapes or galvanized steel for the channel shape extrusion only (PID signs, C1/C2, are to be powder-coated).

The system of sign supports is defined by seven types:

1. Aluminum posts, colour finished, square profile, for pedestrian signs (historic sites include vehicular and MUTCD signage, i.e. stop, yield, speed, etc.).
2. Wood posts, unfinished square profile, for vehicular signs, including MUTCD signage, i.e. stop, yield, speed, etc.
3. Structural aluminum or steel posts, C, T and I shapes, for large vehicular signs, only if deemed necessary by a professional engineer.
4. Delineator, for campsites with integrated sign face and post, integral colour finish, round profile and flexible to vehicle impact.
5. Surface-mounted, sign face parallel with mounting surface.
6. Bracket-mounted, sign face perpendicular with mounting surface.
7. Freestanding, day-use signage that is moveable.

Where deemed aesthetically appropriate, a park or site may elect to install unfinished wood posts in lieu of powder-coated square-profile aluminum posts for pedestrian signs, yielding a system of predominantly wood posts.

It is the responsibility of each park/site/marine conservation area to ensure that off-site sign structures conform to local and provincial roadway regulations and that permission from the appropriate authorities has been secured prior to fabrication and installation.

The park/site is to ensure that breakaway posts are utilized for each installation of roadway signage as required by the provincial authority and where the park/site's experience deems necessary.

All Parks Canada signs—generally roadway—larger than 4.6 square metres (50 sq ft), including footings, breakaways, posts and hardware systems as required, are to be designed and approved by a professional structural engineer.

See Section 6.0 – Installation and Maintenance for additional details and structural engineering guidelines.

5.7.1 Aluminum Posts

Aluminum posts are to be specified for all pedestrian signs up to 3000 mm wide. Posts are square-profile with radiused corners and are to be highly finished architectural quality.

Type and size

Aluminum square tube posts 65 mm x 65 mm (2.5" x 2.5"), extruded structural grade 6061-T6, wall thickness 0.125", corners radiused 0.125".

Post lengths are to be cut to the nearest standard increment of 100 mm, e.g. 2700, 2800, 2900.

Finish

Aluminum posts are to be finished as per *Section 5.3 – Sign and Post Finishes.*

Cap

The top of every post shall be sealed, with a 6 mm thick aluminum plate welded in place. The cap will cover the end of the post to prevent water penetration. The capped end is to be sanded or ground to a flat, seamless finish.

Note – Post protection at grade

For sign installations that are located in manicured areas where the portion of the post at grade may come in contact with maintenance equipment, e.g. grass trimmers or shovelling, it is recommended that the post be fitted with a protective sleeve with integral colour (Parks Canada Heritage Green or black to match metal post colour) for the zone of the post 100 mm above and 100 mm below grade. The protective sleeve is to be of a tight and clean fit to the post.

Figure 5.11 – Aluminum post types

65 mm x 65 mm (2.5" x 2.5")
Structural grade aluminum post
alloy 6061-T6, wall thickness 0.120"
powder-coated Parks Canada
Heritage Green or black – depending
on sign type

5.7.2 Natural Wood Posts

Natural wood posts are generally specified for all vehicular-oriented signs. Poles are square profile with radiused corners and s4s (surfaced 4 sides).

Type and size

- 100 x 100 mm (4" x 4"), 100 x 150 mm (4" x 6"), 150 x 150 mm (6" x 6") or 200 x 200 mm (8" x 8") western red cedar, red pine, Douglas fir, Jack pine, eastern spruce or eastern white cedar.

Post lengths are to be cut to the nearest standard increment of 100 mm, e.g. 2700, 2800, 2900.

- All wood should be well-seasoned and free from defects that will impair strength and durability.
- Posts shall be No. 1 structural grade, s4s (surfaced 4 sides).
- Grading sizes shall be in accordance with CSA standard 0141-1970 (softwood lumber) or latest edition.
- Tops of all posts shall be cut square.

Finish

To ensure longevity of the wood posts, the in-ground portion, which is the most prone to decay, may be encapsulated with an epoxy system formulated for wood products that come into direct contact with water. Wood posts may have the end and sides of the in-ground portion plus 100 mm above grade finished with two coats of a two-part resin, e.g. West System Epoxy 105 resin with the 205 or 206 hardeners, or equivalent. The final mil thickness of the epoxy for the four sides is to be 4 mil and the end face 6 mil/liberal. The coating is to have a minimum of 48 hours of air cure time prior to embedment in the ground.

Cap

The top of all wood posts shall be capped to prevent the absorption of water at the end grain. The cap is to be 3 mm stainless steel affixed in place with a full surface coat of silicone adhesive. The cap is to be inset 3 mm from the edge of all post faces.

Note – Post protection at grade

For sign installations in manicured areas where the portion of the post at grade may come in contact with maintenance equipment, e.g. grass trimmers or shovelling, it is recommended that the post be fitted with a protective sleeve with integral colour (grey for wood posts) for the zone of the post 100 mm above and 100 mm below grade. The protective sleeve is to be of a tight and clean fit to the post.

Figure 5.12 – Wood post types

100 mm x 100 mm (4" x 4")
Natural wood post, No. 1 structural grade S4S red pine, Douglas fir, Jack pine, eastern spruce, eastern or western cedar

100 mm x 150 mm (4" x 6")
Natural wood post, No. 1 structural grade S4S red pine, Douglas fir, Jack pine, eastern spruce, eastern or western cedar

150 mm x 150 mm (6" x 6")
Natural wood post, No. 1 structural grade S4S red pine, Douglas fir, Jack pine, eastern spruce, eastern or western cedar

5.7.3 Structural Aluminum or Steel Posts

Roadway signs may require the use of structural aluminum or steel, C-, T- or I-shape beams that integrate with the standard highway extrusion-based sign face system only if natural wood posts do not meet requirements. If non-wood posts are deemed necessary, anodized structural aluminum posts are the first choice. Given that galvanizing is chrome-based, galvanized structural steel is to be used only if structural aluminum does not meet requirements. The installation site(s) are to be reviewed by a professional engineer for assessment of soil and other conditions. Each sign will require an engineer-approved specification and design. Posts are to be modified where required to incorporate a breakaway design.

Type and size

- Structural aluminum posts, where required, shall be extruded structural grade 6061-T6.
- Steel posts, where required, shall be in accordance with CSA G40.12 (channel steel) and CSA G164 (galvanized steel) or the latest edition of these standards.

Finish

- Structural aluminum posts are to be clear anodized as per ASTM standards.
- Steel components are to be galvanized in accordance with ASTM A-653 DES. G-90.
- Primary Identification signs (PIDS), Sign Type C (FIP standard), posts and base plates are to be fabricated as per FIP standards and powder-coated as per *Section 5.3 – Sign and Post Finishes*. Preparations, cleaning and pre-treatment are to be steel specific and non chrome. The preparations and pre-treatment described in Section 5.3 are specific to aluminum.

Figure 5.13 – Structural aluminum or steel sign post

Engineer-specified structural steel I-beam (dimensions will vary)

5.7.4 Delineators – Campsite Markers

Parks may elect to specify flexible delineators for campsite markers. The delineators are to have an integrated sign face and are to be or equal to Safe-Hit delineators.

Type and size

Delineator is to:

- be constructed of flexible polyethylene plastic that is resistant to ultraviolet light, ozone, and hydrocarbons.
- be cast-in colour.
- have a reactive spring assembly tested to 145 lb tension with stainless steel cable.
- meet MUTCD specifications.
- have posts manufactured to length to achieve an above-grade elevation to the top cap of 1000 mm (to be confirmed by park).
- have integrated sign face with a minimum graphic area of 80 mm x 300 mm.

Delineators are to be provided complete with drive-in anchor system constructed of stainless steel.

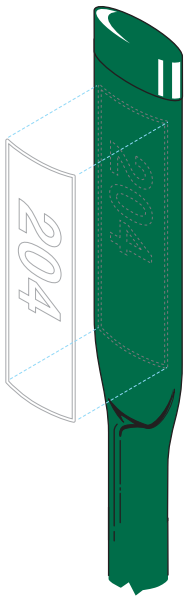
Finish

Delineators must match Parks Canada Heritage Green, Pantone 553C.

Cap

The top of every post shall be sealed and the end will be covered with a cap to prevent water penetration.

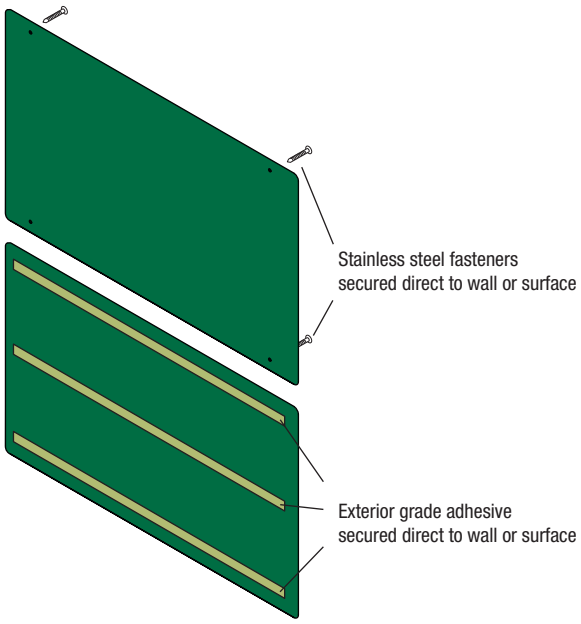
Figure 5.14 – Exploded view of graphic application for campsite delineator



5.7.5 Surface-mounted Signage

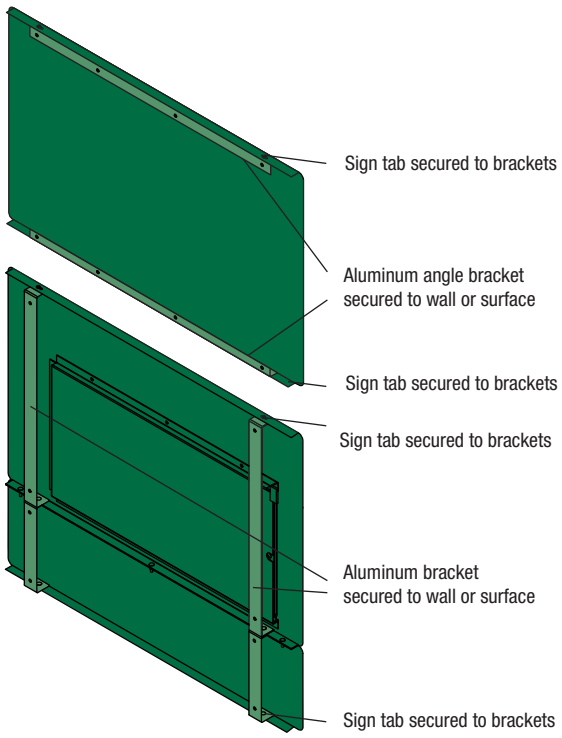
Direct-mounted signage—sign face parallel with mounting surface.

Figure 5.15 – Rear views of direct-mounted signs



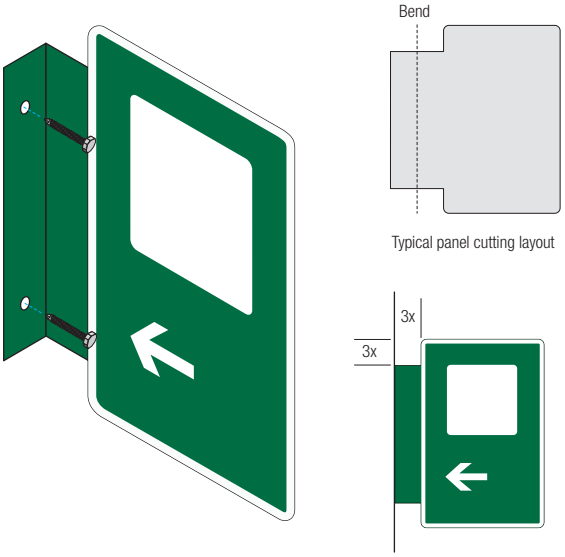
Rail-mounted signage—sign face parallel with mounting surface.

Figure 5.16 – Rear views of rail-mounted signs



Bracket-mounted signage—sign face perpendicular with mounting surface.

Figure 5.17 – Bracket-mounted signage



5.7.6 Freestanding Signage

Freestanding signage—day-use signage that is moveable.

Figure 5.18 – Freestanding signage

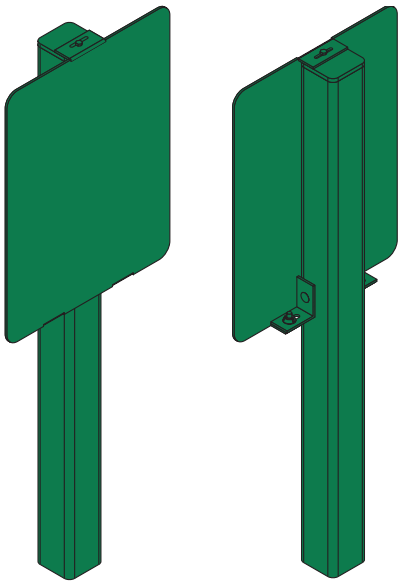
5.8 Sign Face Attachment and Hardware

5.8.1 Hidden Fastening System

To provide for a high aesthetic finish for Parks Canada signs, a hidden fastening system has been developed for both aluminum and wood supports. It is recommended that all pedestrian signs employ the hidden fastening system. Refer to *Appendix G – Technical Drawings* for more information on the hidden fastening system

All fasteners must be tamper resistant with the exception of anchor bolts at footings. All sign fasteners are to be stainless steel. Other hardware systems may be specified if product performance and characteristics are equal or better, but are subject to approval prior to implementation.

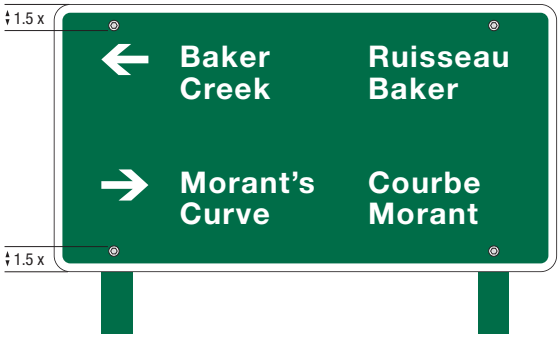
Figure 5.19 – Front and back views of hidden fastener system on single post signs



5.8.2 Standard Fastening

In areas where the hidden fastening system may not be appropriate, standard through-bolts and lag screws may be used provided they are tamper resistant, suitable for exterior signage use and provide a balanced aesthetic with the sign. When using the standard fastening approach, all sign panels must be pre-drilled prior to powder coating. Holes drilled after powder coating may diminish the performance lifespan of the sign and may void warranties.

Figure 5.20 – Standard fastener system as it appears on sign face



5.8.3 Hardware Corrosion Resistance

All bolts, screws, nuts and washers shall be stainless steel unless otherwise specified by a structural engineer or FIP standards for PIDS. When the fastener and post/bracket metals are dissimilar they must be prepared accordingly to prevent a corrosive interaction of dissimilar metals, for example using Nyltite® sleeves, nylon washers.

5.8.4 Finish

Exposed sharp edges and corners of all materials (bolts, etc.) shall be de-burred and made smooth.

Protruding bolts can be a safety hazard. Any bolt projecting more than 6 mm (1/4") beyond the fastened nut must be cut down and ground smooth.

5.9 Packing and Shipping

The sign manufacturer must ensure all signs are adequately protected from damage during shipping. Prior to delivery, all signs must be protected using appropriate wrapping and/or packing material. Any signs damaged (including scratches and dents) during fabrication or shipping shall be replaced or suitably repaired at the manufacturer's expense.

All signs, hardware and posts must be clearly and individually labelled with the correct sign number corresponding to the site sign plan. Each shipment shall include a complete packing list of all contents, sorted by sign number.

5.10 References

- ASTM B221M, *Specification for Aluminum and Aluminum-Alloy Extruded Bars, Rods, Wire, Shapes, and Tubes*
- ASTM B209M, *Specification for Aluminum and Aluminum-Alloy Sheet and Plate*
- ASTM D4956, *Standard Specification for Retroreflective Sheeting for Traffic Control*
- ASTM A153, *Standard Specification for Zinc Coating (Hot-Dip) on Iron and Steel Hardware*
- ASTM A653, *Standard Specification for Steel Sheet, Zinc-Coated (Galvanized) or Zinc-Iron Alloy-Coated (Galvannealed) by the Hot-Dip Process*
- CSA G40.12, *Channel Steel*
- CSA G164, *Galvanized Steel*

5.11 Warranty

It is important that parks and sites negotiate an acceptable warranty for all signs directly from the sign fabricator. This warranty is separate from the warranties provided by the powder coating supplier or sheeting manufacturer. It is recommended that sign manufacturers provide a full replacement warranty for each sign covering all defects in material and labour for the fabrication, for a minimum of 2 years from the date of final inspection and sign-off. The failure of sign sheeting materials, finishes, or structural integrity as a result of poor workmanship shall result in the sign being replaced or refurbished at the manufacturer's expense for the duration of this period. If the sign manufacturer is also engaged to perform installations, these warranties will be extended to include the installation.

In addition, the sign manufacturer should carry all responsibility for all material warranties, i.e. powder coating and pre-treatment and applied graphics for the duration of the finish/material manufacturer's warranty.

5.12 Pre-fabrication sample set and documentation checklist

It is highly recommended that for parks or sites with sizeable quantities of signage cued for fabrication a sample product set be produced by the manufacturer for review and approval prior to proceeding with production. This requirement is generally invoked only for unknown signage manufacturers. Unknown manufactures are those with no current, recognized, successful experience in fabricating Parks Canada signage using these specifications.

The sign manufacturer must ensure the provision of the following documentation prior to initiating the fabrication program:

- Manufacturing process, i.e. process of pre-treatment — to ensure sustainable environmentally friendly measures are implemented
- Material specification, e.g. sheet aluminum, 6061-T6
- Material and process purchase orders and/or packing slips, i.e. sheet aluminum, pre-treatment by out-sourced vendor
- Sheeting manufacturer certifications
- Warranties

6

Installation and Maintenance

Installation and Maintenance

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6.0 Installation and Maintenance

Consistent and uniform installation creates a coherent and appealing signage system that is inviting and accessible to visitors.

This section provides general installation guidelines that will result in the consistent and uniform installation of Parks Canada signs across the system. It is recognized that operational and installation requirements will vary from location to location and that these guidelines may not address all installation situations. As the installation and placement of signs become architectural features within a landscape and contribute to the positive or negative aesthetic appeal of an area, care should be taken to ensure a consistent approach to all sign installations.

This section will address the following elements:

1. General requirements
2. Installation methods
3. Installation height and setback
4. Guidelines for large structural supports
5. Maintenance

6.1 General Requirements

It is important to note that installation requirements have an impact on fabrication. It is therefore important to ensure that all information related to installation is properly and accurately identified during the sign planning phase.

Prior to fabrication, the following actions should be taken:

- Review all installation information—installation depth, height, offset, etc.— to ensure that it is accurate and address any issues.
- Make necessary adjustments regarding natural features, obstacles and terrain conditions. Major changes to sign locations should be identified and updated in the approved sign plan.
- Ensure that the installation type and approach is appropriate for the local conditions.
- Confirm and finalize all installation requirements as defined by a professional engineer.
- Ensure that sign locations do not negatively impact the natural and/or cultural environment and do not detract from scenic viewscapes.
- Ensure that, where required, an environmental assessment is completed (major modifications to sign placement may impact the installation type, height and depth).
- Where required, ensure that underground utilities are marked, e.g. where the possible presence of an underground utility would significantly affect the placement and messaging of a sign.

Prior to installation, the following actions should be taken:

- Ensure that sign locations are accurately identified (staked) according to the approved sign plan.
- Ensure that underground utilities are marked.

Following installation, the following actions should be taken:

- Ensure that the installation area is cleaned up and surplus material removed and properly disposed of (recycled where appropriate).
- Ensure that the landscaped area around the sign is restored or re-naturalized.
- Ensure that all bolts are cut as required and sharp edges sanded down.

6.2 Installation Methods

The Parks Canada signage system is designed to accommodate three principal installation methods. They are:

1. Post mount
2. Surface mount
3. Portable

Within each of these methods, there are a variety of different techniques that can be employed depending on the local installation conditions. It is important that both the installation method and technique be specified during the sign planning phase; however, on-site adjustments may be required.

6.2.1 Post Mount

The vast majority of signs will be installed using a post-mount installation system (aluminum, wood, structural steel, flexible campsite markers). To accommodate the various installation conditions throughout the Parks Canada system, guidelines have been developed for the following post installation techniques:

1. Direct embedment
2. Embedded anchor system
3. Foundation
4. Pad mount

It is recognized that the above installation techniques may not address every installation condition and that site-specific solutions may be required. In such cases, it is important to address these issues during the sign planning phase in order to determine the most appropriate alternative.

The installation of sign posts is generally driven by soil conditions, equipment requirements, sign size and location. It is important to note that these guidelines do NOT apply to large signs that require engineer specifications or roadway signs that require breakaway designs according to the Manual of Uniform Traffic Control Devices or provincial traffic control standards.

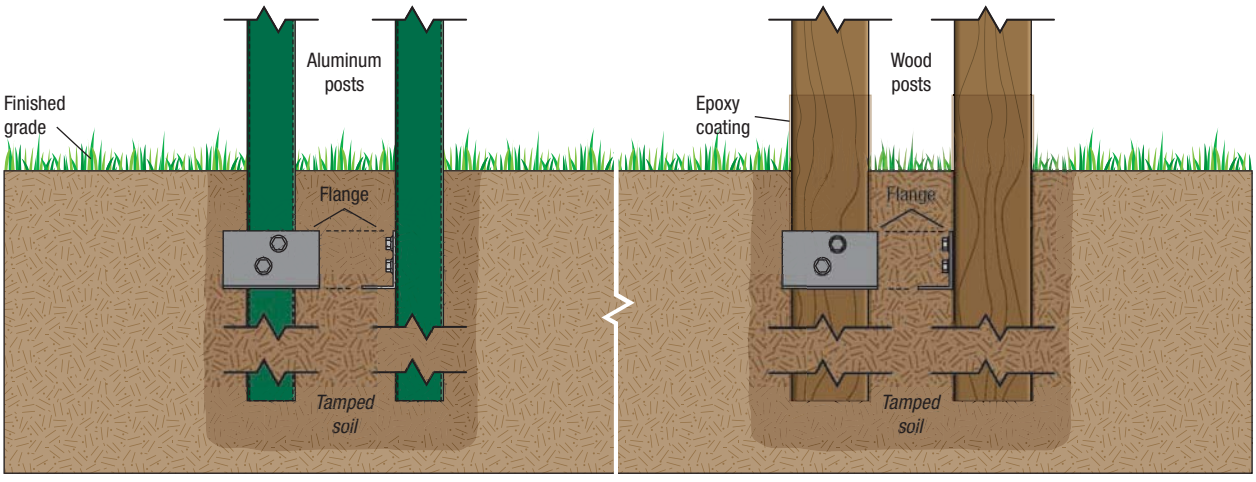
Direct embedment

Direct embedment is the direct burial of sign posts of sufficient length directly into the ground. See Section 6.3 for recommended installation heights to achieve the recommended centre or baseline value.

When using the direct embedment technique, aluminum and wood posts must incorporate a flange to resist unwanted removal and provide added stability for the sign. The flange is to be a stainless steel angle and attached using appropriate fasteners dependent on the post (i.e. stainless steel bolts for aluminum or stainless steel lag bolts for wood).

To increase the life cycle of wood posts, the application of an epoxy system formulated for direct water contact for wood products may be used (*see Section 5.7.2 for more information*).

Figure 6.1 – Direct embedment system for aluminum and wood



Embedded anchor system

Specific to aluminum posts and flexible campsite markers (delineators), the anchor installation system provides for greater flexibility depending on local installation conditions and is less invasive than other installation methods. This approach involves the embedment of an anchor component that projects above grade, to which a 2.5" x 2.5" aluminum post or campsite marker is affixed. The embedded anchor system is ideal for hard to access areas where transporting auger equipment would be difficult and sign face size is limited.

Parks and sites that elect to use the anchor approach should ensure that the material used, whether it is an "off-the-shelf" system or a custom solution, is compatible with the 2.5" x 2.5" aluminum posts. Figure 6.2 illustrates the anchor concept. Parks and sites should work with the sign fabricator to determine the best solution.

Figure 6.2 – Isometric view of possible embedded anchor sytem for 2.5" x 2.5" aluminum posts

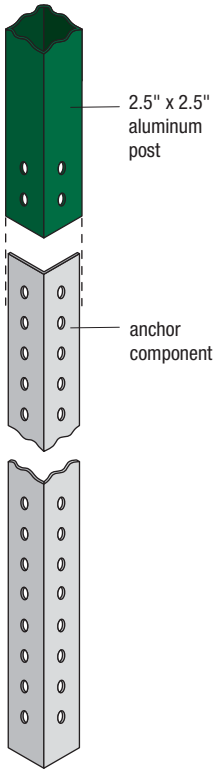
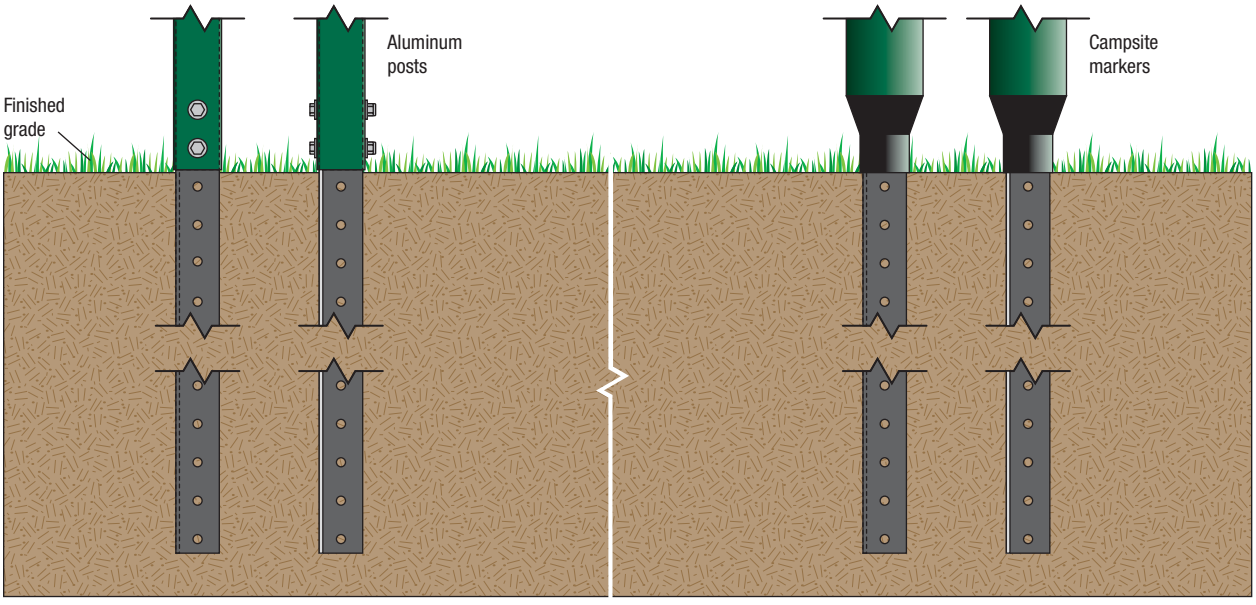


Figure 6.3 – Embedded anchor system applied to aluminum posts and campsite markers



Foundation
The foundations are generally one of three styles of poured-in-place concrete footings:

1. Concrete caisson with either direct embedment of post or a post with a pad-style base plate (to accommodate 2.5" x 2.5" aluminum, wood or structural steel posts).
2. Spread footing, which is similar to a caisson with the addition of a bottom pad of concrete.
3. A floating raft foundation engineered to accommodate bedrock found within the area of the excavation. This approach requires that a pad of concrete be poured directly onto the bedrock with pins/ties tapped into the bedrock itself if anchoring the base plate directly to the bedrock is unsuitable given the type of bedrock or incline of the bedrock surface.

The selection of caisson or spread footing is generally determined by an engineer and is based on location-specific issues such as soil, windload, etc. for a given sign face size.

In situations where bedrock is encountered, it is possible that a slight adjustment in sign location may allow for a direct embed or anchor approach. If adjustments to location are made to allow for the direct embed or anchor technique, the sign plan should be updated to reflect the modification.

Figure 6.4 – Concrete caisson foundations with direct embedded and base plate mounted posts

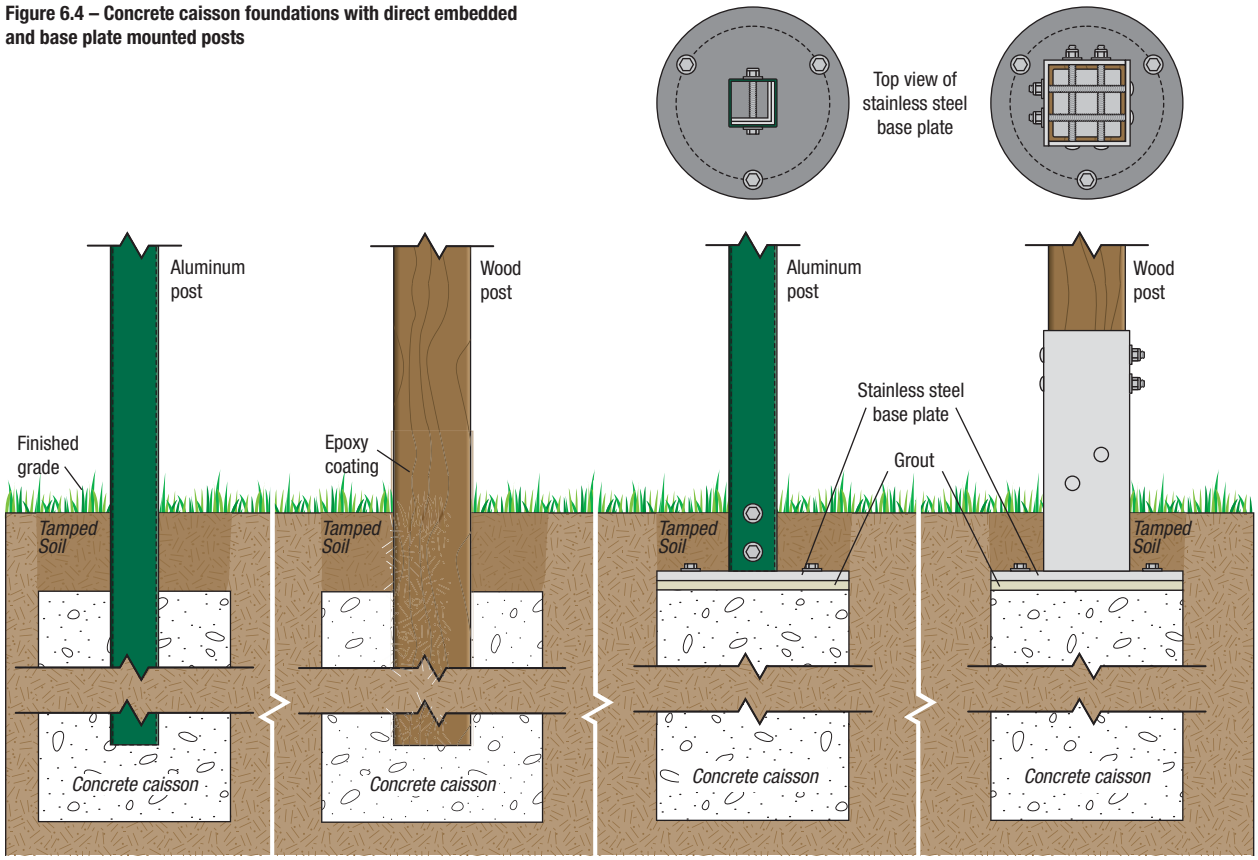


Figure 6.5 – Spread footing foundations with direct embedded and base plate mounted posts

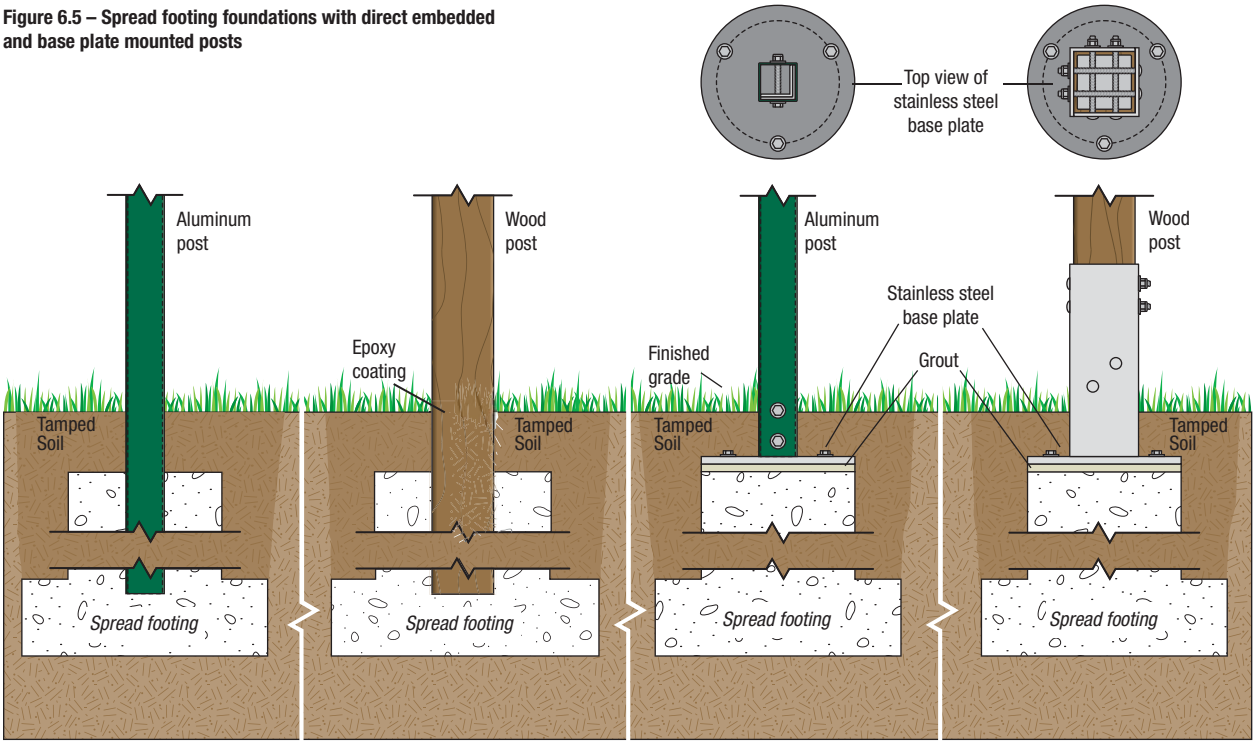
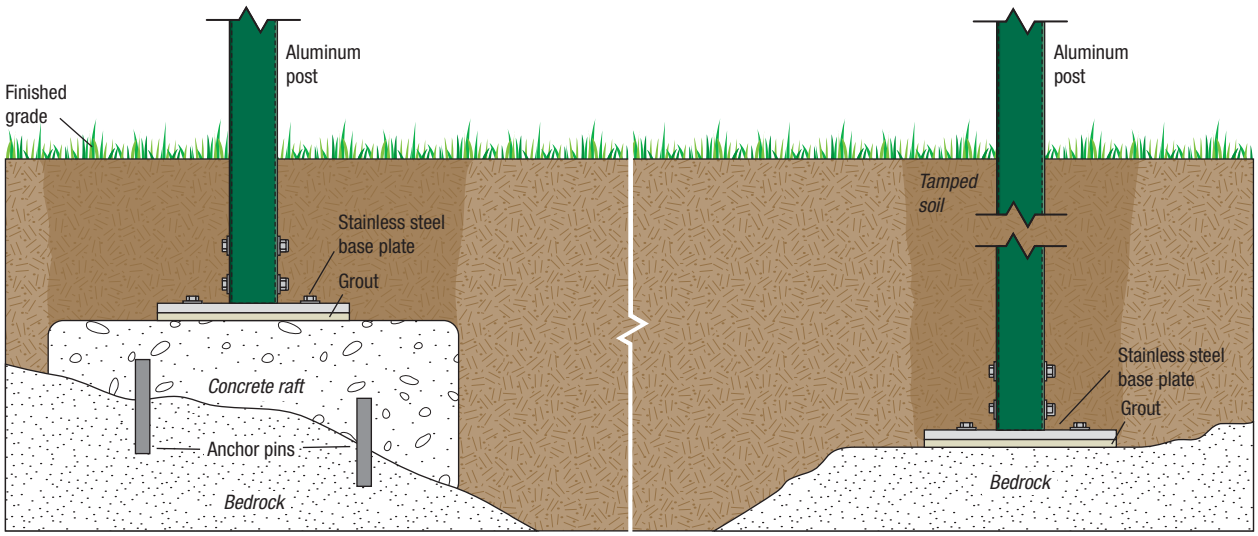


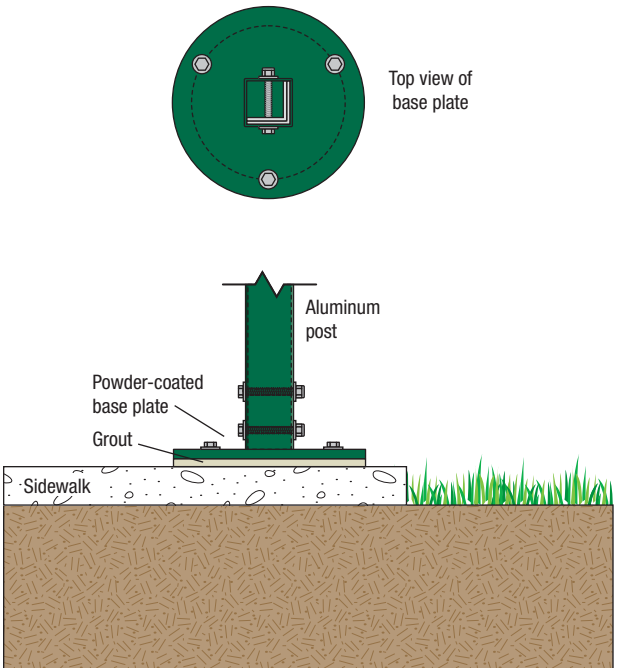
Figure 6.6 – A floating raft foundation (left) is used when anchoring directly onto bedrock (right) is not possible due to type of bedrock or incline of bedrock surface.



Pad mount

The pad-mount installation technique is simply the anchoring of a sign post with a base plate attached directly to a concrete surface such as a sidewalk. The base plate is the same as is used for the other foundation installation techniques and will be typically used for aluminum post installations. As the base plate will be visible above grade (surface) it will be powder-coated to match the required post colour (Heritage Green or black).

Figure 6.7 – Pad-mount installation on sidewalk



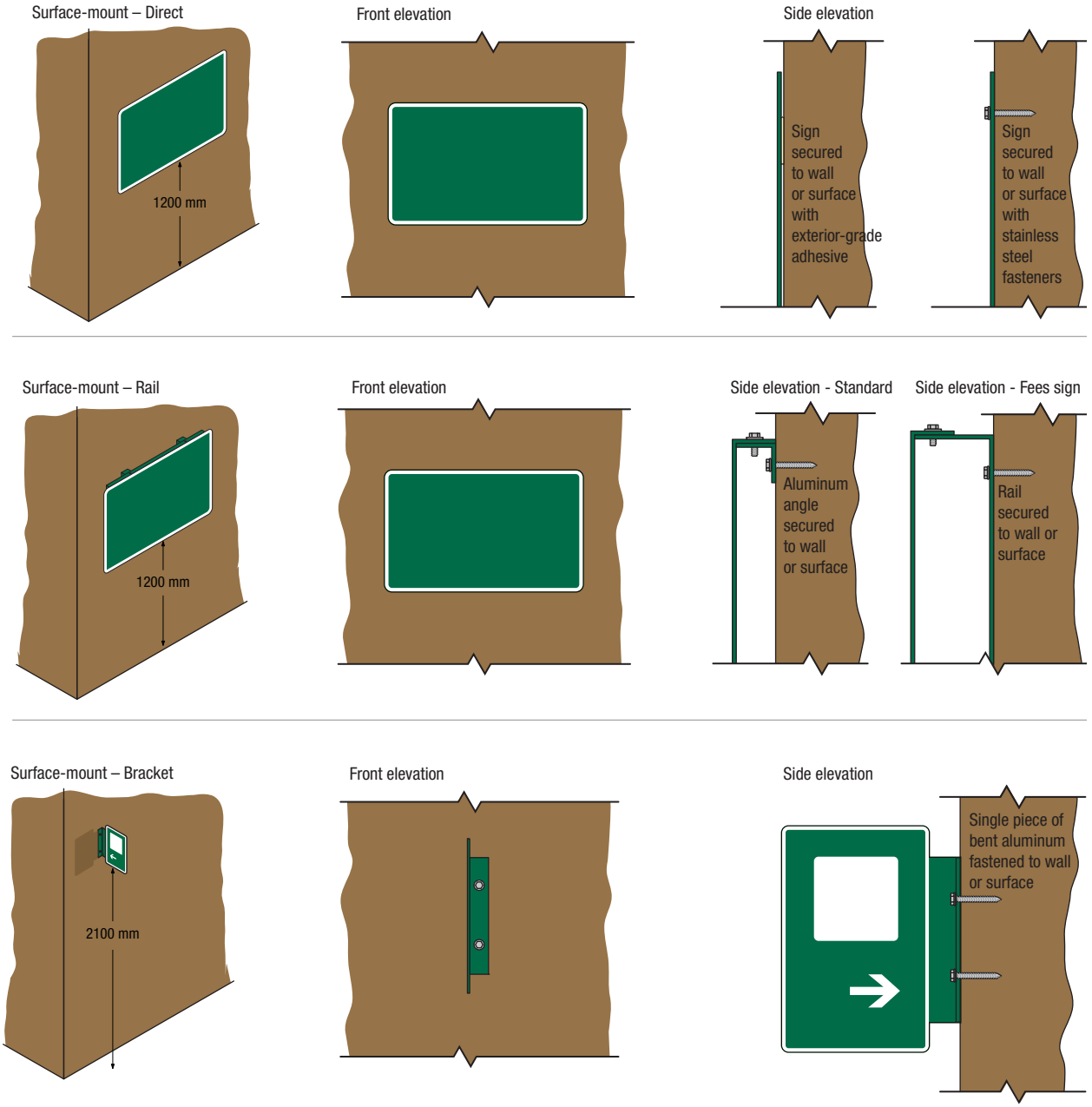
6.2.2 Surface Mount

There are three principal installation techniques for surface mount signs:

1. Direct mount that employs stainless steel fasteners, exterior grade adhesive or other fastening method (signs mounted to a fence) to affix a sign directly to a surface.
2. Rail-mount system that allows for a sign to be affixed to a surface with hidden fasteners and is changeable.
3. Bracket mount that allows for a sign to be installed perpendicular to a surface and is generally double-sided.

When planning for a surface-mount sign, it is essential to determine the appropriate technique during the sign planning phase and prior to fabrication as each technique employs very different fabrication methods.

Figure 6.8 – Surface mount methods

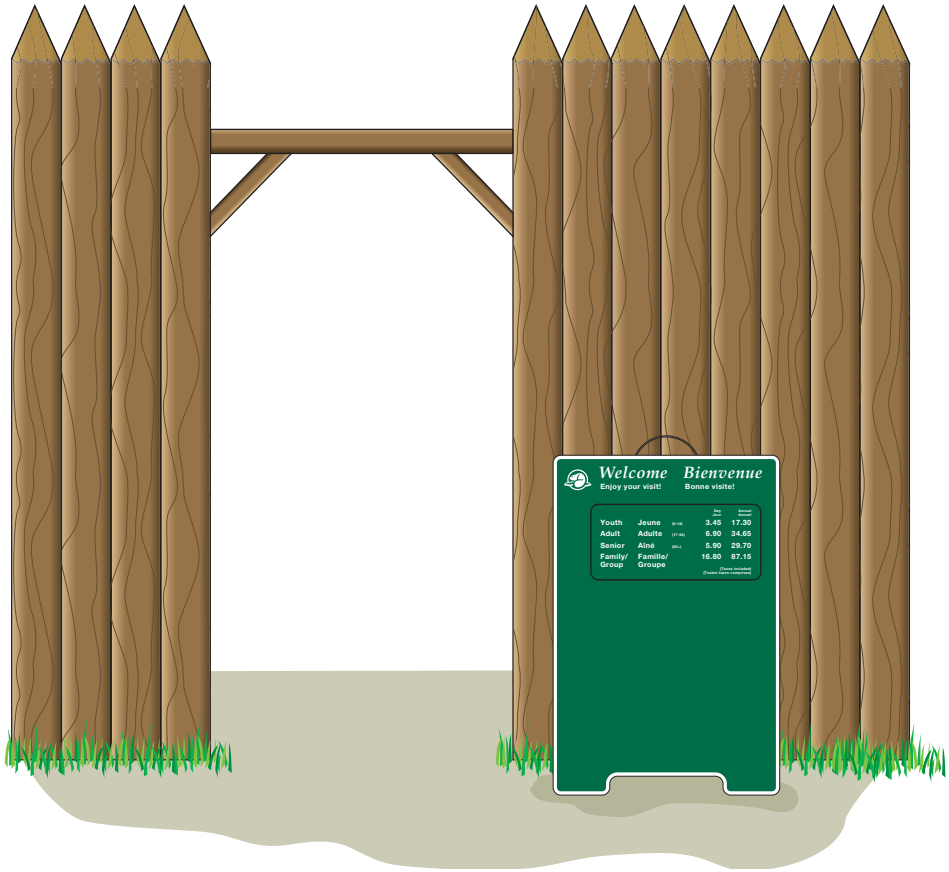


6.2.3 Portable

Portable signs are freestanding signs that are displayed and removed when required. While there are no specific installation techiniques required for portable signs, some consideration is required to:

- ensure the sign does not obstruct pedestrian or traffic flow;
- ensure the sign is placed to the right of the dominant traffic flow where possible;
- ensure the sign in no way poses a hazard to visitors.

Figure 6.9 – Portable sign placement



6.3 Installation Height and Setback

The installation heights and setbacks of signage have a significant impact on the overall aesthetic value of a signage system. In determining the appropriate installation height and offset of a sign, many factors must be taken into consideration, including:

- if the sign is intended for vehicular or pedestrian traffic;
- accessibility;
- conditions specific to the sign location (e.g. hiking trail that becomes a ski trail in winter, large snow drifts, etc.);
- campgrounds;
- parking control signs.

It is recognized that not all situations will permit signs to be installed according to the recommended installation height or setback specifications. It is important to address these issues during the sign planning phase in order to develop a viable solution. Vehicular signs are governed by MUTCD or provincial traffic control standards.

6.3.1 Vehicular Signs

Parks Canada is responsible for ensuring that all vehicular signs are installed in accordance with either the Manual of Uniform Traffic Control Devices for Canada, 4th Edition or provincial/territorial requirements. Parks and sites must therefore ensure that all vehicular signs are installed according to the appropriate standard. This would include elements such as:

- location;
- orientation (angle of sign in relation to approaching traffic);
- lateral placement (offset);
- vertical placement (installation height);
- sign foundations;
- breakaway requirements;
- requirements for barriers;
- advance placement distances.

The information provided below is derived from the Manual of Uniform Traffic Control Devices, 4th Edition, as well as the Transportation Association of Canada's Supplemental Guide for Guide and Information Signage in Canada. It is intended as a reference only and is meant to support, not substitute, information provided in the MUTCD or provincial standards. Prior to beginning any signage initiative involving roadway signs, up-to-date MUTCD or provincial standards must be consulted.

6.3 Installation Height and Setback (CONTINUED)

6.3.2 Vehicular Sign Offset

Signs should be placed so that the distance from the outside edge of roadway to the inside edge of the sign is not less than 2.0 m nor more than 4.5 m. The exception is where conditions do not permit. Refer to the Manual of Uniform Traffic Control Devices for Canada for highway specifications.

Where there is a raised curb, a sign should be placed adjacent to the road with its inside edge not less than 0.3 m nor more than 2.0 m from the curb face.

Figure 6.10 – Areas without raised curbs

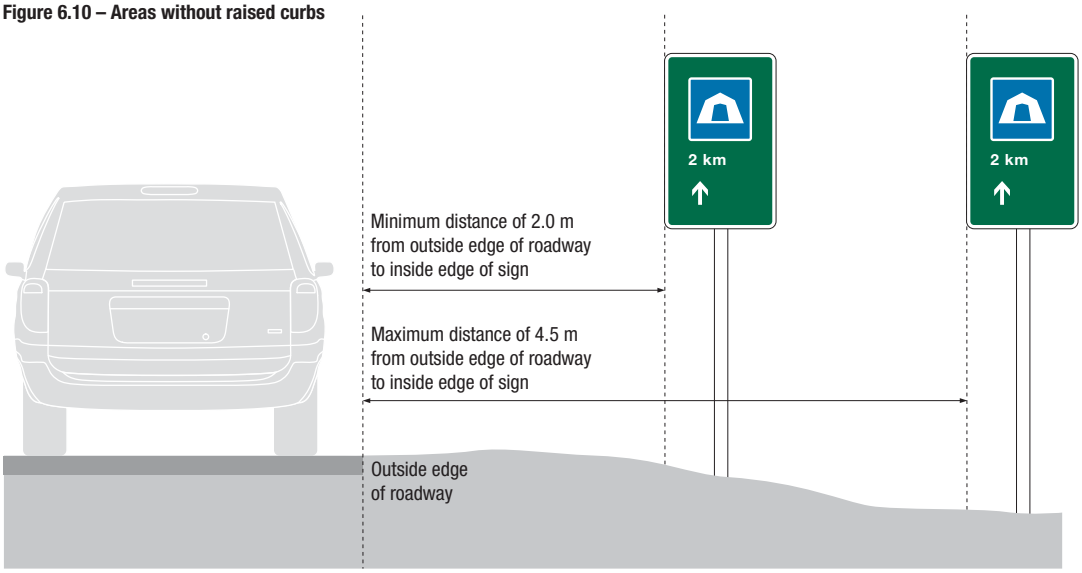
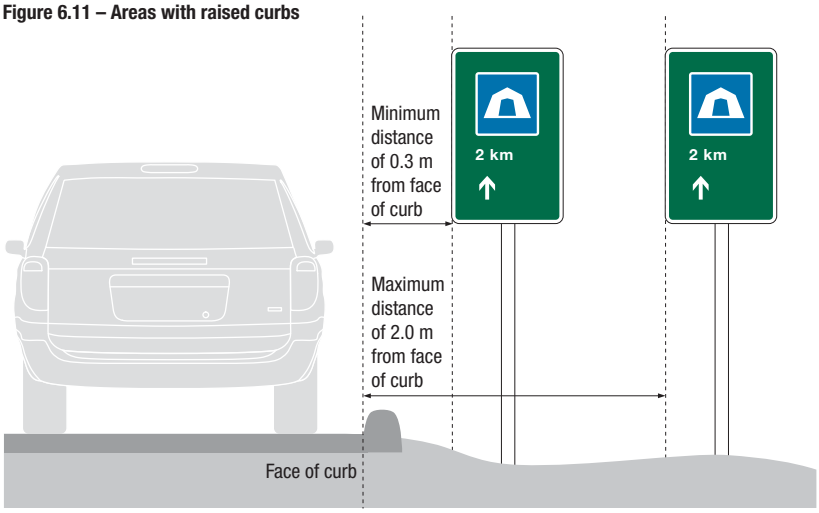


Figure 6.11 – Areas with raised curbs

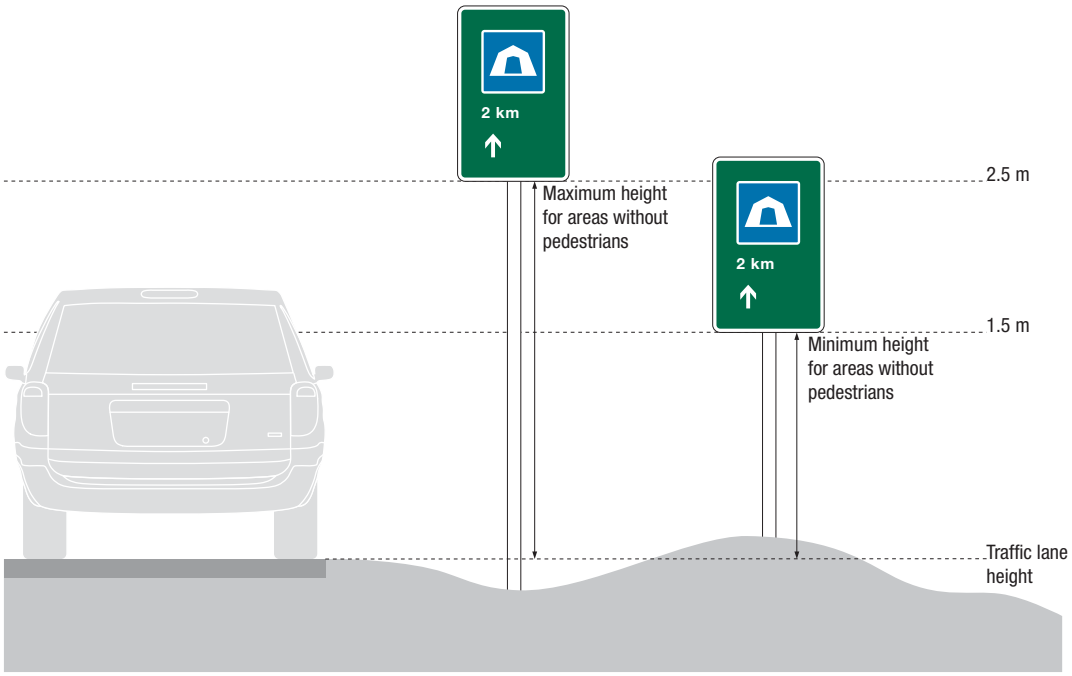


6.3.3 Vehicular Sign Height

Areas without pedestrians

Vehicular signs should be mounted at a height of 1.5 m above the outside edge of the nearest traffic lane to the bottom of the sign. This height may be as much as 2.5 m for special conditions.

Figure 6.12 – Vehicular sign height for areas without pedestrians

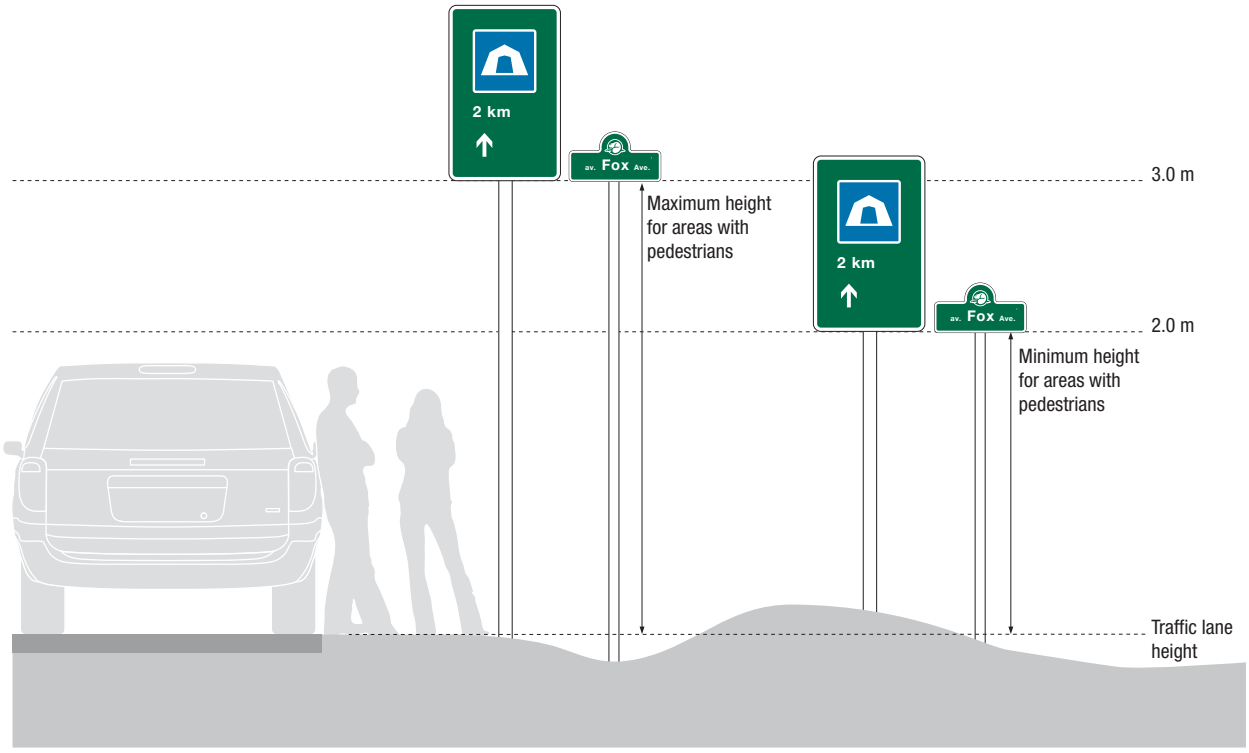


6.3 Installation Height and Setback (CONTINUED)

Areas with pedestrians
At locations where pedestrian traffic is likely, the sign should be mounted at a height of 2.0 m to 3.0 m from the traffic lane to the bottom of the sign.

Freeway sign height
Refer to MUTCD
A1- Signs, A1.7.2 Standard Sign Placement
A5- Freeway, A5.4.3 Lateral Placement and A5.4.4 Vertical Placement

Figure 6.13 – Vehicular sign height for areas with pedestrians



6.3.4 Parking Control (ADA)

When determining the installation height of parking control signs, accessibility considerations must be taken into account. In this regard, Parks Canada has based its parking control installation height guidelines on the American Disabilities Act (ADA) *Standards for Accessible Design* and is consistent with *Access Series: Design Guidelines for Media Accessibility* published by Environment Canada.

Accessible parking signs

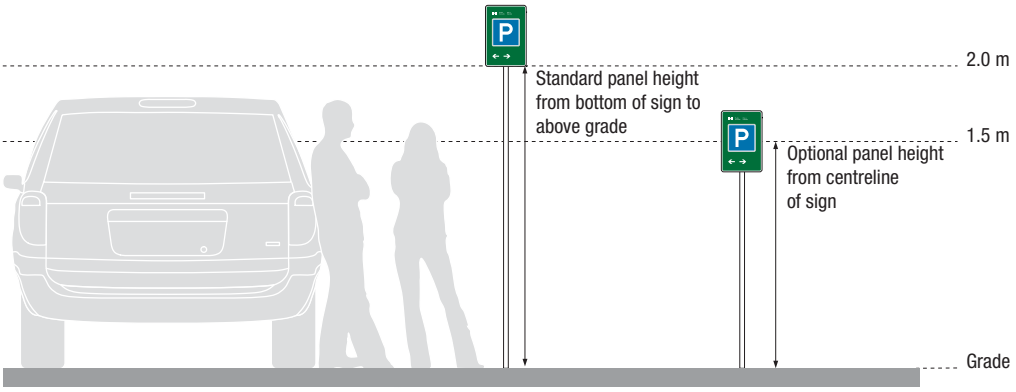
Accessible parking signs should be located so that they cannot be obstructed by a vehicle. This presents difficulties due to the wide range of potential vehicles that could possibly obstruct such a sign. In determining an appropriate location, provincial regulations should first be considered. For signs that are required to be mounted at 2.0 m height, care should be taken to appropriately incorporate them into the landscape and architectural environment; otherwise, they can become intrusive.

In many instances, a sign mounted at a lower height, such as 1.5 m to the center of the sign, can be adequately viewed over the roof of most vehicles, and is a more appropriate response in relation to human scale and overall aesthetic considerations.

Such signs can be typically mounted at relatively low heights and can be incorporated into landscaping elements.

Another option is to place signs on the outer edges of the parking area, avoiding any obstruction from vehicles.

Figure 6.14 – Parking control sign height



6.3.5 Pedestrian Signs

When determining the installation height of pedestrian signs, accessibility considerations must be taken into account. In this regard, Parks Canada has based its pedestrian installation height guidelines on the American Disabilities Act (ADA) *Standards for Accessible Design* and is consistent with *Access Series: Design Guidelines for Media Accessibility* published by Environment Canada.

While vehicular sign installation heights are determined by the installation baseline (distance from travelled surface to bottom of sign), pedestrian sign installation heights are primarily determined by the centreline (distance from travelled surface to mid-height of sign). For overhead signs, the installation height is determined by the baseline.

Wall- and post-mounted signs

Pedestrian signs mounted on a post or wall should be 1.5 m from the centerline of the sign to the ground.

Figure 6.15 – Pedestrian panel height for post-mounted signs

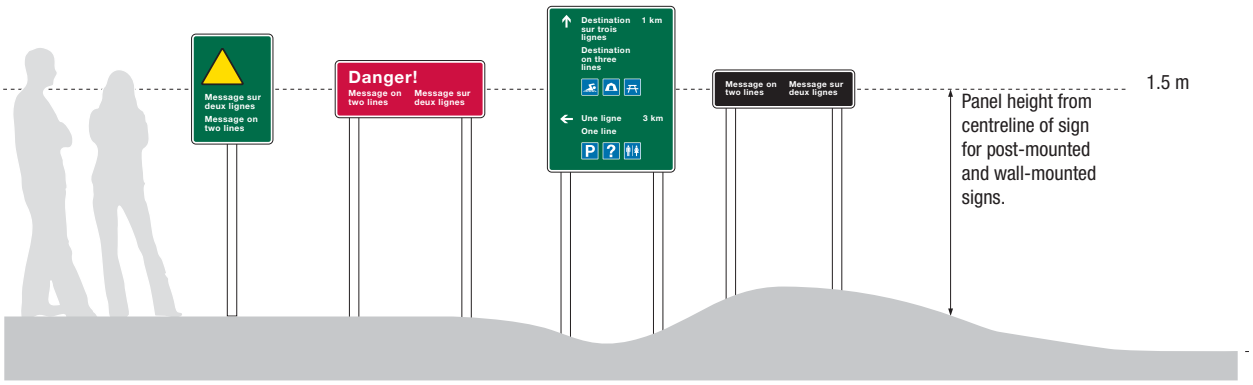


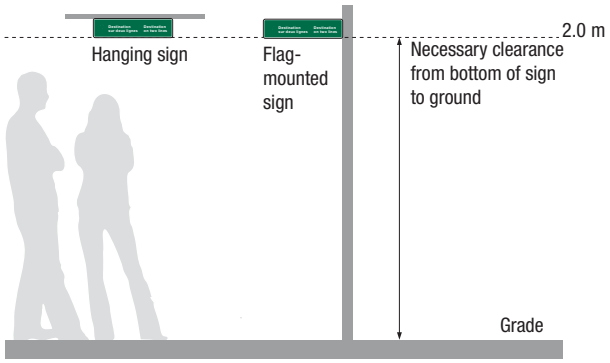
Figure 6.16 – Pedestrian panel height for wall-mounted signs

6.3 Installation Height and Setback (CONTINUED)

Suspended signs

Pedestrian signs that are hanging or flag-mounted must meet requirements for clearance. There must be at least 2.0 m of clearance below the bottom of the sign.

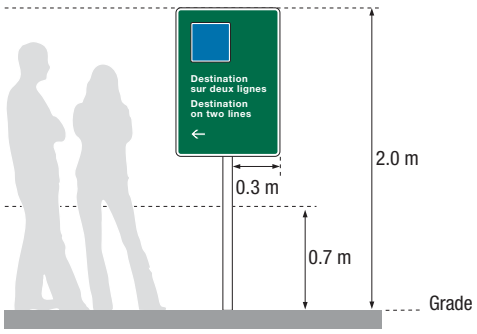
Figure 6.17 – Suspended signs



Protruding signs

No pedestrian signs shall protrude more than 0.3 m from posts in a horizontal zone between 0.7 m from the ground to 2.0 m from the ground.

Figure 6.18 – Protruding signs



Custom applications

Some installation applications may require installation heights that deviate from the standards. Typical examples include signs next to faucets, signs mounted below kiosk windows, signs mounted on gates and signs with campsite numbers.

6.4 Guidelines for Large Sign Structural Supports

The American Association of State Highway and Transportation Officials (AASHTO) Roadside Design Guide[1] defines large highway signs as those greater than 50 square feet (4.6 sq m).

Professional engineers design large highway sign supports as structures. The AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals[2] is typically used for design.

Provincial highway departments normally require conformance to standard structural drawings for their large highway sign supports, based on AASHTO standards. For example, the Ministry of Transportation for Ontario, in its Sign Support Manual[3], requires that its standard drawings for large roadside sign supports (e.g. steel support – signs up to 3.600 x 6.600 m; timber support - signs up to 2.700 x 6.000 m) be used where appropriate.

Large sign supports placed close to the road pose serious roadside hazards to errant vehicles. The AASHTO has developed the concept of a roadside recovery area or clear zone, an unobstructed area provided beyond the edge of the travelled way for the recovery of errant vehicles. Clear zones must be traversable, allowing vehicles to recover or come to a safe stop. Any hazards that remain within the clear zone must be removed, shielded or made breakaway. Large sign supports must therefore be shielded or the supports must break away on impact as a means of preventing injuries and vehicle damage. For example, Alberta Transportation[4] requires that all sign supports within the clear zone either be of a breakaway type meeting the crashworthiness criteria set out in National Cooperative Highway Research Program (NCHRP) Report 350[5] or be protected by guardrail, barrier, or an energy-absorbing system meeting NCHRP Report 350 criteria.

Similar to provincial and state practice, the United States National Park Service has standard drawings for sign support structures for large signs. The 1988 US National Park Service Sign Manual[6] contains drawings, tables and dimensions for signs up to 18 feet wide (5.486 m), including breakaway design features.

The 2002 National Park Service UniGuide Standards Manual[7], which is replacing the 1988 Sign Manual, also contains standard drawings, including tables and dimensions, for sign supports for large road guide signs up to a maximum of 192" wide x 96" high (4.877 m x 2.438 m). In addition to the drawings, there are references to AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals, the AASHTO Roadside Design Guide and the NCHRP Recommended Procedures for the Safety Performance Evaluation of Highway Features (Report 350).

6.4.1 Parks Canada Highway Sign Guidelines

To ensure consistency with provincial highway standards, Parks Canada has made a policy decision to follow provincial standards and practices for roadway signing within national parks, national historic sites and national marine conservation areas. Provincial designs and practices must be followed for support structures for large highway signs over 4.6 square metres in surface area.

Where provincial highway sign support drawings and specifications for large highway signs are unavailable, incomplete, or unacceptable for a Parks Canada setting, a professional engineer must be retained for the design and installation of these large Parks Canada highway sign support structures.

The drawings and specifications for these large highway sign supports must have the seal, date and signature of a professional engineer. Among the factors to consider in the design of the sign support structures are:

- loads (e.g., ice load, wind load, dead load, live load, group loads);
- material design (e.g., aluminum, wood, steel, concrete, etc.);
- fatigue design;
- foundation design (e.g., soil type, frost depth)
- breakaway supports (United States Federal Highway Administration provides Acceptance Letters for breakaway supports. These Acceptance Letters are posted on their web site at http://safety.fhwa.dot.gov/roadway_dept/road_hardware/signsupports.htm);
- clear zone width.

A clear zone is defined as the roadside border area (starting at the edge of the travelled way) that is available for safe use by errant vehicles. The AASHTO has determined variable clear zone widths based on traffic volumes, speeds, and roadside geometry. Vehicles out of control while travelling on a horizontal curve can be expected to travel further away from the roadway due to centrifugal forces on the outside. The tangent section clear zone is increased by a curve correction factor based on the degree of curvature, the design speed, and the roadside width. The AASHTO Roadside Design Guide provides additional guidance.

Where feasible and environmentally acceptable, the clear zone width should be a minimum of 3 metres. Because of the character of the traffic and the relatively low operating speeds on local park roads, wide clear zones are not as important as on high-speed high-volume Parks Canada through highways. On local park roads with a design speed of less than 60 km/h or an average daily traffic of less than 400, the clear zone width may be 0 to 2 metres depending on economic and environmental concerns[8].

Breakaway sign supports are not normally required on sections of highways where the sign supports are placed beyond the clear zone widths.

Further guidance for determining design and installation requirements for these sign support structures can be found in the following references:

a) Latest edition of the AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals
https://bookstore.transportation.org
ISBN 1-56051-309-8

b) Latest edition of AASHTO Roadside Design Guide
https://bookstore.transportation.org
ISBN 1-56051-132-X

References

1. AASHTO, "Roadside Design Guide," American Association of State Highway and Transportation Officials, Washington D.C. (2002).
2. AASHTO, "Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals," American Association of State Highway and Transportation Officials, Washington D.C. (2001).
3. MTO, Sign Support Manual. Ministry of Transportation, Ontario (2002).
4. Alberta Transportation (see http://www.infratrans.gov.ab.ca/INFTRA_Content/docType253/Production/bstprsignmat.pdf)
5. NCHRP Report 350, National Cooperative Highway Research Program, Transportation Research Board, Washington D.C. (1993).
6. 1988 US National Park Service Sign Manual, National Park Service, 1988.
7. 2002 National Park Service UniGuide Standards Manual, June, 2002. (http://www.nps.gov/hfc/acquisition/uniguide.htm#)
8. AASHTO, Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT=<400) (2001).

6.5 Maintenance

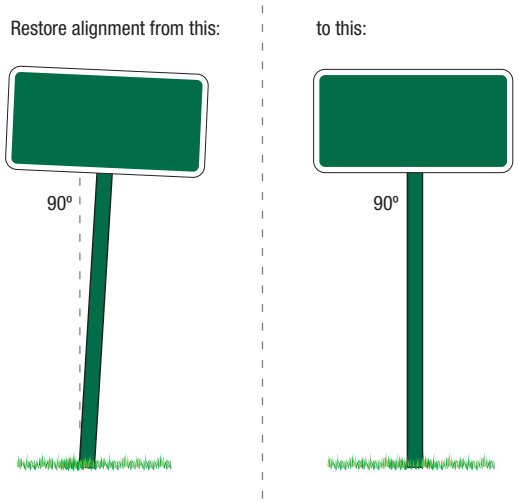
Proper sign maintenance is a critical component of any signage system. Benefits of a good sign maintenance regime include:

- Increased life expectancy of signs resulting in substantial savings in overall program costs.
- Proper due diligence in ensuring any damaged/missing traffic or safety signs are repaired/replaced thereby mitigating any potential risk of accident and/or associated liability.
- Ensuring that retroreflective properties of signs are not inhibited by dirt, thereby allowing the sign to function as intended.
- Ensuring that broken or noticeably weathered signs do not dilute the effect of the other signs in the system.
- Mitigating the potential for the proliferation of vandalism by ensuring vandalized signs are cleaned or replaced. The presence of signs of vandalism on a sign acts as an invitation for other incidents of vandalism.
- Providing a quality visitor experience and reflecting positively on the organization and its staff.
- Contributing to a strong and coherent external image for Parks Canada.

Each park, historic site and marine conservation area is required to put in place a maintenance program for signs as part of its asset maintenance requirements. The following principles should be integrated into sign maintenance programs:

- Conduct a comprehensive inspection of signs a minimum of twice a year (fall/spring) to coincide with the beginning and end of the prime visitor season. This includes a comprehensive review of all signage on the one-year anniversary of each installation (minimum one year post thaw) to ensure each sign post is true and vertical (*see Figure 6.19 for more information*).
- Inspect high visitor areas more frequently.
- Inspect any Warning (caution and danger), Regulatory (prohibition, mandatory and parking control), and Emergency signs more frequently to ensure continued due diligence.
- Ensure that tree branches, shrubs, weeds and brush around the sign are removed to ensure that there are unobstructed sight lines to the sign and that it is clearly legible to the approaching visitor.
- Signs should be clear of grease, using manufacturer-recommended products.

Figure 6.19 – Restore alignment



- Retroreflective signs are to be clean and free of stains/damage to ensure good legibility and maximum reflectivity.
- Staff should report problems at any time. If signs are well integrated into an asset management database, it will be easy to order replacements and keep track of situations that should be monitored (such as vandalism occurrences, decreased level of retro-reflectivity, etc.).
- Signs should be replaced immediately if deemed to be non-repairable.

Cleaning

- Flush sign face with clean water to remove loose dirt.
- Wash sign with soft brush, rag or sponge. Use a mild, non-abrasive biodegradable detergent (manufacturer-recommended product) that will not damage adjacent vegetation and is chemically neutral and free of strong aromatic solvents or alcohol. A list of appropriate cleaners tested for reflective sheeting should be available from the sheeting manufacturer.
- Wash from the top down. Avoid abrading the surface with unnecessary scrubbing.
- Rinse entire sign with clean water and allow to dry.

A

Appendix A – Symbols

Appendix A – Symbols

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Introduction

The following pages contain the approved symbols for use on Parks Canada signs. The use of symbols not included in the approved Parks Canada symbol set is prohibited. In addition, all media (publications, interpretive exhibits, web site, etc.) are required to use the approved Parks Canada symbol set to ensure consistency and increase comprehensibility of the symbols through use.

As signage is a mandatory application under the Federal Identity Program, Parks Canada is required to adopt the symbol set developed for government-wide use. The symbol set for the Government of Canada is based on the following national and international standards:

- National Standard of Canada CAN/CSA-Z321-96, *Signs and Symbols for the Workplace*
- International Organization for Standardization ISO 7001, *Public Information Symbols*

Parks Canada's unique operational requirements necessitated the addition of various symbols not included in the Government of Canada symbol set. To ensure maximum visual coherence with the FIP symbol set and harmonization with national and international standards, the Parks Canada symbol set has been designed by adopting the basic principles outlined in the above-noted standards.

To ensure proper due diligence and maximum comprehensibility of the new symbols, the Parks Canada symbol set has undergone comprehension testing based on the evaluation methodology prescribed by the International Standards Association. Regulatory and warning symbols scoring a comprehension rate of 80% or higher have been adopted for use. Information symbols scoring a comprehension rate of 60% or higher have been approved for use. In order to ensure comprehension and promote positive visitor experiences, it is recommended that information symbols scoring less than 80% be accompanied by a supporting text message.

All symbols that have been tested are listed in this appendix and the status of each symbol is provided. Symbols that have passed comprehension testing and have been approved for use are indicated with a solid image. Symbols for which a text message is required or recommended are identified. Those symbols that failed comprehension testing and have been retired from use are presented in a light tint with a note explaining their status.

During the process of developing this symbol set every effort has been made to address the operational requirements of Parks Canada. In the event that a symbol is required that is not part of the approved Parks Canada symbol set, a written request must be sent to the Parks Canada Identity Program Officer at pimpc-pcip@pc.gc.ca. A review of the request will take place and, if deemed appropriate for inclusion in the Parks Canada symbol set, a new symbol will be designed and tested for comprehension according to the standards outlined above. Appendix A will be updated as new symbols are released.

Generic vs. Specific Symbols

A symbol is a simplified image of a subject or referent. The challenge in designing symbols is to identify image content that can be expressed in simple visual terms. That is, the subject can be distilled into a graphic image and be readily understood by a wide audience. Some subjects lend themselves well to symbolization. Subjects that are complex or obscure are more difficult to communicate in the form of a symbol. Most subjects can be described at a basic, generic level. Climbing, the act of scaling a vertical slope, is the generic form of this activity. Types of climbing, mountain, rock, ice, etc., are specific descriptions of this activity and add an additional level of meaning to the generic subject.

An outcome of the symbol evaluation process has been the confirmation that symbols that attempt to communicate specific aspects of a subject tend to be less comprehensible than symbols designed to communicate the generic form of a subject. A unique symbol for each of the following types of slippery conditions was developed and tested: slippery floor, slippery rocks, unstable ground and ice/snow on trails. The function of each is the same, to warn of the potential risk of falling due to a slippery surface. Testing revealed that most viewers were unable to discern the specific condition contributing to a slippery surface, e.g. ice, loose surface, etc. The majority of respondents did, however, identify the generic aspect of the symbol—risk of falling due to a slippery surface.

The development and maintenance of the symbol set is influenced by the comprehension evaluation exercise. Some symbols which tend toward communicating specific subjects have been deemed redundant and retired from the set. In these situations, direction to use a generic symbol and an approved supporting bilingual text message which describes the specific condition has been provided.

For example, a "Do not feed the elk" symbol was developed and tested. Two percent of viewers correctly interpreted the intended meaning of this symbol. A generic "Do not feed the animals" symbol was also tested with a successful comprehension rate of 93%. The "Do not feed the elk" symbol, and other symbols intended to represent a species-specific feeding prohibition, will be retired and replaced with the generic "Do not feed the animals" symbol. In this example a specific supporting text message "Do not feed the elk" can be included with the generic symbol to help convey the intended meaning of the sign.

Signage and Communication

The communication function of the signage program is broad in scope, ranging from identification and directional information to regulatory and warning messages. Decisions regarding the use of symbols/signage should be made with the following in mind:

- Appropriate attitudes and motivation of sign readers are required before compliance with any sign will result.
- A graphic symbol is not intended as an easy alternative to surveillance, physical barriers, education, or other means of achieving a safe situation.
- An appropriate symbol and text message working in tandem are a potent combination, providing an increased level of sign comprehension not achievable when these elements are used individually.

Symbol Inventory – Sorted by Name

Symbol name	Code	Classification	Status
A			
Access for the Hearing Impaired	6-1-039	Information	
Access for the Physically Impaired	6-1-032	Information	
Access for the Visually Impaired	6-1-042	Information	
Accessible Elevator	6-1-036	Information	
Accessible Parking	6-1-038	Information	
Accessible Ramp	6-1-037	Information	failed; retired
Accessible Telephone	6-1-035	Information	
Accessible Toilet for Men	6-1-034	Information	
Accessible Toilet for Women	6-1-033	Information	
Accessible Shower	6-1-106	Information	
Accommodation	6-2-003	Information	
Air Transportation	6-3-004	Information	
Alcohol, No	1-1-050	Prohibition	
All-terrain Vehicles	6-4-018	Information	
All-terrain Vehicles, No	1-1-005	Prohibition	
Amphitheatre	6-1-084	Information	failed; retired
Anchorage	6-4-210	Information	text recommended
Archeological Site	6-1-085	Information	failed; retired
Avalanches	4-1-032	Danger	text required
B			
Backpacking/Bicycling	6-4-024	Information	
Backpacking/Hiking	6-4-006	Information	
Backpacking/Hiking, No	1-1-054	Prohibition	
Backpacking/ Horseback Riding	6-4-022	Information	
Back-country Camping	6-4-025	Information	
Baggage	6-1-025	Information	text recommended
Baggage Cart	6-1-027	Information	text recommended
Baggage Locker	6-1-026	Information	
Bar/Cocktails	6-2-005	Information	
Be Alert!	3-1-004	Caution	text required
Beach (generic)	6-4-221	Information	under development
Beach Logs	3-1-109	Caution	failed; retired
Bears	3-1-151	Caution	text required
Bicycle Parking	6-1-102	Information	
Bicycle Parking, No	1-1-016	Prohibition	
Bicycle Rental	6-2-014	Information	
Bicycles	6-4-104	Information	
Bicycles, No	1-1-011	Prohibition	
Biohazard	4-1-006	Danger	failed; retired
Bison	3-1-152	Caution	text required
Bison Viewing	6-1-088	Information	
Blower On	2-1-002	Mandatory	under development
Boat Launch	6-4-217	Information	
Boat Pump-out	6-1-061	Information	under development
Boat Rafting, No	1-1-045	Prohibition	under development
Boat Tie-up	6-4-219	Information	
Boat Tour	6-4-208	Information	failed; retired
Boat Trailer Parking	6-1-090	Information	text recommended
Boat Trailer Parking, No	1-1-017	Prohibition	
Boats, No	1-1-007	Prohibition	
Book Store	6-2-019	Information	failed; retired
Breathing Protection	2-1-034	Mandatory	under development
Buses	6-1-008	Information	failed; use 6-3-001
Bus and Taxi Transportation	6-3-003	Information	
Bus Parking	6-1-092	Information	
Bus Tour	6-4-012	Information	failed; retired
Bus Transportation	6-3-001	Information	
Buses, No	1-1-002	Prohibition	
C			
Cafeteria	6-2-006	Information	failed; retired
Camper Trucks	6-4-014	Information	failed; use 6-4-013
Campfire	6-1-063	Information	
Campfires, No	1-1-052	Prohibition	
Camping	6-4-001	Information	
Camping, No	1-1-048	Prohibition	
Canine (generic)	3-1-154	Caution	text required
Canoe Camping	6-4-220	Information	text recommended
Canoe Rental	6-2-015	Information	
Canoeing	6-4-214	Information	
Canoeing, No	1-1-063	Prohibition	
Car & Boat Trailer Parking	6-1-091	Information	
Car Ferry	6-3-008	Information	
Car Rental	6-2-013	Information	failed; retired
Car Wash	6-2-012	Information	
Car Washing, No	1-1-066	Prohibition	
Cars	6-1-009	Information	text recommended
Cars or Bicycles, No	1-1-013	Prohibition	
Cars, No	1-1-003	Prohibition	

Symbol Inventory – Sorted by Name (CONTINUED)

Symbol name	Code	Classification	Status
Caution, Presence of Ionizing Radiation	3-1-005	Caution	failed; retired
Caving Area	6-4-009	Information	text recommended
Chairlift	6-4-303	Information	
Change Rooms - Female	6-1-049	Information	text recommended
Change Rooms - Male	6-1-048	Information	text recommended
Change Table	6-1-051	Information	
Chemical Burn	4-1-005	Danger	under development
Classic Skiing	6-4-310	Information	
Cloakroom	6-1-023	Information	text recommended
Coffee Shop	6-2-001	Information	text recommended
Cold Temperatures (generic)	3-1-137	Caution	text required
Cold Water	3-1-112	Caution	failed; use 3-1-137
Compressed Gas	4-1-007	Danger	failed; retired
Crafts	6-1-052	Information	text recommended
Crevasses	3-1-173	Caution	failed; retired
Cross-country Skiing, No	1-1-057	Prohibition	
Currency Exchange	6-1-022	Information	
D			
Deadheads, Rocks	3-1-108	Caution	failed; retired
Deer (generic)	3-1-155	Caution	text required
Direction	5-1-011	Emergency	
Direction	5-1-012	Emergency	
Direction	5-1-013	Emergency	
Direction	5-1-014	Emergency	
Direction	5-1-015	Emergency	
Direction	5-1-016	Emergency	
Direction	5-1-017	Emergency	
Direction	5-1-018	Emergency	
Direction	6-1-094	Information	
Direction	6-1-095	Information	
Direction	6-1-096	Information	
Direction	6-1-097	Information	
Direction	6-1-098	Information	
Direction	6-1-099	Information	
Direction	6-1-100	Information	
Direction	6-1-101	Information	
Dirt Bikes	6-4-019	Information	
Dirt Bikes, No	1-1-006	Prohibition	
Diving	6-4-203	Information	failed; retired
Diving, No	1-1-062	Prohibition	
Docking	6-4-218	Information	retired
Dog Sled	6-4-313	Information	
Dogs on Leash	2-1-004	Mandatory	
Dogs on Leash	6-1-067	Information	
Dogs, No	1-1-001	Prohibition	
Downhill Skiing	6-4-302	Information	
Downhill Skiing, No	1-1-056	Prohibition	
Drink, Do Not	1-1-042	Prohibition	
Drinking Fountain	6-1-055	Information	
Drinking Water	6-1-056	Information	
E			
Electrical Hazard	4-1-002	Danger	text required
Electrical Hook-up	6-1-062	Information	
Emergency Eyewash	5-1-009	Emergency	under development
Emergency Shower	5-1-008	Emergency	under development
Emergency Telephone	5-1-010	Emergency	
Entry, No	1-1-018	Prohibition	retired
Escalator	6-1-018	Information	
Escalator Down	6-1-020	Information	retired
Escalator Up	6-1-019	Information	retired
Exhibit	6-1-078	Information	
Explosion Hazard	4-1-004	Danger	text required
Extinguish Fires	2-1-003	Mandatory	
Eye Protection	2-1-032	Mandatory	under development
F			
Fall Protection Equipment	2-1-038	Mandatory	under development
Family Washroom	6-1-103	Information	
Feed the Animals (generic), Do Not	1-1-073	Prohibition	
Feed the Birds, Do Not	1-1-071	Prohibition	
Feed the Elk, Do Not	1-1-072	Prohibition	failed; retired
Feed the Fish, Do Not	1-1-078	Prohibition	
Fire Alarm	5-1-001	Emergency	under development
Fire Axe	5-1-005	Emergency	under development
Fire Extinguisher	5-1-002	Emergency	
Fire Hose	5-1-003	Emergency	
Fire Hydrant	5-1-004	Emergency	
Firewood	6-1-064	Information	text required
Firewood, No	1-1-019	Prohibition	failed; retired
First Aid	5-1-006	Emergency	
Fish Hatchery	6-2-018	Information	failed; retired
Fishing	6-4-207	Information	

Symbol Inventory – Sorted by Name (CONTINUED)

Symbol name	Code	Classification	Status
Fishing, No	1-1-046	Prohibition	
Flammable	4-1-001	Danger	under development
Float Planes, No	1-1-008	Prohibition	
Freight Elevator	6-1-014	Information	
G			
Gasoline Station	6-2-011	Information	
Gift Shop	6-2-020	Information	
Golf	6-4-101	Information	
Golf Carts	6-1-089	Information	
Gondola	6-4-304	Information	
Gradual to Steep Drop-off	3-1-132	Caution	failed; use 3-1-131
Grey Water Disposal	6-4-201	Information	failed; use 6-1-053
Grey Water Disposal Drain	6-1-053	Information	
Groceries/Shopping	6-2-009	Information	text recommended
Group Camping	6-4-002	Information	failed; use 6-4-001
Guided Tour	6-1-079	Information	failed; retired
H			
Hair Protection	2-1-035	Mandatory	under development
Head Protection	2-1-031	Mandatory	under development
Hearing Impaired Telephone	6-1-040	Information	
Hearing Protection	2-1-033	Mandatory	under development
Helicopter Transportation	6-3-005	Information	
Heritage River	6-1-087	Information	failed; retired
Hiking	6-4-005	Information	failed; use 6-4-006
Hiking/Bicycling	6-4-023	Information	failed; use 6-4-024
Hitching Rail	6-4-106	Information	text recommended
Horse Camping	6-4-026	Information	
Horse Paddock	6-4-107	Information	text recommended
Horseback Riding	6-4-021	Information	
Horseback Riding, No	1-1-075	Prohibition	
Hospital	6-1-005	Information	
Hunting	6-4-020	Information	failed; retired
Hunting, No	1-1-053	Prohibition	
I			
Ice Bridges	3-1-174	Caution	failed; retired
Ice Fishing	6-4-316	Information	text recommended
Ice Hockey	6-4-308	Information	
Ice Skating	6-4-307	Information	
Ice Skating, No	1-1-058	Prohibition	

Symbol name	Code	Classification	Status
Ice/Snow on Trails	3-1-171	Caution	failed; use 3-1-002
Ignition, No	1-1-047	Prohibition	under development
Information	6-1-006	Information	
In-line Skating	6-4-105	Information	
In-line Skating, No	1-1-055	Prohibition	
Interpretation	6-1-080	Information	text recommended
Interpretation Centre	6-1-081	Information	text recommended
Interpretive Trail	6-4-027	Information	failed; retired
K			
Kayak Rental	6-2-016	Information	text recommended
Kayaking	6-4-215	Information	text recommended
Kayaking, No	1-1-064	Prohibition	
L			
Large Cats (generic)	3-1-153	Caution	text required
Laundry	6-2-017	Information	
Lawn Bowling	6-4-103	Information	text recommended
Lighthouse	6-1-058	Information	
Littering, No	1-1-079	Prohibition	
Locks	6-4-209	Information	under development
Lookout	6-4-008	Information	text recommended
Lost and Found	6-1-028	Information	failed; retired
M			
Marketplace	6-2-010	Information	
Meeting Room	6-1-031	Information	failed; retired
Metal Detectors, No	1-1-065	Prohibition	text required
Monument	6-1-086	Information	failed; retired
Mooring, No	1-1-076	Prohibition	under development
Moose	3-1-160	Caution	text required
Motorboating	6-4-212	Information	
Mountain Bikes	6-4-017	Information	retired
Museum	6-1-082	Information	failed; retired
N			
Nursery	6-1-050	Information	failed; retired
O			
Observation Tower	6-1-059	Information	text recommended
Open Flame, No	1-1-051	Prohibition	
Outhouse	6-1-065	Information	text recommended
Overhead Crane	3-1-003	Caution	under development

Symbol Inventory – Sorted by Name (CONTINUED)

Symbol name	Code	Classification	Status
Overnight Shelter	6-4-016	Information	failed; retired
P			
Paddling, No (generic)	1-1-080	Prohibition	
Park Bench	6-1-066	Information	
Parking	6-1-007	Information	
Parking Area 1	6-1-107	Information	
Parking Area 2	6-1-108	Information	
Parking Area 3	6-1-109	Information	
Parking Area 4	6-1-110	Information	
Parking Area 5	6-1-111	Information	
Parking Area 6	6-1-112	Information	
Parking Area 7	6-1-113	Information	
Parking Area 8	6-1-114	Information	
Parking Area 9	6-1-115	Information	
Parking, No	1-1-044	Prohibition	
Passenger Elevator	6-1-013	Information	text recommended
Pedestrian Access	6-1-068	Information	
Pedestrian Access, No	1-1-010	Prohibition	
Pedestrians or Bicycles, No	1-1-012	Prohibition	
Pick Up After Dogs	2-1-005	Mandatory	
Picnic Area	6-4-003	Information	
Picnic Shelter	6-4-004	Information	
Picnics, No	1-1-049	Prohibition	
Playground	6-4-007	Information	
Poison	4-1-003	Danger	text required
Poison Ivy	3-1-176	Caution	text required
Police	6-1-004	Information	
Portage	6-4-216	Information	text recommended
Postal Facilities	6-1-021	Information	
Protective Footwear	2-1-037	Mandatory	under development
Protective Gloves	2-1-036	Mandatory	under development
R			
Rail Transportation	6-3-007	Information	
Rapids/Falls	3-1-110	Caution	text required
Recreational Vehicles	6-4-013	Information	
Recreational Vehicle Parking	6-1-104	Information	
Recreational Vehicles, No	1-1-014	Prohibition	
Recyclable Material, No	1-1-067	Prohibition	failed; retired
Recycle - Cans	6-1-071	Information	
Recycle - Glass	6-1-072	Information	
Recycle - Organic (composting)	6-1-076	Information	
Recycle - Paper	6-1-073	Information	
Recycle - Plastic	6-1-074	Information	
Recycle - Styrofoam	6-1-075	Information	
Recycle Symbol	6-1-070	Information	
Recycling Bin	6-1-069	Information	
Restaurant	6-2-002	Information	
Rip Currents	3-1-102	Caution	failed; use 3-1-101
Rock Climbing	6-4-011	Information	failed; retired
Rock/Mountain Climbing	6-4-010	Information	
Rockfall	3-1-134	Caution	text required
Rogue Waves	3-1-111	Caution	text required
Rowboating	6-4-213	Information	failed; retired
S			
Safety Lane	3-1-001	Caution	failed; retired
Sailboating	6-4-211	Information	text recommended
Scuba Diving	6-4-204	Information	text recommended
Scuba Diving, No	1-1-061	Prohibition	
Security Post	6-1-012	Information	failed; retired
Services in Both Official Languages - English/ Français	6-1-029	Information	
Services in Both Official Languages - Français/ English	6-1-030	Information	
Shellfish - Poison	4-1-034	Danger	failed; use 4-1-003
Shower	6-1-045	Information	
Showers - Female	6-1-047	Information	
Showers - Male	6-1-046	Information	
Shut-off Valve	6-1-054	Information	under development
Sink Holes	3-1-104	Caution	failed; retired
Sink Holes	4-1-033	Danger	failed; retired
Skate-Skiing	6-4-311	Information	under development
Ski Jumping	6-4-306	Information	failed; retired
Slippery Conditions	3-1-002	Caution	text required
Slippery Rocks	3-1-105	Caution	failed; use 3-1-002
Small Airport	6-3-006	Information	failed; retired
Smoking Area	6-1-093	Information	
Smoking, No	1-1-041	Prohibition	
Snack Bar	6-2-008	Information	text recommended
Snakes (generic)	3-1-156	Caution	text required
Snow Bridges	3-1-175	Caution	failed; retired
Snowboarding	6-4-305	Information	text recommended

Symbol Inventory – Sorted by Name (CONTINUED)

Symbol name	Code	Classification	Status
Snowmobiles, No	1-1-009	Prohibition	
Snowmobiling	6-4-315	Information	
Snowshoeing	6-4-314	Information	
Stairs	6-1-015	Information	
Stairs Down	6-1-017	Information	failed; retired
Stairs Up	6-1-016	Information	failed; retired
Steep Drop-off	3-1-106	Caution	failed; retired
Steep Hill (cross-country skiing)	3-1-177	Caution	under development
Steep Hill (cross-country skiing)	6-4-312	Information	failed; retired
Steep Terrain	3-1-131	Caution	text required
Stretcher	5-1-007	Emergency	under development
Strong Currents	3-1-101	Caution	text required
Strong Winds	3-1-135	Caution	text required
Surge Channels	3-1-103	Caution	failed; retired
Sweepers	3-1-107	Caution	failed; retired
Swimming	6-4-202	Information	
Swimming, No	1-1-060	Prohibition	
T			
Take the Plants, Do Not	1-1-070	Prohibition	
Taxi Transportation	6-3-002	Information	
Telephone	6-1-011	Information	
Tennis	6-4-102	Information	
Theatre	6-1-083	Information	
Thin Ice	3-1-172	Caution	text required
Ticks	3-1-157	Caution	failed; retired
Tobogganing	6-4-309	Information	
Tobogganing, No	1-1-059	Prohibition	
Toilet for Men	6-1-001	Information	
Toilet for Men and Women	6-1-003	Information	
Toilet for Women	6-1-002	Information	
Touch, Do Not	1-1-043	Prohibition	failed; retired
Trailer Dumping	6-1-060	Information	
Trailer Parking	6-1-105	Information	
Trailers	6-4-015	Information	
Trailers, No	1-1-015	Prohibition	
Tree Cutting, No	1-1-069	Prohibition	
Tree Failure	3-1-136	Caution	text required
Trucks	6-1-010	Information	
Trucks, No	1-1-004	Prohibition	
TTY Telephone	6-1-041	Information	
Turtles	3-1-159	Caution	text required
U			
Unstable Ground	3-1-133	Caution	retired; use 3-1-002
V			
Vending Machine	6-2-007	Information	
W			
Wading, No	1-1-077	Prohibition	
Walk Bicycle	2-1-006	Mandatory	
Walk on Dunes, Do Not	1-1-068	Prohibition	failed; retired
Warden Station	6-1-057	Information	text required
Wasps	3-1-158	Caution	text required
Waste - Organic	6-1-077	Information	failed; retired
Waste Disposal	2-1-001	Mandatory	
Waste Disposal	6-1-024	Information	
Waterfall	6-4-222	Information	under development
Waterslide	6-4-206	Information	
Water-skiing	6-4-205	Information	
Water-skiing, No	1-1-074	Prohibition	
Water Transportation	6-3-010	information	text recommended
Way In	6-1-043	Information	failed; retired
Way Out	6-1-044	Information	failed; retired
Winter Warming Hut	6-4-301	Information	
Work in Progress	3-1-006	Caution	text required
Y			
Youth Hostel	6-2-004	Information	failed; retired

Symbol Inventory – Sorted by Code

Code	Symbol name	Status
	Prohibition symbols	
1-1-001	Dogs, No	
1-1-002	Buses, No	
1-1-003	Cars, No	
1-1-004	Trucks, No	
1-1-005	All-terrain Vehicles, No	
1-1-006	Dirt Bikes, No	
1-1-007	Boats, No	
1-1-008	Float Planes, No	
1-1-009	Snowmobiles, No	
1-1-010	Pedestrian Access, No	
1-1-011	Bicycles, No	
1-1-012	Pedestrians or Bicycles, No	
1-1-013	Cars or Bicycles, No	
1-1-014	Recreational Vehicles, No	
1-1-015	Trailers, No	
1-1-016	Bicycle Parking, No	
1-1-017	Boat Trailer Parking, No	
1-1-018	Entry, No	retired
1-1-019	Firewood, No	failed; retired
1-1-041	Smoking, No	
1-1-042	Drink, Do Not	
1-1-043	Touch, Do Not	failed; retired
1-1-044	Parking, No	
1-1-045	Boat Rafting, No	under development
1-1-046	Fishing, No	
1-1-047	Ignition, No	under development
1-1-048	Camping, No	
1-1-049	Picnics, No	
1-1-050	Alcohol, No	
1-1-051	Open Flames, No	
1-1-052	Campfires, No	
1-1-053	Hunting, No	
1-1-054	Backpacking/Hiking, No	
1-1-055	In-line Skating, No	
1-1-056	Downhill Skiing, No	
1-1-057	Cross-country Skiing, No	
1-1-058	Ice Skating, No	
1-1-059	Tobogganing, No	
1-1-060	Swimming, No	
1-1-061	Scuba Diving, No	
1-1-062	Diving, No	
1-1-063	Canoeing, No	
1-1-064	Kayaking, No	
1-1-065	Metal Detectors, No	text required
1-1-066	Car Washing, No	
1-1-067	Recyclable Material, No	failed; retired
1-1-068	Walk on Dunes, Do Not	failed; use 1-1-010
1-1-069	Tree Cutting, No	
1-1-070	Take the Plants, Do Not	
1-1-071	Feed the Birds, Do Not	
1-1-072	Feed the Elk, Do Not	failed; use 1-1-073
1-1-073	Feed the Animals (generic), Do Not	
1-1-074	Water-skiing, No	
1-1-075	Horseback Riding, No	
1-1-076	Mooring, No	under development
1-1-077	No Wading	
1-1-078	Feed the Fish, Do Not	
1-1-079	No Littering	
1-1-080	No Paddling (generic)	
	Mandatory symbols	
2-1-001	Waste Disposal	
2-1-002	Blower On	under development
2-1-003	Extinguish Fires	
2-1-004	Dogs on Leash	
2-1-005	Pick Up After Dogs	
2-1-006	Walk Bicycle	
2-1-031	Head Protection	under development
2-1-032	Eye Protection	under development
2-1-033	Hearing Protection	under development
2-1-034	Breathing Protection	under development
2-1-035	Hair Protection	under development
2-1-036	Protective Gloves	under development
2-1-037	Protective Footwear	under development
2-1-038	Fall Protection Equipment	under development

Symbol Inventory – Sorted by Code (CONTINUED)

Code	Symbol name	Status
	Caution symbols	
3-1-001	Safety Lane	failed; use MUTCD
3-1-002	Slippery Conditions	text required
3-1-003	Overhead Crane	under development
3-1-004	Be Alert!	text required
3-1-005	Caution, Presence of Ionizing Radiation	failed; retired
3-1-006	Work in Progress	text required
3-1-101	Strong Currents	text required
3-1-102	Rip Currents	failed; use 3-1-101
3-1-103	Surge Channels	failed; retired
3-1-104	Sink Holes	failed; retired
3-1-105	Slippery Rocks	failed; use 3-1-002
3-1-106	Steep Drop-off	failed; retired
3-1-107	Sweepers	failed; retired
3-1-108	Deadheads, Rocks	failed; retired
3-1-109	Beach Logs	failed; retired
3-1-110	Rapids/Falls	text required
3-1-111	Rogue Waves	text required
3-1-112	Cold Water	failed; use 3-1-137
3-1-131	Steep Terrain	text required
3-1-132	Gradual to Steep Drop-off	failed; use 3-1-131
3-1-133	Unstable Ground	failed; use 3-1-002
3-1-134	Rockfall	text required
3-1-135	Strong Winds	text required
3-1-136	Tree Failure	text required
3-1-137	Cold Temperatures (generic)	text required
3-1-151	Bears	text required
3-1-152	Bison	text required
3-1-153	Large Cats (generic)	text required
3-1-154	Canine (generic)	text required
3-1-155	Deer (generic)	text required
3-1-156	Snakes (generic)	text required
3-1-157	Ticks	failed; retired
3-1-158	Wasps	text required
3-1-159	Turtles	text required
3-1-160	Moose	text required
3-1-171	Ice/Snow on Trails	failed; use 3-1-002
3-1-172	Thin Ice	text required
3-1-173	Crevasses	failed; retired
3-1-174	Ice Bridges	failed; retired
3-1-175	Snow Bridges	failed; retired
3-1-176	Poison Ivy	text required
3-1-177	Steep Hill (cross-country skiing)	under development
	Danger symbols	
4-1-001	Flammable	under development
4-1-002	Electrical Hazard	text required
4-1-003	Poison	text required
4-1-004	Explosion Hazard	text required
4-1-005	Chemical Burn	under development
4-1-006	Biohazard	failed; retired
4-1-007	Compressed Gas	failed; retired
4-1-032	Avalanches	text required
4-1-033	Sink Holes	failed; retired
4-1-034	Shellfish - Poison	failed; use 4-1-003

Symbol Inventory – Sorted by Code (CONTINUED)

Code	Symbol name	Status
	Emergency symbols	
5-1-001	Fire Alarm	under development
5-1-002	Fire Extinguisher	
5-1-003	Fire Hose	
5-1-004	Fire Hydrant	
5-1-005	Fire Axe	under development
5-1-006	First Aid	
5-1-007	Stretcher	under development
5-1-008	Emergency Shower	under development
5-1-009	Emergency Eyewash	under development
5-1-010	Emergency Telephone	
5-1-011	Direction	
5-1-012	Direction	
5-1-013	Direction	
5-1-014	Direction	
5-1-015	Direction	
5-1-016	Direction	
5-1-017	Direction	
5-1-018	Direction	
	Guidance and information symbols	
6-1-001	Toilet for Men	
6-1-002	Toilet for Women	
6-1-003	Toilet for Men and Women	
6-1-004	Police	
6-1-005	Hospital	
6-1-006	Information	
6-1-007	Parking	
6-1-008	Buses	retired; use 6-3-001
6-1-009	Cars	text recommended
6-1-010	Trucks	
6-1-011	Telephone	
6-1-012	Security Post	failed; retired
6-1-013	Passenger Elevator	text recommended
6-1-014	Freight Elevator	text recommended
6-1-015	Stairs	
6-1-016	Stairs Up	failed; retired
6-1-017	Stairs Down	failed; retired
6-1-018	Escalator	
6-1-019	Escalator Up	retired
6-1-020	Escalator Down	retired
6-1-021	Postal Facilities	
6-1-022	Currency Exchange	
6-1-023	Cloakroom	
6-1-024	Waste Disposal	
6-1-025	Baggage	text recommended
6-1-026	Baggage Locker	
6-1-027	Baggage Cart	text recommended
6-1-028	Lost and Found	failed; retired
6-1-029	Services in Both Official Languages - English/Français	
6-1-030	Services in Both Official Languages - Français/English	
6-1-031	Meeting Room	failed; retired
6-1-032	Access for the Physically Impaired	
6-1-033	Accessible Toilet for Women	
6-1-034	Accessible Toilet for Men	
6-1-035	Accessible Telephone	
6-1-036	Accessible Elevator	text recommended
6-1-037	Accessible Ramp	failed; retired
6-1-038	Accessible Parking	
6-1-039	Access for the Hearing Impaired	
6-1-040	Hearing Impaired Telephone	
6-1-041	TTY Telephone	
6-1-042	Access for the Visually Impaired	
6-1-043	Way In	failed; retired
6-1-044	Way Out	failed; retired
6-1-045	Shower	
6-1-046	Showers - Male	
6-1-047	Showers - Female	
6-1-048	Change Rooms - Male	text recommended
6-1-049	Change Rooms - Female	text recommended
6-1-050	Nursery	failed; retired
6-1-051	Change Table	
6-1-052	Crafts	text recommended
6-1-053	Grey Water Disposal Drain	text recommended
6-1-054	Shut-off Valve	under development
6-1-055	Drinking Fountain	
6-1-056	Drinking Water	text recommended
6-1-057	Warden Station	text required
6-1-058	Lighthouse	
6-1-059	Observation Tower	
6-1-060	Trailer Dumping	
6-1-061	Boat Pump-out	under development
6-1-062	Electrical Hook-up	
6-1-063	Campfire	
6-1-064	Firewood	text required
6-1-065	Outhouse	

Symbol Inventory – Sorted by Code (CONTINUED)

Code	Symbol name	Status
6-1-066	Park Bench	
6-1-067	Dogs on Leash	
6-1-068	Pedestrian Access	
6-1-069	Recycling Bin	
6-1-070	Recycle Symbol	
6-1-071	Recycle – Cans	
6-1-072	Recycle – Glass	
6-1-073	Recycle – Paper	
6-1-074	Recycle – Plastic	
6-1-075	Recycle – Styrofoam	
6-1-076	Recycle – Organic (composting)	
6-1-077	Waste – Organic	failed; retired
6-1-078	Exhibit	
6-1-079	Guided Tour	failed; retired
6-1-080	Interpretation	text recommended
6-1-081	Interpretation Centre	text recommended
6-1-082	Museum	failed; retired
6-1-083	Theatre	
6-1-084	Amphitheatre	failed; retired
6-1-085	Archeological Site	failed; retired
6-1-086	Monument	failed; retired
6-1-087	Heritage River	failed; retired
6-1-088	Bison Viewing	
6-1-089	Golf Carts	
6-1-090	Boat Trailer Parking	text recommended
6-1-091	Car & Boat Trailer Parking	
6-1-092	Bus Parking	
6-1-093	Smoking Area	
6-1-094	Direction	
6-1-095	Direction	
6-1-096	Direction	
6-1-097	Direction	
6-1-098	Direction	
6-1-099	Direction	
6-1-100	Direction	
6-1-101	Direction	
6-1-102	Bicycle Parking	
6-1-103	Family Washroom	
6-1-104	Recreational Vehicle Parking	
6-1-105	Trailer Parking	text recommended
6-1-106	Accessible Shower	
6-1-107	Parking Area 1	
6-1-108	Parking Area 2	
6-1-109	Parking Area 3	

Code	Symbol name	Status
6-1-110	Parking Area 4	
6-1-111	Parking Area 5	
6-1-112	Parking Area 6	
6-1-113	Parking Area 7	
6-1-114	Parking Area 8	
6-1-115	Parking Area 9	
6-2-001	Coffee Shop	text recommended
6-2-002	Restaurant	
6-2-003	Accommodation	
6-2-004	Youth Hostel	failed; retired
6-2-005	Bar/Cocktails	
6-2-006	Cafeteria	failed; use 6-2-002
6-2-007	Vending Machine	
6-2-008	Snack Bar	text recommended
6-2-009	Groceries/Shopping	text recommended
6-2-010	Marketplace	
6-2-011	Gasoline Station	
6-2-012	Car Wash	
6-2-013	Car Rental	failed; retired
6-2-014	Bicycle Rental	
6-2-015	Canoe Rental	
6-2-016	Kayak Rental	text recommended
6-2-017	Laundry	
6-2-018	Fish Hatchery	failed; retired
6-2-019	Book Store	failed; retired
6-2-020	Gift Shop	
6-3-001	Bus Transportation	
6-3-002	Taxi Transportation	
6-3-003	Bus and Taxi Transportation	text recommended
6-3-004	Air Transportation	
6-3-005	Helicopter Transportation	
6-3-006	Small Airport	failed; retired
6-3-007	Rail Transportation	
6-3-008	Car Ferry	
6-3-009	Shuttle Bus Trolley	text recommended
6-3-010	Water Transportation	text recommended
6-4-001	Camping	
6-4-002	Group Camping	failed; use 6-4-001
6-4-003	Picnic Area	
6-4-004	Picnic Shelter	
6-4-005	Hiking	retire; use 6-4-006
6-4-006	Backpacking/Hiking	
6-4-007	Playground	
6-4-008	Lookout	

Symbol Inventory – Sorted by Code (CONTINUED)

Code	Symbol name	Status
6-4-009	Caving Area	
6-4-010	Rock/Mountain Climbing	
6-4-011	Rock Climbing	failed; retired
6-4-012	Bus Tour	failed; retired
6-4-013	Recreational Vehicles	
6-4-014	Camper Trucks	failed; use 6-4-013
6-4-015	Trailers	
6-4-016	Overnight Shelter	failed; retired
6-4-017	Mountain Bikes	retired
6-4-018	All-terrain Vehicles	
6-4-019	Dirt Bikes	
6-4-020	Hunting	failed; retired
6-4-021	Horseback Riding	
6-4-022	Backpacking/Horseback Riding	
6-4-023	Hiking/Bicycling	retired; use 6.4.024
6-4-024	Backpacking/ Bicycling	
6-4-025	Back-country Camping	
6-4-026	Horse Camping	
6-4-027	Interpretive Trail	failed; retired
6-4-101	Golf	
6-4-102	Tennis	
6-4-103	Lawn Bowling	text recommended
6-4-104	Bicycles	
6-4-105	In-line Skating	
6-4-106	Hitching Rail	text recommended
6-4-107	Horse Paddock	text recommended
6-4-201	Grey Water Disposal	failed; use 6-1-053
6-4-202	Swimming	
6-4-203	Diving	failed; retired
6-4-204	Scuba Diving	
6-4-205	Water-skiing	
6-4-206	Waterslide	

Code	Symbol name	Status
6-4-207	Fishing	
6-4-208	Boat Tour	failed; retired
6-4-209	Locks	under development
6-4-210	Anchorage	text recommended
6-4-211	Sailboating	text recommended
6-4-212	Motorboating	
6-4-213	Rowboating	failed; retired
6-4-214	Canoeing	
6-4-215	Kayaking	text recommended
6-4-216	Portage	text recommended
6-4-217	Boat Launch	
6-4-218	Docking	retired
6-4-219	Boat Tie-up	
6-4-220	Canoe Camping	text recommended
6-4-221	Beach (generic)	under development
6-4-222	Waterfall	under development
6-4-301	Winter Warming Hut	
6-4-302	Downhill Skiing	
6-4-303	Chairlift	
6-4-304	Gondola	
6-4-305	Snowboarding	
6-4-306	Ski Jumping	failed; retired
6-4-307	Ice Skating	
6-4-308	Ice Hockey	
6-4-309	Tobogganing	
6-4-310	Classic Skiing	
6-4-311	Skate-Skiing	under development
6-4-312	Steep Hill (cross-country skiing)	failed; retired
6-4-313	Dog Sled	
6-4-314	Snowshoeing	
6-4-315	Snowmobiling	
6-4-316	Ice Fishing	text recommended

Prohibition Symbols

Code	Name and description	Status	Symbol
1-1-001	**No Dogs** To indicate an area where dogs are prohibited.		
1-1-002	**No Buses** To indicate an area where buses are prohibited.		
1-1-003	**No Cars** To indicate an area where cars are prohibited.		
1-1-004	**No Trucks** To indicate an area where trucks are prohibited.		
1-1-005	**No All-terrain Vehicles** To indicate an area where all-terrain vehicles are prohibited.		
1-1-006	**No Dirt Bikes** To indicate an area where dirt bikes are prohibited.		
1-1-007	**No Boats** To indicate an area where boats are prohibited.		
1-1-008	**No Float Planes** To indicate an area where float planes are prohibited.		

Code	Name and description	Status	Symbol
1-1-009	**No Snowmobiles** To indicate an area where snowmobiles are prohibited.		
1-1-010	**No Pedestrian Access** To indicate an area where pedestrian access is prohibited.		
1-1-011	**No Bicycles** To indicate an area where bicycles are prohibited.		
1-1-012	**No Pedestrians or Bicycles** To indicate an area where pedestrians and bicycles are prohibited.		
1-1-013	**No Cars or Bicycles** To indicate an area where cars and bicycles are prohibited.		
1-1-014	**No Recreational Vehicles** To indicate an area where recreational vehicles are prohibited.		
1-1-015	**No Trailers** To indicate an area where trailers are prohibited.		
1-1-016	**No Bicycle Parking** To indicate an area where bicycle parking is prohibited.		

Prohibition Symbols (CONTINUED)

Code	Name and description	Status	Symbol
1-1-017	**No Boat Trailer Parking** To indicate an area where boat trailer parking is prohibited.		
1-1-018	**No Entry** To indicate an area where entry is prohibited.	Retired	
1-1-019	**No Firewood** To indicate an area where any transportation of firewood is prohibited.	Retired – failed comprehension testing	
1-1-041	**No Smoking** To indicate an area where smoking is prohibited.		
1-1-042	**Do Not Drink** To indicate an area where the (tap) water is not drinkable.		
1-1-043	**Do Not Touch** To indicate a surface that must not be touched.	Retired – failed comprehension testing	
1-1-044	**No Parking** To indicate an area where parking is prohibited.		
1-1-045	**No Boat Rafting** To indicate an area where boat rafting is prohibited.	Under development	

Code	Name and description	Status	Symbol
1-1-046	**No Fishing** To indicate an area where fishing is prohibited.		
1-1-047	**No Ignition** To indicate an area where motor ignition is prohibited.	Under development	
1-1-048	**No Camping** To indicate an area where camping is prohibited.		
1-1-049	**No Picnics** To indicate an area where picnics are prohibited.		
1-1-050	**No Alcohol** To indicate an area where the consumption of alcohol is prohibited.		
1-1-051	**No Open Flames** To indicate an area where open flames are prohibited.		
1-1-052	**No Campfires** To indicate an area where campfires are prohibited.		
1-1-053	**No Hunting** To indicate an area where hunting is prohibited.		

Prohibition Symbols (CONTINUED)

Code	Name and description	Status	Symbol
1-1-054	**No Backpacking/ Hiking** To indicate an area where backpacking and hiking is prohibited.		
1-1-055	**No In-line Skating** To indicate an area where in-line skating is prohibited.		
1-1-056	**No Downhill Skiing** To indicate an area where downhill skiing is prohibited.		
1-1-057	**No Cross-country Skiing** To indicate an area where cross-country skiing is prohibited.		
1-1-058	**No Ice Skating** To indicate an area where ice skating is prohibited.		
1-1-059	**No Tobogganing** To indicate an area where tobogganing is prohibited.		
1-1-060	**No Swimming** To indicate an area where swimming is prohibited.		
1-1-061	**No Scuba Diving** To indicate an area where scuba diving is prohibited.		

Code	Name and description	Status	Symbol
1-1-062	**No Diving** To indicate an area where diving is prohibited.		
1-1-063	**No Canoeing** To indicate an area where canoeing is prohibited.		
1-1-064	**No Kayaking** To indicate an area where kayaking is prohibited.		
1-1-065	**No Metal Detectors** To indicate an area where the use of metal detectors is prohibited.	Text message required	
1-1-066	**No Car Washing** To indicate an area where car washing is prohibited.		
1-1-067	**No Recyclable Material** To indicate a receptacle where recyclable material is prohibited.	Retired – failed comprehension testing	
1-1-068	**Do Not Walk on Dunes** To indicate an area where walking on the dunes is prohibited.	Retired – failed comprehension testing; use symbol 1-1-010 with specific text message	

Prohibition Symbols (CONTINUED)

Code	Name and description	Status	Symbol
1-1-069	**No Tree Cutting** To indicate an area where tree cutting is prohibited.		
1-1-070	**Do Not Take the Plants** To indicate an area where taking plant matter is prohibited.		
1-1-071	**Do Not Feed the Birds** To indicate an area where feeding birds is prohibited.		
1-1-072	**Do Not Feed the Elk** To indicate an area where feeding elk is prohibited.	Retired – failed comprehension testing; use symbol 1-1-073 with specific text message	
1-1-073	**Do Not Feed the Animals (generic)** To indicate an area where feeding animals is prohibited.		
1-1-074	**No Water-skiing** To indicate an area where water-skiing is prohibited.		

Code	Name and description	Status	Symbol
1-1-075	**No Horseback Riding** To indicate an area where horseback riding is prohibited.		
1-1-076	**No Mooring** To indicate an area where boat mooring or tie-up is prohibited.	Under development	
1-1-077	**No Wading** To indicate an area where wading is prohibited.		
1-1-078	**Do Not Feed the Fish** To indicate an area where feeding fish is prohibited.		
1-1-079	**No Littering** To indicate an area where littering is prohibited.		
1-1-080	**No Paddling (generic)** To indicate an area where paddling is prohibited.		

Mandatory Symbols

Code	Name and description	Status	Symbol
2-1-001	**Waste Disposal** To indicate the location of a receptacle into which waste must be deposited.		
2-1-002	**Blower On** To indicate an area where blowers must be used.	Under development	
2-1-003	**Extinguish Fires** To indicate an area where fires must be extinguished.		
2-1-004	**Dogs on Leash** To indicate an area where dogs must be kept on a leash.		
2-1-005	**Pick Up After Dogs** To indicate an area where the collection of dog waste is required.		
2-1-006	**Walk Bicycle** To indicate an area where bicycles must be dismounted and walked.		
2-1-031	**Head Protection** To indicate an area or specific activity where head protection must be worn.	Under development	

Code	Name and description	Status	Symbol
2-1-032	**Eye Protection** To indicate an area or specific activity where eye protection must be worn.	Under development	
2-1-033	**Hearing Protection** To indicate an area or specific activity where hearing protection must be worn.	Under development	
2-1-034	**Breathing Protection** To indicate an area or specific activity where breathing protection must be worn.	Under development	
2-1-035	**Hair Protection** To indicate an area or specific activity where hair protection must be worn.	Under development	
2-1-036	**Protective Gloves** To indicate an area or specific activity where protective gloves must be worn.	Under development	
2-1-037	**Protective Footwear** To indicate an area or specific activity where protective footwear must be worn.	Under development	
2-1-038	**Fall Protection Equipment** To indicate an area or specific activity where fall protection equipment must be worn.	Under development	

Caution Symbols

Code	Name and description	Status	Symbol
3-1-001	**Safety Lane** To indicate the location of a safety lane.	Retired – failed comprehension testing; use MUTCD crosswalk symbol	
3-1-002	**Slippery Conditions** To indicate the presence of slippery conditions.	Text message required	
3-1-003	**Overhead Crane** To indicate the potential hazard of an overhead crane.	Under development	
3-1-004	**Be Alert!** To indicate the presence of a potential hazard.	Text message required	
3-1-005	**Caution, Presence of Ionizing Radiation** To indicate the actual or potential presence of ionizing radiation.	Retired – failed comprehension testing	
3-1-006	**Work in Progress** To indicate the location of a work site.	Text message required	
3-1-101	**Strong Currents** To indicate the presence of a strong current hazard.	Text message required	
3-1-102	**Rip Currents** To indicate the presence of hazardous strong currents that may carry you away.	Retired – failed comprehension testing; use symbol 3-1-101 with specific text message	

Code	Name and description	Status	Symbol
3-1-103	**Surge Channels** To indicate the potential hazard of the water level rising suddenly.	Retired – failed comprehension testing	
3-1-104	**Sink Holes** To indicate the presence of a sink hole hazard.	Retired – failed comprehension testing	
3-1-105	**Slippery Rocks** To indicate the presence of a slippery rock hazard.	Retired – failed comprehension testing; use symbol 3-1-002 with specific text message	
3-1-106	**Steep Drop-off** To indicate an area where the bottom drops off suddenly.	Retired – failed comprehension testing	
3-1-107	**Sweepers** To indicate the presence of hazardous fallen trees in the water.	Retired – failed comprehension testing	
3-1-108	**Deadheads, Rocks** To indicate the presence of hazardous rocks and logs under water.	Retired – failed comprehension testing	
3-1-109	**Beach Logs** To indicate the potential hazard of logs moving in stormy weather.	Retired – failed comprehension testing	
3-1-110	**Rapids/Falls** To indicate the presence of hazardous falls or rapids ahead.	Text message required	

Caution Symbols (CONTINUED)

Code	Name and description	Status	Symbol
3-1-111	**Rogue Waves** To indicate an area where extra large waves occur.	Text message required	
3-1-112	**Cold Water** To indicate the presence of very cold water.	Retired – failed comprehension testing; use symbol 3-1-137 with specific text message	
3-1-131	**Steep Terrain** To indicate an area of steep terrain or dangerous cliffs.	Text message required	
3-1-132	**Gradual to Steep Drop-off** To indicate an area of gradual to steep drop-off.	Retired – failed comprehension testing; use symbol 3-1-131 with specific text message	
3-1-133	**Unstable Ground** To indicate the presence of slippery or unstable ground.	Retired – failed comprehension testing; use symbol 3-1-002 with specific text message	
3-1-134	**Rockfall** To indicate the presence of a falling rock hazard.	Text message required	
3-1-135	**Strong Winds** To indicate the presence of a strong wind hazard.	Text message required	
3-1-136	**Tree Failure** To indicate the presence of a falling tree or branch hazard.	Text message required	

Code	Name and description	Status	Symbol
3-1-137	**Cold Temperatures (generic)** To indicate the presence of a cold temperature hazard.	Text message required	
3-1-151	**Bears** To indicate the presence of a bear hazard.	Text message required	
3-1-152	**Bison** To indicate the presence of a bison hazard.	Text message required	
3-1-153	**Large Cats (generic)** To indicate the presence of a large cat hazard.	Text message required	
3-1-154	**Canine (generic)** To indicate the presence of a canine hazard.	Text message required	
3-1-155	**Deer (generic)** To indicate the presence of a deer hazard.	Text message required	
3-1-156	**Snakes (generic)** To indicate the presence of a snake hazard.	Text message required	
3-1-157	**Ticks** To indicate the presence of a tick hazard.	Retired – failed comprehension testing	

Caution Symbols (CONTINUED)

Code	Name and description	Status	Symbol
3-1-158	**Wasps** To indicate the presence of a wasp hazard.	Text message required	
3-1-159	**Turtles** To indicate the presence of a turtle hazard.	Text message required	
3-1-160	**Moose** To indicate the presence of a moose hazard.	Text message required	
3-1-171	**Ice/Snow on Trails** To indicate the presence of a slippery trail due to ice and/or snow.	Retired – failed comprehension testing; use symbol 3-1-002 with specific text message	
3-1-172	**Thin Ice** To indicate the presence of a thin ice hazard.	Text message required	

Code	Name and description	Status	Symbol
3-1-173	**Crevasses** To indicate the presence of a hidden crevasse hazard.	Retired – failed comprehension testing	
3-1-174	**Ice Bridges** To indicate the presence of an ice bridge hazard.	Retired – failed comprehension testing	
3-1-175	**Snow Bridges** To indicate the presence of a snow bridge hazard.	Retired – failed comprehension testing	
3-1-176	**Poison Ivy** To indicate the presence of a poison ivy hazard.	Text message required	
3-1-177	**Steep Hill (cross-country skiing)** To indicate the presence of a steep hill hazard.	Under development	

Danger Symbols

Code	Name and description	Status	Symbol
4-1-001	**Flammable** To indicate the presence of a flammable substance.	Under development	
4-1-002	**Electrical Hazard** To indicate the presence of an electrical hazard.	Text message required	
4-1-003	**Poison** To indicate the presence of a poisonous substance.	Text message required	
4-1-004	**Explosion Hazard** To indicate the presence of an explosion hazard.	Text message required	
4-1-005	**Chemical Burn** To indicate the presence of a highly corrosive chemical.	Under development	

Code	Name and description	Status	Symbol
4-1-006	**Biohazard** To indicate the presence of an infectious substance (e.g. viruses, bacteria, fungi or moulds).	Retired – failed comprehension testing	
4-1-007	**Compressed Gas** To indicate the presence of compressed gas.	Retired – failed comprehension testing	
4-1-032	**Avalanches** To indicate the presence of an avalanche hazard.	Text message required	
4-1-033	**Sink Holes** To indicate the presence of a sink hole hazard.	Retired – failed comprehension testing	
4-1-034	**Shellfish - Poison** To indicate the presence of poisonous shellfish.	Retired – failed comprehension testing; use symbol 4-1-003 with specific text message	

Emergency Symbols

Code	Name and description	Status	Symbol
5-1-001	**Fire Alarm** To indicate and identify a device that initiates a fire alarm and emits an acoustic and/or visual alarm and/or notification of fire.	Under development	
5-1-002	**Fire Extinguisher** To indicate the location of a fire extinguisher.		
5-1-003	**Fire Hose** To indicate the location of a fire hose (standpipe).		
5-1-004	**Fire Hydrant** To indicate the location of a fire hydrant.		
5-1-005	**Fire Axe** To indicate the location of a fire axe.	Under development	
5-1-006	**First Aid** To indicate the location of a facility providing first aid (e.g. a first aid station or a first aid kit).		
5-1-007	**Stretcher** To indicate the location of a stretcher.	Under development	

Code	Name and description	Status	Symbol
5-1-008	**Emergency Shower** To indicate the location of an emergency shower.	Under development	
5-1-009	**Emergency Eyewash** To indicate the location of an emergency eyewash facility.	Under development	
5-1-010	**Emergency Telephone** To indicate the location of an emergency telephone.		
5-1-011	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other “Emergency” symbols.		
5-1-012	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other “Emergency” symbols.		
5-1-013	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other “Emergency” symbols.		
5-1-014	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other “Emergency” symbols.		

Emergency Symbols (CONTINUED)

Code	Name and description	Status	Symbol
5-1-015	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other "Emergency" symbols.		
5-1-016	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other "Emergency" symbols.		

Code	Name and description	Status	Symbol
5-1-017	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other "Emergency" symbols.		
5-1-018	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other "Emergency" symbols.		

Guidance and Information Symbols

Code	Name and description	Status	Symbol
6-1-001	**Toilet for Men** To indicate the location of a toilet intended for men only.		
6-1-002	**Toilet for Women** To indicate the location of a toilet intended for women only.		
6-1-003	**Toilet for Men and Women** To indicate a facility which is intended for men and women. This symbol may also serve as an advance sign to indicate the location of separate facilities.		
6-1-004	**Police** To indicate the location of a police force or security staff.		
6-1-005	**Hospital** To indicate the location of a hospital.		
6-1-006	**Information** To indicate the location of a facility or service that provides information.		
6-1-007	**Parking** To indicate an area where vehicles may be parked.		

Code	Name and description	Status	Symbol
6-1-008	**Buses** To indicate a facility specifically designed (or intended) for buses.	Retired – failed comprehension testing; use symbol 6-3-001	
6-1-009	**Cars** To indicate a facility specifically designed (or intended) for cars.	Text message recommended	
6-1-010	**Trucks** To indicate a facility specifically designed (or intended) for trucks.		
6-1-011	**Telephone** To indicate the location of a telephone.		
6-1-012	**Security Post** To indicate the location of a security post.	Retired – failed comprehension testing	
6-1-013	**Passenger Elevator** To indicate the location of a passenger elevator.	Text message recommended	
6-1-014	**Freight Elevator** To indicate the location of a freight elevator.	Text message recommended	
6-1-015	**Stairs** To indicate the location of stairs.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-1-016	**Stairs Up** To indicate the location of stairs that lead to an upper level.	Retired – failed comprehension testing	
6-1-017	**Stairs Down** To indicate the location of stairs that lead to a lower level.	Retired – failed comprehension testing	
6-1-018	**Escalator** To indicate the location of escalators which provide transportation to an upper or lower level.		
6-1-019	**Escalator Up** To indicate the location of escalators which provide transportation to an upper level.	Retired – failed comprehension testing	
6-1-020	**Escalator Down** To indicate the location of escalators which provide transportation to a lower level.	Retired – failed comprehension testing	
6-1-021	**Postal Facilities** To indicate the location of a facility which provides postal service (i.e. any type of facility from a post office to a mail box).		
6-1-022	**Currency Exchange** To indicate the location of a facility where currency may be exchanged.		
6-1-023	**Cloakroom** To indicate the location where clothing may be kept.	Text message recommended	

Code	Name and description	Status	Symbol
6-1-024	**Waste Disposal** To indicate the location of a receptacle for the disposal of waste.		
6-1-025	**Baggage** To indicate the location of baggage-related functions.	Text message recommended	
6-1-026	**Baggage Locker** To indicate the location where lockable space for baggage is for rent.		
6-1-027	**Baggage Cart** To indicate the location where baggage carts are available.	Text message recommended	
6-1-028	**Lost and Found** To indicate the location of a lost and found.	Retired – failed comprehension testing	
6-1-029	**Services in Both Official Languages - English/Français** To indicate the availability of government services in both official languages.		
6-1-030	**Services in Both Official Languages - Français/English** To indicate the availability of government services in both official languages.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-1-031	**Meeting Room** To indicate the location of meeting room facilities.	Retired – failed comprehension testing	
6-1-032	**Access for the Physically Impaired** To indicate a facility accessible to, usable by, or intended for persons with mobility impairments.		
6-1-033	**Accessible Toilet for Women** To indicate a toilet that is accessible to, usable by, or intended for women whose mobility is restricted by a physical handicap.		
6-1-034	**Accessible Toilet for Men** To indicate a toilet that is accessible to, usable by, or intended for men whose mobility is restricted by a physical handicap.		
6-1-035	**Accessible Telephone** To indicate the location of a public telephone accessible by people with disabilities.		
6-1-036	**Accessible Elevator** To indicate the location of an elevator for people with disabilities.	Text message recommended	
6-1-037	**Accessible Ramp** To indicate the location of a ramp for people with disabilities.	Retired – failed comprehension testing	
6-1-038	**Accessible Parking** To indicate a parking area for people with disabilities.		

Code	Name and description	Status	Symbol
6-1-039	**Access for the Hearing Impaired** To indicate a facility or service that is equipped to serve people with a hearing impairment.		
6-1-040	**Hearing Impaired Telephone** To indicate the location of a public telephone with functions for people with a hearing impairment.		
6-1-041	**TTY Telephone** To indicate the location of a text telephone device for people with a hearing impairment.		
6-1-042	**Access for the Visually Impaired** To indicate the location of services for people with a visual impairment.		
6-1-043	**Way In** To indicate the location of an entrance or preferred way to enter.	Retired – failed comprehension testing	
6-1-044	**Way Out** To indicate the location of an exit or preferred way to go out.	Retired – failed comprehension testing	
6-1-045	**Shower** To indicate the location of a shower.		
6-1-046	**Showers - Male** To indicate the location of showers intended for men only.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-1-047	**Showers - Female** To indicate the location of showers intended for women only.		
6-1-048	**Change Rooms - Male** To indicate the location of a change room intended for men only.	Text message recommended	
6-1-049	**Change Rooms - Female** To indicate the location of a change room intended for women only.	Text message recommended	
6-1-050	**Nursery** To indicate the location of a nursery or child care.	Retired – failed comprehension testing	
6-1-051	**Change Table** To indicate the location of a diaper changing station.		
6-1-052	**Crafts** To indicate the location of a crafts area.	Text message recommended	
6-1-053	**Grey Water Disposal Drain** To indicate the location of a grey water disposal drain.	Text message recommended	
6-1-054	**Shut-off Valve** To indicate the location of a shut-off valve.	Under development	

Code	Name and description	Status	Symbol
6-1-055	**Drinking Fountain** To indicate the location of a drinking fountain.		
6-1-056	**Drinking Water** To signify drinkable (tap) water.	Text message recommended	
6-1-057	**Warden Station** To indicate the location of a park warden station.	Text message required	
6-1-058	**Lighthouse** To indicate the location of a lighthouse.		
6-1-059	**Observation Tower** To indicate the location of an observation tower.	Text message recommended	
6-1-060	**Trailer Dumping** To indicate the location of a trailer dumping area.		
6-1-061	**Boat Pump-out** To indicate the location of a boat pump-out.	Under development	
6-1-062	**Electrical Hook-up** To indicate the location of an electrical hook-up.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-1-063	**Campfire** To indicate an area where campfires are permitted.		
6-1-064	**Firewood** To indicate the location of firewood.	Text message required	
6-1-065	**Outhouse** To indicate the location of an outhouse.	Text message recommended	
6-1-066	**Park Bench** To indicate the location of a park bench.		
6-1-067	**Dogs on Leash** To indicate an area where dogs on leash are permitted.		
6-1-068	**Pedestrian Access** To indicate the location of pedestrian access.		
6-1-069	**Recycling Bin** To indicate the location of a recycling receptacle.		
6-1-070	**Recycle Symbol** To indicate the location of recycling facilities.		

Code	Name and description	Status	Symbol
6-1-071	**Recycle - Cans** To indicate the location of a can recycling receptacle.		
6-1-072	**Recycle - Glass** To indicate the location of a glass recycling receptacle.		
6-1-073	**Recycle - Paper** To indicate the location of a paper recycling receptacle.		
6-1-074	**Recycle - Plastic** To indicate the location of a plastic recycling receptacle.		
6-1-075	**Recycle - Styrofoam** To indicate the location of a styrofoam recycling receptacle.		
6-1-076	**Recycle - Organic (composting)** To indicate the location of a composting receptacle.		
6-1-077	**Waste - Organic** To indicate the location of an organic waste receptacle.	Retired – failed comprehension testing	
6-1-078	**Exhibit** To indicate the location of an exhibit.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-1-079	**Guided Tour** To indicate the location of guided tours.	Retired – failed comprehension testing	
6-1-080	**Interpretation** To indicate the area where activities such as interpretation, a guided tour or lecture, or an amphitheatre are located.	Text message recommended	
6-1-081	**Interpretation Centre** To indicate the location of an interpretation centre.	Text message recommended	
6-1-082	**Museum** To indicate the location of a museum.	Retired – failed comprehension testing	
6-1-083	**Theatre** To indicate the location of a theatre.		
6-1-084	**Amphitheatre** To indicate the location of an amphitheatre.	Retired – failed comprehension testing	
6-1-085	**Archaeological Site** To indicate the location of an archaeological site.	Retired – failed comprehension testing	
6-1-086	**Monument** To indicate the location of a monument or plaque.	Retired – failed comprehension testing	

Code	Name and description	Status	Symbol
6-1-087	**Heritage River** To indicate the location of a heritage river route.	Retired – failed comprehension testing	
6-1-088	**Bison Viewing** To indicate an area where bison may be viewed.		
6-1-089	**Golf Carts** To indicate an area where golf carts are permitted.		
6-1-090	**Boat Trailer Parking** To indicate an area where boat trailers may be parked.	Text message recommended	
6-1-091	**Car & Boat Trailer Parking** To indicate an area where car and boat trailers may be parked.		
6-1-092	**Bus Parking** To indicate an area where buses may be parked.		
6-1-093	**Smoking Area** To indicate an area where smoking is permitted.		
6-1-094	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other “Guidance and Information” symbols.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-1-095	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other "Guidance and Information" symbols.		
6-1-096	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other "Guidance and Information" symbols.		
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6-1-101	**Direction** Indicating direction, this symbol may be used only in conjunction with one of the other "Guidance and Information" symbols.		
6-1-102	**Bicycle Parking** To indicate an area where bicycles may be parked.		

Code	Name and description	Status	Symbol
6-1-103	**Family Washroom** To indicate the location of a family washroom.		
6-1-104	**Recreational Vehicle Parking** To indicate an area where recreational vehicles may be parked.		
6-1-105	**Trailer Parking** To indicate an area where trailers may be parked.	Text message recommended	
6-1-106	**Accessible Shower** To indicate a shower facility accessible to, usable by, or intended for persons with mobility impairments.		
6-1-107	**Parking Area 1** To indicate an area where vehicles may be parked when multiple areas are involved.		
6-1-108	**Parking Area 2** To indicate an area where vehicles may be parked when multiple areas are involved.		
6-1-109	**Parking Area 3** To indicate an area where vehicles may be parked when multiple areas are involved.		
6-1-110	**Parking Area 4** To indicate an area where vehicles may be parked when multiple areas are involved.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-1-111	**Parking Area 5** To indicate an area where vehicles may be parked when multiple areas are involved.		
6-1-112	**Parking Area 6** To indicate an area where vehicles may be parked when multiple areas are involved.		
6-1-113	**Parking Area 7** To indicate an area where vehicles may be parked when multiple areas are involved.		
6-1-114	**Parking Area 8** To indicate an area where vehicles may be parked when multiple areas are involved.		
6-1-115	**Parking Area 9** To indicate an area where vehicles may be parked when multiple areas are involved.		
6-2-001	**Coffee Shop** To indicate the location of a coffee shop.	Text message recommended	
6-2-002	**Restaurant** To indicate the location of a full-service restaurant.		
6-2-003	**Accommodation** To indicate the location of accommodation.		

Code	Name and description	Status	Symbol
6-2-004	**Youth Hostel** To indicate the location of a youth hostel.	Retired – failed comprehension testing	
6-2-005	**Bar/Cocktails** To indicate the location of a facility serving alcohol.		
6-2-006	**Cafeteria** To indicate the location of a cafeteria.	Retired – failed comprehension testing; use symbol 6-2-002	
6-2-007	**Vending Machine** To indicate the location of a vending machine.		
6-2-008	**Snack Bar** To indicate the location of a snack bar.	Text message recommended	
6-2-009	**Groceries/Shopping** To indicate the location of a grocery store.	Text message recommended	
6-2-010	**Marketplace** To indicate the location of a marketplace.		
6-2-011	**Gasoline Station** To indicate the location where motor vehicals may be refueled.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-2-012	**Car Wash** To indicate the location of a car wash.		
6-2-013	**Car Rental** To indicate the location of car rental facilities.	Retired – failed comprehension testing	
6-2-014	**Bicycle Rental** To indicate the location of bicycle rental facilities.		
6-2-015	**Canoe Rental** To indicate the location of canoe rental facilities.		
6-2-016	**Kayak Rental** To indicate the location of kayak rental facilities.	Text message recommended	
6-2-017	**Laundry** To indicate the location of laundry facilities.		
6-2-018	**Fish Hatchery** To indicate the location of a fish hatchery.	Retired – failed comprehension testing	
6-2-019	**Book Store** To indicate the location of a book store.	Retired – failed comprehension testing; use symbol 6-2-020	

Code	Name and description	Status	Symbol
6-2-020	**Gift Shop** To indicate the location of a gift shop.		
6-3-001	**Bus Transportation** To indicate an area where transportation by bus is available.		
6-3-002	**Taxi Transportation** To indicate an area where transportation by taxi is available (e.g. a taxi stand).		
6-3-003	**Bus and Taxi Transportation** To indicate an area where transportation by bus and taxi is available.	Text message recommended	
6-3-004	**Air Transportation** To indicate the location of a facility where transportation by air is available.		
6-3-005	**Helicopter Transportation** To indicate the location of a facility where transportation by helicopter is available.		
6-3-006	**Small Airport** To indicate the location of a small airport.	Retired – failed comprehension testing	
6-3-007	**Rail Transportation** To indicate the location of a facility where transportation by rail is available.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-3-008	**Car Ferry** To indicate the location of a car ferry.		
6-3-009	**Shuttle Bus Trolley** To indicate the location of a passenger shuttle.	Text message recommended	
6-3-010	**Water Transportation** To indicate the location of water transportation.	Text message recommended	
6-4-001	**Camping** To indicate an area where camping is permitted.		
6-4-002	**Group Camping** To indicate an area where group camping is permitted.	Retired – failed comprehension testing; use symbol 6-4-001 with specific text message	
6-4-003	**Picnic Area** To indicate an area where picnic tables are available.		
6-4-004	**Picnic Shelter** To indicate the location of a picnic shelter.		
6-4-005	**Hiking** To indicate the location of a hiking trail.	Retired – failed comprehension testing; use symbol 6-4-006	

Code	Name and description	Status	Symbol
6-4-006	**Backpacking/Hiking** To indicate the location of a backpacking or hiking trail.		
6-4-007	**Playground** To indicate the location of a playground.		
6-4-008	**Lookout** To indicate the location of a lookout area.		
6-4-009	**Caving Area** To indicate the location of a caving area.	Text message recommended	
6-4-010	**Rock/Mountain Climbing** To indicate the location of a mountain or rock climbing area.		
6-4-011	**Rock Climbing** To indicate the location of a rock climbing area.	Retired – failed comprehension testing	
6-4-012	**Bus Tour** To indicate the location of a bus tour area.	Retired – failed comprehension testing	
6-4-013	**Recreational Vehicles** To indicate the location of a recreational vehicle area.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-4-014	**Camper Trucks** To indicate the location of a camper truck area.	Retired – failed comprehension testing; use symbol 6-4-013	
6-4-015	**Trailers** To indicate the location of a trailer area.		
6-4-016	**Overnight Shelter** To indicate the location of an overnight shelter.	Retired – failed comprehension testing	
6-4-017	**Mountain Bikes** To indicate the location of a mountain bike trail.	Retired – failed comprehension testing	
6-4-018	**All-terrain Vehicles** To indicate the location of an all-terrain vehicle trail.		
6-4-019	**Dirt Bikes** To indicate the location of a dirt bike trail.		
6-4-020	**Hunting** To indicate the location of a hunting area.	Retired – failed comprehension testing	
6-4-021	**Horseback Riding** To indicate the location of a horseback riding area.		

Code	Name and description	Status	Symbol
6-4-022	**Backpacking/ Horseback Riding** To indicate the location of a shared backpacking and horseback riding trail.		
6-4-023	**Hiking/Bicycling** To indicate the location of a shared hiking and bicycling trail.	Retired – failed comprehension testing; use symbol 6-4-024	
6-4-024	**Backpacking/ Bicycling** To indicate the location of a shared backpacking and bicycling trail.		
6-4-025	**Back-country Camping** To indicate the location of a camping area accessible by backpacking only.		
6-4-026	**Horse Camping** To indicate the location of a camping area accesssible by horseback only.		
6-4-027	**Interpretive Trail** To indicate the location of an interpretive trail.	Retired – failed comprehension testing	
6-4-101	**Golf** To indicate the location of a golf area.		
6-4-102	**Tennis** To indicate the location of a tennis court.		

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-4-103	**Lawn Bowling** To indicate the location of a lawn bowling green.	Text message recommended	
6-4-104	**Bicycles** To indicate the location of a bicycle area.		
6-4-105	**In-line Skating** To indicate the location of an area where in-line skating is permitted.		
6-4-106	**Hitching Rail** To indicate the location of an area for horse tie-up.	Text message recommended	
6-4-107	**Horse Paddock** To indicate the location of a horse paddock.	Text message recommended	
6-4-201	**Grey Water Disposal** To indicate the location of a grey water disposal area.	Retired – failed comprehension testing; use symbol 6-1-053	
6-4-202	**Swimming** To indicate the location of a swimming area.		
6-4-203	**Diving** To indicate the location of a diving area.	Retired – failed comprehension testing	

Code	Name and description	Status	Symbol
6-4-204	**Scuba Diving** To indicate the location of a scuba diving area.	Text message recommended	
6-4-205	**Water-skiing** To indicate the location of a water skiing area.		
6-4-206	**Waterslide** To indicate the location of a waterslide.		
6-4-207	**Fishing** To indicate the location of a fishing area.		
6-4-208	**Boat Tour** To indicate the location of a boat tour.	Retired – failed comprehension testing	
6-4-209	**Locks** To indicate the location of locks.	Under development	
6-4-210	**Anchorage** To indicate the location of an anchorage area.	Text message recommended	
6-4-211	**Sailboating** To indicate the location of an area where sailboating is permitted.	Text message recommended	

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-4-212	**Motorboating** To indicate the location of an area where motorboating is permitted.		
6-4-213	**Rowboating** To indicate the location of an area where rowboating is permitted.	Retired – failed comprehension testing	
6-4-214	**Canoeing** To indicate the location of an area where canoeing is permitted.		
6-4-215	**Kayaking** To indicate the location of an area where kayaking is permitted.	Text message recommended	
6-4-216	**Portage** To indicate the location of a portaging area.	Text message recommended	
6-4-217	**Boat Launch** To indicate the location of a boat launching area.		
6-4-218	**Docking** To indicate the location of a docking area.	Retired	
6-4-219	**Boat Tie-up** To indicate the location of a boat tie-up area.		

Code	Name and description	Status	Symbol
6-4-220	**Canoe Camping** To indicate the location of a camping area accessible by canoe only.	Text message recommended	
6-4-221	**Beach (generic)** To indicate the location of a shore of the sea.	Under development	
6-4-222	**Waterfall** To indicate the location of a waterfall.	Under development	
6-4-301	**Winter Warming Hut** To indicate the location of a winter warming hut.		
6-4-302	**Downhill Skiing** To indicate the location of a downhill skiing area.		
6-4-303	**Chairlift** To indicate the location of a chairlift.		
6-4-304	**Gondola** To indicate the location of a gondola.		
6-4-305	**Snowboarding** To indicate the location of a snowboarding area.	Text message recommended	

Guidance and Information Symbols (CONTINUED)

Code	Name and description	Status	Symbol
6-4-306	**Ski Jumping** To indicate the location of a ski jumping area.	Retired – failed comprehension testing	
6-4-307	**Ice Skating** To indicate the location of an ice skating rink.		
6-4-308	**Ice Hockey** To indicate the location of an ice hockey area.		
6-4-309	**Tobogganing** To indicate the location of a tobogganing area.		
6-4-310	**Classic Skiing** To indicate the location of a classic-style skiing area.		
6-4-311	**Skate-Skiing** To indicate the location of a skate-skiing area.	Under development	

Code	Name and description	Status	Symbol
6-4-312	**Steep Hill (cross-country skiing)** To indicate the location of a steep hill for cross-country skiing.	Retired – failed comprehension testing	
6-4-313	**Dog Sled** To indicate the location of a dog sledding area.		
6-4-314	**Snowshoeing** To indicate the location of a snowshoeing area.		
6-4-315	**Snowmobiling** To indicate the location of a snowmobiling area.		
6-4-316	**Ice Fishing** To indicate the location of an ice fishing area.	Text message recommended	

B

Appendix B – Standard Signage Messages

Appendix B – Standard Signage Messages

Messages in Both Official Languages 2

Messages in Both Official Languages

A/V Production Room	Salle de production audiovisuelle
Access for the hearing impaired	Accès, déficience auditive
Access for the physically handicapped	Accès pour les handicapés physiques
Access restricted Obtain permits from... (Park Office, Warden's Office, Information Centre, etc.)	**Accès limité** Obtenir un permis... (au bureau du parc, au bureau du garde, au centre d'information, etc.)
Administration and maintenance personnel only	Réservé au personnel d'administration et d'entretien
Administration Building	Administration
Admittance on business only	Entrée par affaires seulement
Air patrol	Surveillance aérienne
Air transportation	Transport par avion
All digging prohibited	Défense de creuser
All garbage must be packed out	Rapportez vos déchets
All sales are final	Aucun remboursement ou échange
Amphitheatre	Amphithéâtre
Angle parking only	Stationnement en oblique
Area closed	Zone fermée
Area closed Restoration site	**Zone fermée** Travaux de restauration
Area temporarily closed to allow vegetation to recover	Zone fermée temporairement pour permettre la régénération de la végétation
Articles left at owner's risk	Nous ne sommes pas responsables des objets perdus ou volés
Artisan Shop	Atelier des artisans
Arts and Crafts School	École d'artisanat
Attention Proceed at your own risk	**Attention** Circulez à vos risques
Authorized personnel only	Réservé au personnel autorisé
Authorized persons only	Réservé aux personnes autorisées
Authorized vehicles only	Réservé aux véhicules autorisés
Avalanche area Do not stop	**Zone d'avalanche** Arrêt interdit
Baggage cart	Chariot à bagages
Baggage locker	Consigne à bagages
Barricade ahead	Barrage routier
Be careful	Prudence
Be prepared to stop	Préparez-vous à arrêter
Bear hazard Trail closed	**Zone fréquentée par les ours** Sentier fermé
Bison paddock	Enclos de bisons
Bison paddock No admittance	**Enclos de bisons** Accès interdit
Blacksmith Shop	Forge

Blasting zone	Zone de dynamitage
Board Room	Salle de conférence
Boardwalk slippery when wet	Passerelle glissante lorsque mouillée
Boat launching ramp No water-skiing	**Rampe de mise à l'eau** Ski nautique interdit
Boat tours Loading area	**Excursions en bateau** Embarcadère
Boat trailer parking area	Stationnement de remorques pour embarcations
Boil water	Faire bouillir l'eau
Breathing protection must be worn	Masque respiratoire obligatoire
Bridge Maximum 9 tons	**Pont** Maximum 9 tonnes
Bridge clearance: XX m	Hauteur libre : XX m
Bridge closed	Pont fermé
Bridge unsafe	Pont dangereux
Bus	Autocar
Buses – Trailers – Cars	Autocars – Caravanes – Voitures
Buses and taxis only	Réservé aux autocars et aux taxis
Buses prohibited	Interdit aux autocars
Cafeteria	Cafétéria
Camper traffic only	Accès limité aux camionnettes de camping
Campground gates closed from midnight to 06:00	Barrières du terrain de camping fermées de minuit à 6 h
Camping area	Aire de camping
Camping area No picnicking	**Aire de camping** Défense de pique-niquer
Camping by reservation only	Camping sur réservation seulement
Camping prohibited	Camping interdit
Canal banks unstable Proceed at your own risk	**Berges instables** Circulez à vos risques
Canteen	Cantine
Car	Voiture
Car rental	Location de voitures
Car washing prohibited	Lavage de voitures interdit
Carpentry Shop	Atelier de menuiserie
Caution! Area used as a shooting range	**Attention!** Secteur utilisé comme champ de tir
Caution! Attendant's visibility restricted by fog Use care!	**Attention!** Le brouillard peut restreindre la visibilité du préposé Soyez prudent!
Caution! Be alert!	**Attention!** Soyez vigilant!
Caution! Do not drink the water	**Attention!** Eau non potable

Messages in Both Official Languages (CONTINUED)

Caution! Golf cart crossing	**Attention!** Traverse de voiturettes de golf
Caution! Hazardous area Undercut banks	**Attention!** Zone dangereuse Berges instables
Caution! Highway crossing	**Attention!** Croisement avec une route principale
Caution! Low ceiling	**Attention!** Plafond bas
Caution! Microwaves	**Attention!** Micro-ondes
Caution! Narrow road	**Attention!** Chaussée étroite
Caution! Open slowly	**Attention!** Ouvrir avec soin
Caution! Pedestrian crosswalk	**Attention!** Passage pour piétons
Caution! Ski run Yield to skiers	**Attention!** Piste de ski Céder le passage aux skieurs
Caution! Slippery conditions	**Attention!** Conditions glissantes
Caution! Use handrail	**Attention!** Tenir la rampe
Caution! Wildlife crossing	**Attention!** Passage d'animaux sauvages
Central Pump House	Poste central de pompage
Change Rooms	Vestiaires
Charcoal only	Charbon de bois seulement
Check-out time: 14:00	Heure de départ : 14 h
Children only	Réservé aux enfants
Climbing prohibited	Défense d'escalader
Climbing route only	Réservé aux alpinistes
Cloakroom	Vestiaire
Closed	Fermé
Closed Avalanche danger	**Fermé** Risque d'avalanche
Closed Fire trail	**Fermé** Sentier coupe-feu
Closed for the season	Fermé pour la saison
Closed November 1 to April 30	Fermé du 1er novembre au 30 avril
Closed on Saturdays and Sundays	Fermé le samedi et le dimanche
Closed on Wednesdays and Thursdays	Fermé le mercredi et le jeudi
Closed to fishing	Fermé à la pêche
Closed to motor vehicles to allow ground cover regeneration	Interdit aux véhicules automobiles pour permettre la régénération de la couverture végétale
Closed until the end of construction	Fermé pour la durée des travaux
Computer Room	Salle d'ordinateurs
Conference Room	Salle de conférence
Construction	Travaux
Construction site No admittance	**Chantier** Accès interdit
Controlled access Visitors must report to security	**Accès contrôlé** Les visiteurs doivent s'adresser au service de sécurité
Courtroom	Salle d'audience
Cross-country ski trails closed from Monday to Friday	Pistes de ski de fond fermées du lundi au vendredi
Currency exchange	Change de devises
Danger! Area closed to the public during reconstruction	**Danger!** Zone interdite au public pendant les travaux de réfection
Danger! Bank eroding Stay behind barrier	**Danger!** Berge en érosion Ne pas franchir la barrière
Danger! Bear trap	**Danger!** Piège à ours
Danger! Broken pavement	**Danger!** Chaussée défoncée
Danger! Chemical burn	**Danger!** Brûlure chimique
Danger! Deep water	**Danger!** Eau profonde
Danger! Do not enter mine	**Danger!** Accès à la mine interdit
Danger! Drop-off	**Danger!** Dénivellation abrupte
Danger! Electrical hazard	**Danger!** Ligne sous tension
Danger! Erosion	**Danger!** Érosion
Danger! Explosion hazard	**Danger!** Risque d'explosion
Danger! Flammable	**Danger!** Matières inflammables
Danger! Keep away	**Danger!** Ne pas s'approcher
Danger! Keep hands off	**Danger!** Ne pas toucher
Danger! Live bears	**Danger!** Attention aux ours
Danger! Men at work	**Danger!** Hommes au travail
Danger! No climbing	**Danger!** Défense d'escalader
Danger! No running	**Danger!** Défense de courir
Danger! Quicksand	**Danger!** Sables mouvants
Danger! Soft ground	**Danger!** Sol meuble

Messages in Both Official Languages (CONTINUED)

Danger! Thin ice	**Danger!** Glace mince
Dangerous waters Water activities not advisable	**Eaux dangereuses** Activités nautiques déconseillées
Darkroom	Chambre noire
Deliveries at rear	Livraison à l'arrière
Depth	Profondeur
Dim lights	Baissez vos phares
Dining Room	Salle à manger
Distance shown in kilometers	Distance indiquée en kilomètres
Do not camp in the picnic area	Défense de camper dans l'aire de pique-nique
Do not climb tower	Défense de monter dans la tour
Do not cross the barrier	Ne pas franchir la barrière
Do not cut or remove wood	Défense de couper ou de prendre du bois
Do not disturb	Ne pas déranger
Do not enter	Entrée interdite
Do not feed wild animals	Défense de nourrir les animaux sauvages
Do not fill water tanks from the cleaning hose	Défense de remplir les réservoirs d'eau avec le tuyau de nettoyage
Do not go beyond the protective fence	Ne pas franchir la barrière de sécurité
Do not handle	Défense de toucher
Do not leave clothes here	Ne pas laisser de vêtements ici
Do not leave fire unattended	Ne pas laisser de feu sans surveillance
Do not open door except in an emergency	N'ouvrir cette porte qu'en cas d'urgence
Do not pass	Défense de dépasser
Do not stop	Arrêt interdit
Docking by special permit only	Amarrage interdit sans permis
Docking limited to 48 hours	Amarrage limité à 48 heures
Docking prohibited after 22:00	Amarrage interdit après 22 h
Dogs prohibited	Chiens interdits
Drains disconnected for winter	Tuyaux d'évacuation démontés pour l'hiver
Drinking fountain	Eau potable
Electric fence	Clôture électrique
Electrical Room	Local de service électrique
Electrical Shop	Atelier d'entretien électrique
Elevator for freight	Monte-charge
Elevator for people	Ascenseur
Emergency	Urgence
Emergency exit	Sortie de secours

Emergency exit Press on bar	**Sortie de secours** Appuyer sur la barre
Emergency exit only	Sortie de secours seulement
Emergency eyewash	Douche oculaire
Emergency shelter	Abri d'urgence
Emergency shower	Douche d'urgence
Emergency telephone	Téléphone de secours
Emergency use only	Utiliser en cas d'urgence seulement
Emergency vehicles only	Réservé aux véhicules de secours
Employee parking	Stationnement réservé aux employés
Employees only	Réservé aux employés
End of avalanche area	Fin de la zone d'avalanche
End of blasting zone	Fin de la zone de dynamitage
End of play area	Fin de l'aire de jeux
End of school zone	Fin de la zone scolaire
End of supervised zone No boats allowed	**Fin de la zone surveillée** Embarcations interdites
End of trail	Fin du sentier
Enjoy your stay	Bon séjour
Entrance	Entrée
Evacuation point for disabled persons	Point d'évacuation des personnes handicapées
Exhibits	Exposition
Exit	Sortie
Eye protection must be worn	Lunettes protectrices obligatoires
Fasten your seat belt	Bouclez votre ceinture de sécurité
Fire alarm	Avertisseur d'incendie
Fire axe	Hache d'incendie
Fire department vehicles only	Réservé au service d'incendie
Fire door	Porte coupe-feu
Fire Equipment Building	Dépôt d'outillage contre le feu
Fire escape	Escalier de secours
Fire extinguisher	Extincteur
Fire Hall	Caserne de pompiers
Fire hose	Tuyau d'incendie
Fire road No private vehicles	**Chemin coupe-feu** Interdit à la circulation
Fire safety Keep door closed	**Sécurité-incendie** Garder la porte fermée
Firearms prohibited	Armes à feu interdites
Firewood Open from noon to 20:00	**Bois à brûler** Ouvert de midi à 20 h
First aid	Premiers soins
Flagman ahead	Signaleur

Messages in Both Official Languages (CONTINUED)

Food and beverages not permitted	Nourriture et boissons interdites
Food storage locked from 23:00 to 08:00	Garde-manger sous clé de 23 h à 8 h
Foot protection must be worn	Chaussures protectrices obligatoires
Footwear not allowed on deck	Chaussures interdites autour de la piscine
For a guided tour, please check in at the Visitor Centre	Pour une visite guidée, présentez-vous au centre d'accueil
For more information, call…	Pour plus de renseignements, composez le…
For use by reservation only	Sur réservation seulement
Free admission	Entrée libre
Full	Complet
Garbage and sewage disposal No admittance	**Dépotoir et égouts** Accès interdit
Garbage space	Dépôt d'ordures
Gasoline Station	Poste d'essence
Gate closed from sunset to 08:30	Barrière fermée du coucher du soleil à 8 h 30
Gift and Souvenir Shop	Boutique de cadeaux et de souvenirs
Go slowly	Lentement
Government of Canada	Gouvernement du Canada
Grocery Store	Épicerie
Ground fires strictly prohibited	Feux à même le sol interdits
Guests only	Réservé aux clients
Guide on duty	Guide sur place
Guided hike	Randonnée pédestre guidée
Hand protection must be worn	Gants protecteurs obligatoires
Head protection must be worn	Casque protecteur obligatoire
Hearing protection must be worn	Protecteurs auditifs obligatoires
Helicopter transportation	Transport par hélicoptère
High-security zone Authorized persons only	**Zone de haute sécurité** Réservée aux personnes autorisées
Hiking trail	Sentier de randonnée pédestre
Historic site	Lieu historique
Hold children by the hand	Tenir les enfants par la main
Horses	Chevaux
Hours of operation 08:00 – 16:00	**Heures d'ouverture** 8 h – 16 h
Hours of operation 08:00 – 12:00 and 13:00 – 16:30	**Heures d'ouverture** 8 h – 12 h et 13 h – 16 h 30
If brakes fail, use runaway lane	Si vos freins manquent, utilisez la voie de détresse
In case of fire, do not use elevators	En cas d'incendie, ne pas utiliser les ascenseurs
In case of fire, smoke, gas, alert occupants	En cas de feu, fumée, gaz, alerter les occupants
In case of fire, smoke, gas, evacuate	En cas de feu, fumée, gaz, évacuer les lieux
In case of fire, smoke, gas, phone fire department	En cas de feu, fumée, gaz, appeler le service d'incendie
In case of fire, smoke, gas, sound fire alarm	En cas de feu, fumée, gaz, actionner l'avertisseur
Information	Renseignements
Information Centre	Centre d'information
Interpretation Centre	Centre d'interprétation
Interpretation Service	Service d'interprétation
Interpretation Workshop	Atelier d'interprétation
Interpretive exhibit	Exposition d'interprétation
Interpretive walk	Promenade d'interprétation
It is unlawful to feed wild animals in the park	Il est illégal de nourrir les animaux sauvages dans le parc
Janitor	Concierge
Janitorial Room	Service d'entretien
Keep away	Ne pas approcher
Keep door closed	Garder la porte fermée
Keep off	Accès interdit
Keep pets on leash	Tenir les animaux domestiques en laisse
Keep the shelter clean Pack out litter to the parking area	**Veuillez garder les lieux propres** Rapportez vos déchets au stationnement
Keep well away from canyon edge	Ne pas approcher du bord du canyon
Kitchen	Cuisine
Kitchen staff only	Réservé au personnel de la cuisine
Library	Bibliothèque
Live bear trap Do not enter or touch	**Piège à ours** Ne pas entrer ni toucher
Local traffic only	Réservé à la circulation locale
Lockers	Casiers
Loop	Boucle
Machine to be used by authorized personnel only	Machine réservée au personnel autorisé
Mail Room	Salle du courrier
Maintenance and construction vehicles only	Réservé aux véhicules d'entretien et de chantier
Maintenance Engineering	Service d'entretien
Marsh area	Zone marécageuse
Mechanical Room	Local des installations mécaniques
Meeting in progress	Réunion en cours
Motor boats prohibited	Interdit aux embarcations à moteur
Motorized vehicles are not permitted within the park	Véhicules automobiles interdits dans le parc

Messages in Both Official Languages (CONTINUED)

Motorized vehicles proceeding beyond this point will be impounded	Tout véhicule automobile franchissant cette limite sera saisi
National park boundary	Limite du parc national
Natural bridge	Pont naturel
Naturalist's Office	Bureau du naturaliste
Nature Centre	Centre d'interprétation de la nature
No admittance	Accès interdit
No boating	Navigation de plaisance interdite
No camping or overnight parking	Défense de camper ou de stationner pour la nuit
No campfires	Feux de camp interdits
No dishwashing near pump	Défense de laver la vaisselle près de la pompe
No diving	Défense de plonger
No driving on the shoulder	Défense de rouler sur l'accotement
No exit	Sans issue
No fishing	Pêche interdite
No fishing from boardwalk	Défense de pêcher depuis la passerelle
No golf carts allowed	Voiturettes de golf interdites
No heavy trucks	Poids lourds interdits
No hunting	Chasse interdite
No hunting Wildlife sanctuary	**Chasse interdite** Refuge faunique
No hunting on road or right-of-way	Chasse interdite sur la route ou sur son emprise
No mooring	Amarrage interdit
No motorized vehicles	Véhicules motorisés interdits
No motorized vehicles on beach	Véhicules motorisés interdits sur la plage
No open fires	Feux à ciel ouvert interdits
No overnight parking	Stationnement de nuit interdit
No parking	Stationnement interdit
No parking Bus transportation	**Stationnement interdit** Transport par autocar
No pets allowed	Animaux domestiques interdits
No pets allowed on beach	Animaux domestiques interdits sur la plage
No smoking except in designated areas	**Défense de fumer** sauf dans les endroits désignés à cette fin
No smoking in elevators	Défense de fumer dans les ascenseurs
No stopping	Arrêt interdit
No swimmers allowed on premises	Interdit aux baigneurs
No swimming	Baignade interdite

No trailers over 5 m long	Interdit aux caravanes de plus de 5 m de longueur
No trespassing	Passage interdit
No vacancy	Complet
No vehicles beyond this point	Limite d'accès des véhicules
No vehicles permitted on airstrip	Véhicules interdits sur la piste d'atterrissage
No vehicles permitted on slopes or ditches	Véhicules interdits sur les pentes ou dans les fossés
No visitors after 23:00	Pas de visiteurs après 23 h
Office closed	Bureau fermé
Open daily	Ouvert tous les jours
Open today	Ouvert aujourd'hui
Opening this door will cause an alarm to sound	L'ouverture de cette porte déclenchera une alarme
Operating instructions	Mode d'emploi
Operational Centre	Centre opérationnel
Operational Centre Service vehicles only	**Centre opérationnel** Accès réservé aux véhicules d'entretien
Out of order	Hors service
Outdoor Theatre	Théâtre en plein air
Overnight boat docking by permit only	Amarrage de nuit interdit sans permis
Parallel parking only	Stationnement latéral
Park Superintendent's Office	Bureau du directeur du parc
Parking	Stationnement
Parking prohibited	Stationnement interdit
Parks Canada boats only	Réservé aux embarcations de Parcs Canada
Parks Canada Training School	Centre de formation de Parcs Canada
Parkway closed from November 1 to April 30	Promenade fermée du 1er novembre au 30 avril
Passing permitted	Dépassement autorisé
Pavement ends	Fin du revêtement
Permit required	Permis obligatoire
Personnel only	Réservé au personnel
Photocopier	Photocopieur
Picnic area	Aire de pique-nique
Picnics prohibited	Pique-nique interdit
Play area	Aire de jeux
Playground	Terrain de jeux
Please close cover after use	Veuillez refermer le couvercle
Please deposit expired permits	Veuillez remettre les permis expirés

Messages in Both Official Languages (CONTINUED)

Please discard sanitary napkins and tampons in container provided for this purpose and not in toilet bowl	Veuillez jeter serviettes hygiéniques et tampons dans le récipient prévu à cette fin et non dans la cuvette
Please follow the path	Veuillez suivre le sentier
Please proceed	Veuillez avancer
Please remove overshoes	Veuillez enlever vos couvre-chaussures
Please use front door	Veuillez utiliser la porte avant
Plumber's Shop	Atelier de plomberie
Police	Police
Post Office	Bureau de poste
Postal facilities	Service de la poste
Private road	Chemin privé
Projection Room	Salle de projection
Public notice	Avis public
Push to open door	Appuyer pour ouvrir
Quiet hours 23:00 – 07:00	**Heures de tranquillité** 23 h – 7 h
Receiving	Réception
Reception	Accueil
Records Office	Service des dossiers
Registration	Inscription
Registration booth closed for the season	Poste d'inscription fermé pour la saison
Reserved parking	Stationnement réservé
Restaurant	Restaurant
Restored area	Secteur restauré
Restricted area	Interdit au public
Ring bell	Sonner
Road closed	Route fermée
Road construction Proceed at your own risk	**Travaux routiers** Circulez à vos risques
Rocks become slippery when wet	Rochers glissants
Rough road next XX km	Chaussée cahoteuse sur XX km
Runaway lane	Voie de détresse
Seasonal birding trail	Sentier saisonnier d'observation des oiseaux
Security zone Authorized persons only	**Zone de sécurité** Personnes autorisées seulement
Seeded area Keep off	**Semis** Accès interdit
Service vehicles only	Accès réservé aux véhicules d'entretien
Services in both official languages	Services dans les deux langues officielles
Sewage Treatment Plant No admittance	**Station d'épuration des eaux usées** Accès interdit
Shipping	Expédition
Shipping and Receiving	Expédition et réception
Shower	Douche
Skate change entrance	Entrée du vestiaire des patineurs
Skiing on roadway prohibited	Ski interdit sur la chaussée
Slippery when wet	Chaussée glissante
Slow	Lentement
Slow down	Ralentir
Slower traffic keep right	Circulation lente à droite
Smoking permitted	Permission de fumer
Smoking prohibited	Défense de fumer
Snack Bar	Casse-croûte
Snow study area Please keep out	**Secteur d'étude de la neige** Entrée interdite
Soft shoulder	Accotement mou
Speed radar enforced	Vitesse surveillée par radar
Staff Residence	Résidence du personnel
Staff Room	Salle du personnel
Stairs	Escalier
Stairs, down	Escalier descendant
Stairs, up	Escalier montant
Stay on trail	Demeurer sur le sentier
Stay out of the woods	Accès à la forêt interdit
Steep hill	Pente raide
Steep hill Use lower gear	**Pente raide** Rétrograder
Stone Cutting Shop	Atelier de taille de la pierre
Stop for pedestrians	Priorité aux piétons
Stop to register	Inscription obligatoire
Storage	Entreposage
Stretcher	Civière
Structure under repair	Travaux de réfection
Structure under restoration	Travaux de restauration
Taxi transportation	Transport par taxi
Taxis only	Réservé aux taxis
Telephone	Téléphone
Telephone equipment	Équipement téléphonique
Thank you for your co-operation	Merci de votre collaboration
Theatre	Théâtre
This area is being redeveloped for your enjoyment. Sorry for the inconvenience.	Nous réaménageons cette zone pour mieux vous servir. Nous regrettons les contretemps que cela peut vous causer.

Messages in Both Official Languages (CONTINUED)

This area is being restored For your safety and protection, please do not enter this work area	**Travaux de restauration** Accès au chantier interdit pour des raisons de sécurité
To purchase any of the above items, please see the guide on duty	Pour acheter ces articles, voir le guide de service
Toll gate	Poste de péage
Traffic controlled by lights	Feux de circulation
Trailer parking	Stationnement de caravanes
Trailers prohibited	Interdit aux caravanes
Truck	Camion
Two-hour parking 9:00 – 18:00	Stationnement de deux heures entre 9 h et 18 h
Under repair	En réparation
Unloading zone 10-minute limit	**Zone de déchargement** Maximum 10 minutes
Unsupervised beach Swim at your own risk	**Plage non surveillée** Baignade à vos risques
Unsupervised beyond this point	Limite de la zone surveillée
Use low gear	Rétrograder
Use other door	Utiliser l'autre porte
Use revolving door	Utiliser la porte tournante
Use washrooms in the other building	Utiliser les toilettes de l'autre bâtiment
Vehicle Shed	Hangar pour véhicules
Visitor Reception Centre	Centre d'accueil
Visitors are requested to report to the commissionaire	Les visiteurs sont priés de s'adresser au commissionnaire

Wait for green light before entering (lock)	Attendre le feu vert avant d'entrer (écluse)
Wait for signal	Attendre le signal
Waiting area	Salle d'attente
Warden service only	Réservé aux gardes
Warden's Office	Bureau du garde
Watch for maintenance and emergency vehicles	Attention aux véhicules d'entretien et de secours
Watch for snowplows and sanders	Attention aux chasse-neige et aux épandeurs de sable
Watch for trucks 07:30 – 17:00	Attention aux camions entre 7 h 30 et 17 h
Watch your step	Attention à la marche
Water on highway	Route partiellement inondée
Water supply No admittance	**Réservoir d'eau** Accès interdit
Weight limit: 20 t	Poids maximum : 20 t
Wheelchairs and pedestrians only	Réservé aux fauteuils roulants et aux piétons
Winter parking Lots Numbers 1, 2 and 3	**Stationnement d'hiver** Lots numéros 1, 2 et 3
Workshop	Atelier
Yield	Céder
You are here	Vous êtes ici
Your co-operation will be appreciated	Merci de votre collaboration

C

Appendix C – Manual of Uniform Traffic Control Devices for Canada

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Overview

The following list is based on the Manual of Uniform Traffic Control Devices for Canada, 4th Edition. All aspects (design, colour, layout, etc.) of the signs listed here must conform to relevant provincial standards. As not all provinces and territories have adopted the Manual of Uniform Traffic Control Devices for Canada, it is essential that each park and site be aware of which traffic standards to apply. As most of these signs are of a standard format, provincial traffic authorities would be a good source of information on how and where to order these signs.

MUTCD Sign Types

MUTCD Regulatory signs

Right-of-way control

- Stop sign
- Yield sign
- Railway crossing sign

Speed control

- Maximum speed sign
- Truck maximum speed sign
- Minimum speed sign

Turn control

- Through traffic prohibited (plus supplementary tabs) signs
- Right (left) turn prohibited signs
- Turn right (left) sign
- Turns prohibited sign (i.e. mandatory straight)
- U-turn prohibited sign
- Right (left) turn on red traffic signal prohibited sign

Directional traffic control

- One-way sign
- Entry prohibited sign
- Wrong way sign
- Two-way traffic sign
- Keep right sign

Passing control

- Passing prohibited sign
- Passing permitted sign
- Keep right except to pass sign
- Slower traffic keep right sign
- Yield centre lane to opposing traffic sign

Lane designation

- Right (left) turn only lane sign
- Straight through or right (left) turn only lane sign
- Right or left turn only lane sign
- All movements permitted lane sign
- Straight through only lane sign
- Two-way left-turn lane sign
- Side-mounted lane designation signs
- Side-mounted multiple lane designation signs

Specific types of road user control

- Truck control signs
- Truck route sign
- Trucks prohibited sign
- Lane use restricted sign
- Truck inspection station signs

Dangerous goods route signs

- Dangerous goods route sign
- Dangerous goods prohibited sign

Weight limit control sign

Reserved lane signs

Miscellaneous regulatory signs

Temporary regulatory tab signs

MUTCD Sign Types (CONTINUED)

MUTCD Warning signs

Road alignment

- Single turn and curve sign
- Reverse turn sign
- Reverse curve sign
- Winding road sign
- Advisory speed sign
- Checkerboard sign (indicates termination of a road)
- Chevron alignment sign

Intersections

- Concealed road signs
- T intersection sign
- Y intersection sign
- Merge sign
- Added lane sign

Specific road features

- Double arrow sign
- Railway crossing ahead signs
- Hill signs
- Bump sign
- Road narrows signs
- Narrow structure sign
- Lane ends sign
- Pavement ends sign
- Low clearance signs
- Distance advisory tab signs
- Object marker

Divided highway transitions

- Divided highway begins sign
- Divided highway ends signs
- Divided highway ahead sign

Traffic regulation ahead

- Stop ahead sign
- Yield ahead sign
- Two-way traffic ahead sign
- Signals ahead sign
- Prepare to stop sign
- Prepare to stop at railway crossing sign
- Reserved lane ahead sign
- Reserved lane crossing sign
- Maximum speed ahead sign

Intermittent or moving hazards

- Slippery when wet sign
- Fallen rock sign
- Bicycle crossing ahead sign
- Truck entrance sign
- School bus stop ahead
- Snowmobile crossing sign
- Open bridge sign
- School bus entrance sign
- Deer crossing sign
- Moose crossing sign
- Cattle crossing sign
- Fire truck entrance sign
- Horse with rider sign
- Horse-drawn vehicle sign
- Bridge ices sign

Temporary warning tab signs

MUTCD Sign Types (CONTINUED)

MUTCD Guide, information and freeway signs

Given its unique operational requirements, Parks Canada has adopted elements of the MUTCD standards for its information and directional signage while devising its own system. All information and directional signs within park or site boundaries must conform to Parks Canada standards, except for the following, which should conform to MUTCD:

Route marker signs

- Route marker (either Trans-Canada or province specific) signs
- Junction signs
- Arrow tab signs (when used with route markers)
- Advance turn arrow tab signs (when used with route marker)
- Directional arrow tab signs (when used with route markers)
- Cardinal direction tab signs (when used with route markers)
- By-pass tab sign (when used with route markers)
- End of route tab sign (when used with route markers)

Cul-de-sac sign

Passing lane ahead sign

MUTCD Pedestrian crossing control

Pedestrian crosswalks

- Pedestrian crosswalk sign
- Pedestrian crosswalk ahead sign

School crosswalks

- School crosswalk sign
- School crosswalk ahead sign

Special crosswalk (pedestrian crossovers, pedestrian corridors)

- Special crosswalk overhead sign
- Pedestrian pushbutton sign

Traffic control signals at pedestrian crossing

- School area sign
- School area reduced speed zones
- Playground area sign

Other pedestrian signs

- Contra-flow warning for pedestrians sign

MUTCD Pavement markings and signals

Must conform to MUTCD – see relevant manuals for details.

D

Appendix D – Sign Type Configurations

Appendix D – Sign Type Configurations

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Introduction

Appendix D provides a breakdown of the sign classification system outlined in Section 3. It details layout options by sign type and provides information concerning sign function and application.

Detailed planning information is outlined by sign type to assist the sign planner in applying the optimum layout configuration.

Each sign is identified by sign type and layout number. This combination informs the designer on how to properly lay out the signs. Appendix E, which is primarily used by the designer, provides detailed layout specifications for each type.

Sign Type A1

Off-Site

A1 Tourism-Oriented Directional Sign Patch

A1 Tourism-Oriented Directional Sign (TODS) patches consist of the Parks Canada identifier (combination of the Parks Canada beaver and the "Canada" wordmark). The patch identifies Parks Canada activities or sites of significant interest to the travelling public on provincial TODS.

A1 Tourism-Oriented Directional Sign Patch		Message elements:	
A1-22 *Layout 22, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Patch used on provincial highway TODS. Its placement must be negotiated with appropriate government jurisdictions.		For vehicular use only

Sign Type B1

Off-Site

B1 Highway Advance

B1 highway advance signs consist of a text message and Parks Canada identifier (combination of Parks Canada beaver and the "Canada" wordmark).

They identify the name of the national park, national historic site or national marine conservation area and convey direction and/or distance.

Their function is to identify and give direction to Parks Canada activities or sites of significant interest to the travelling public on provincial highways.

This sign type introduces the trailblazer system, as visitors would see the highway advance sign, determine where to go, then follow the trailblazers.

B1 Highway advance		Message elements: Text	
B1-23 *Layout 23, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Must be negotiated with the province. Should appear on highways not subject to TODS and/or at complicated decision points along secondary roadways. If necessary, a secondary message may be used to identify exits, route number, or to communicate distance.	The ideal highway advance sign would include the Parks Canada identifier, the name of the park/site/marine conservation area, Parks Canada Heritage Green background, bilingual message and layout. However, it is recognized that these content specifications are subject to provincial/municipal approval and must be negotiated at the Field Unit level.	For vehicular use only
B1-24 *Layout 24, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Must be negotiated with the province. Should appear on highways not subject to TODS and/or at complicated decision points along secondary roadways. If necessary, a secondary message may be used to identify exits, route number, or to communicate distance.	The ideal highway advance sign would include the Parks Canada identifier, the name of the park/site/marine conservation area, Parks Canada Heritage Green background, bilingual message and layout. However, it is recognized that these content specifications are subject to provincial/municipal approval and must be negotiated at the Field Unit level.	For vehicular use only

Sign Type B2, B3, B4

Off-Site

B2, B3, B4 Trailblazers

B2, B3, B4 trailblazers consist of the Parks Canada identifier, distance indicator and arrow.

Their function is to aid in directing users to a national park, national historic site or national marine conservation area and they are meant to be employed in conjunction with a highway advance sign.

B2 Trailblazer	Message elements: Canada km →	
B2-25 *Layout 25, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Should be placed near decision points when a user must leave the current road to continue on to destination. Should always be followed by a B3 or B4 trailblazer to confirm that visitors are on the right path unless the trailblazer is placed just before the destination entrance. Located on secondary roadways. Must not be used on major highways (e.g. Trans-Canada Highway).	**Message load** 1 symbol per sign maximum 1 destination maximum Standard artwork is available	Recommended for vehicular use only

B3 Trailblazer	Message elements: Canada →	
B3-25 *Layout 25, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Should be placed near decision points when a user must leave the current road to continue on to destination. Should always be followed by a B3 or B4 trailblazer to confirm that visitors are on the right path unless the trailblazer is placed just before destination entrance. Must not be used on major highways (e.g. Trans-Canada Highway).	**Message load** 1 symbol per sign maximum 1 destination maximum Standard artwork is available	Recommended for vehicular use only

B4 Trailblazer	Message elements: Canada km	
B4-25 *Layout 25, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Used in situations where it is important to communicate that the site is straight ahead. In some cases, such as canals, it may be necessary to provide a nameplate indicating a specific section of a site. Must not be used on major highways (e.g. Trans-Canada Highway).	**Message load** 1 symbol per sign maximum 1 destination maximum Standard artwork is available	Recommended for vehicular use only

Sign Type B5

Off-Site

B5 Trailblazer Nameplate

B5 trailblazer nameplates consist of a text message.

Their function is to identify the national park, national historic site or national marine conservation area, and aid in directing users to that destination. This sign always appears with B2, B3, B4 trailblazer signs.

The message must be bilingual and appear in an over-under format.

B5 Trailblazer nameplate		Message elements: **Text**	
B5-25 *Layout 25, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
 Under review	The B5 trailblazer nameplate is a secondary component of the trailblazer system and must be accompanied by a B2, B3, B4 trailblazer. The B5 trailblazer nameplate is mounted below a B2, B3, B4 trailblazer sign.	**Message load** 1 destination maximum	Recommended for vehicular use only

Sign Type C1, C2

Primary Identification

C1, C2 Primary Identification Signs (PIDs)

C1 and C2 primary identification signs consist of the Parks Canada signature, a text message, and the "Canada" wordmark.

Their function is to identify and confirm arrival at a national park, national historic site, national marine conservation area or a facility located within these sites.

PIDs must be installed only on federal property (i.e. within the property boundaries).

The secondary message can be a major facility, a section of a NP/NHS/NMCA, or other destination worthy of noting.

PIDs can be used on both vehicle and pedestrian routes and must appear near the entrance to the site or building.

C1 Primary Identification sign	Message elements: Text	
C1-26 *Layout 26, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
	Message load 1 Parks Canada signature 1 Canada wordmark 1 destination maximum	**Message load** 1 Parks Canada signature 1 Canada wordmark 1 destination maximum
Must appear near the entrance to the site or building and, depending on the nature of the site, several PIDs may be required to identify all principal points of access. PIDs can be two-sided or wall-mounted.		

C2 Primary Identification sign	Message elements: Text	
C2-26 *Layout 26, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
	Message load 1 Parks Canada signature 1 Canada wordmark 1 destination maximum 1 secondary text message maximum	**Message load** 1 Parks Canada signature 1 Canada wordmark 1 destination maximum 1 secondary text message maximum
Must appear near the entrance to the site or building and, depending on the nature of the site, several PIDs may be required to identify all principal points of access. PIDs can be two-sided or wall-mounted.		

Sign Type D1

Special Purpose

D1 Welcome Signs

D1 welcome signs consist of the beaver symbol and a standard text message.

Their function is to inform visitors, after following a series of trailblazers, that they are entering a national park, national historic site or national marine conservation area.

D1 Welcome signs	Message elements: Text	
D1-27 *Layout 27, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 This is the preferred layout for D1 welcome signs. Designed as a one-sided sign or combination two-sided sign with D2 sign type on the back. Must appear only following PID.	**Message load** 1 beaver symbol per sign 1 standard welcome message	**Message load** 1 beaver symbol per sign 1 standard welcome message
D1-28 *Layout 28, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway. Designed as a one-sided sign or combination two-sided sign with D2 sign type on the back. Must appear only following PID.	**Message load** 1 beaver symbol per sign 1 standard welcome message	**Message load** 1 beaver symbol per sign 1 standard welcome message

Sign Type D2

Special Purpose

D2 Thank You Signs

D2 thank you signs consist of the beaver symbol and a standard text message.

Their function is to inform visitors that they are leaving a national park, national historic site or national marine conservation area.

D2 Thank you signs	Message elements: Text	
D2-27 *Layout 27, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 This is the preferred layout for D2 thank you signs. Designed as a one-sided sign or combination two-sided sign with D1 sign type on the back.	**Message load** 1 beaver symbol per sign 1 standard thank you message	**Message load** 1 beaver symbol per sign 1 standard thank you message
D2-28 *Layout 28, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway. Designed as a one-sided sign or combination two-sided sign with D1 sign type on the back.	**Message load** 1 beaver symbol per sign 1 standard thank you message	**Message load** 1 beaver symbol per sign 1 standard thank you message

Sign Type D3, D4

Special Purpose

D3 Day-use Area Signs and D4 Day-use Area Tabs

D3 day-use area signs consist of the beaver symbol and a text message. D4 day-use area tabs consist of a text message and/or symbols.

The function of a day-use area sign is to inform visitors that they are still in a national park, national historic site or national marine conservation area.

The tab is used to provide information specific to that day-use area.

D3 Day-use area sign	Message elements: Text	
D3-28 *Layout 28, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used to welcome visitors to day-use area. May use D4 day-use area tab mounted below.	**Message load** 1 beaver symbol per sign 1 destination maximum	**Message load** 1 beaver symbol per sign 1 destination maximum

D4 Day-use area tab	Message elements: Text	
D4-29 *Layout 29, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 May be used only mounted below a D3 day-use area sign. Must not exceed 1/2 the height of D3.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 text messages maximum	**Message load** 6 symbols per sign maximum 6 text messages maximum

Sign Type E1, E2, E3

Special Purpose

E1, E2, E3 Fee Signs

E1 fee signs consist of the beaver symbol and a text message. E2 fee sign inserts and E3 fee sign tabs consist of a text message.

Their function is to provide fee information to visitors before entering a national park, national historic site or national marine conservation area.

Design specifications for post- and surface-mounted fee signs have been standardized.

Approved pricing, provided in the National Pricing Compendium only, must be used on fee signs.

E1 Fee signs

Message elements: Text

E1-30 *Layout 30, Appendix E*	Vehicular specifications	Pedestrian specifications
 Used to welcome visitors. Sign structure holding E2 fee sign insert.	**Message load** 1 beaver symbol per sign 1 standard welcome message **Standard panel** 1 standard panel size Standard artwork available	**Message load** 1 beaver symbol per sign 1 standard welcome message **Standard panel** 1 standard panel size Standard artwork available

E1-31 *Layout 31, Appendix E*	Vehicular specifications	Pedestrian specifications
 Used to welcome visitors. Sign structure holding E2 fee sign insert.	**Message load** 1 beaver symbol per sign 1 standard welcome message **Standard panel** 1 standard panel size Standard artwork available	**Message load** 1 beaver symbol per sign 1 standard welcome message **Standard panel** 1 standard panel size Standard artwork available

E2 Fee sign insert

Message elements: Text

E2-32 *Layout 32, Appendix E*	Vehicular specifications	Pedestrian specifications
 Fee sign insert placed in E1 fee sign.	**Message load** 1 approved fee message, as per National Pricing Compendium **Standard panel** 2 standard panel sizes available Standard template available	**Message load** 1 approved fee message, as per National Pricing Compendium **Standard panel** 2 standard panel sizes available Standard template available

E3 Fee sign tab

Message elements: Text

E3-29 *Layout 29, Appendix E*	Vehicular specifications	Pedestrian specifications
 May be used only mounted below an E1 fee sign. Must not exceed 1/2 the height of E1.	**Message load** Must not exceed 1/2 the height of E1.	**Message load** Must not exceed 1/2 the height of E1.

Sign Type E4

Special Purpose

E4 Fee Signs

E4 fee signs consist of the beaver symbol and a text message.

Their function is to provide fee information to visitors before entering a national park, national historic site or national marine conservation area.

Design specifications for post- and surface-mounted fee signs have been standardized.

Approved pricing, provided in the National Pricing Compendium only, must be used on fee signs.

E4 Fee signs	Message elements: Text	
E4-30 *Layout 30, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used to welcome visitors. Fees are part of E4. No insert required.	**Message load** 1 beaver symbol per sign 1 standard welcome message 1 approved fee message, as per National Pricing Compendium **Standard panel** 1 standard panel size Standard artwork available	**Message load** 1 beaver symbol per sign 1 standard welcome message 1 approved fee message, as per National Pricing Compendium **Standard panel** 1 standard panel size Standard artwork available
E4-31 *Layout 31, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used to welcome visitors. Fees are part of E4. No insert required.	**Message load** 1 beaver symbol per sign 1 standard welcome message 1 approved fee message, as per National Pricing Compendium **Standard panel** 1 standard panel size Standard artwork available	**Message load** 1 beaver symbol per sign 1 standard welcome message 1 approved fee message, as per National Pricing Compendium **Standard panel** 1 standard panel size Standard artwork available

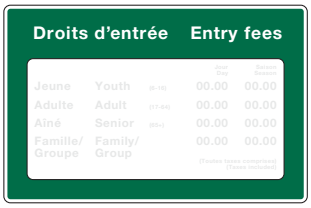
Sign Type E5, E6

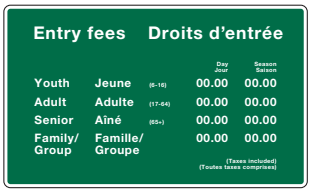
Special Purpose

E5, E6 Fee Signs

E5 and E6 fee signs consist of a text message.

Their function is to confirm fee information to visitors as the fee transaction is being conducted. These fee signs are designed to be surface-mounted on the side of an entrance kiosk where the fee transaction is conducted.

E5 Fee sign	Message elements: **Text**	
E5-32 *Layout 32, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Sign structure holding E2 fee sign insert.	**Message load** 1 approved fee message, as per National Pricing Compendium **Standard panel** 1 standard panel size available	**Message load** 1 approved fee message, as per National Pricing Compendium **Standard panel** 1 standard panel size available

E6 Fee sign	Message elements: **Text**	
E6-32 *Layout 32, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Fee sign. No insert required.	**Message load** 1 approved fee message, as per National Pricing Compendium **Standard panel** 1 standard panel size available	**Message load** 1 approved fee message, as per National Pricing Compendium **Standard panel** 1 standard panel size available

Sign Type E7, E8

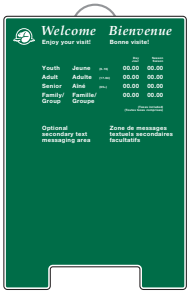
Special Purpose

E7, E8 Fee Signs

E7 and E8 fee signs consist of the beaver symbol and a text message.

A portable sandwich board used to provide fee information to pedestrians before entering a national park, national historic site or national marine conservation area.

E7 Fee sign		Message elements: Text	
E7-30 *Layout 30, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used to welcome visitors. Sign structure holding E2 fee sign insert.	**Message load** 1 beaver symbol per sign 1 standard welcome message 1 fee message, as per National Pricing Compendium Number of supporting messages determined by available space **Standard panel** 1 standard panel size available	**Message load** 1 beaver symbol per sign 1 standard welcome message 1 fee message, as per National Pricing Compendium Number of supporting messages determined by available space **Standard panel** 1 standard panel size available

E8 Fee sign		Message elements: Text	
E8-30 *Layout 30, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used to welcome visitors. Fees are part of E8. No insert required.	**Message load** 1 beaver symbol per sign 1 standard welcome message 1 fee message, as per National Pricing Compendium Number of supporting messages determined by available space **Standard panel** 1 standard panel size available	**Message load** 1 beaver symbol per sign 1 standard welcome message 1 fee message, as per National Pricing Compendium Number of supporting messages determined by available space **Standard panel** 1 standard panel size available

Sign Type F1, F2

Special Purpose

F1, F2 Street Identification

F1 street identification signs consist of the beaver symbol and a text message. F2 street identification signs consist of a text message only.

Their function is to identify street destinations to users within a national park, national historic site or national marine conservation area.

F1 Street identification	Message elements: Text	
F1-33 *Layout 33, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Street sign with beaver symbol used with single or multiple destinations. When multiple destinations appear on one post, sign type F2 is placed below sign type F1.	**Message load** 1 beaver symbol per sign 1 destination maximum	**Message load** 1 beaver symbol per sign 1 destination maximum

F2 Street identification	Message elements: Text	
F2-33 *Layout 33, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Street sign without beaver symbol used when multiple street names appear on one post. Sign type F2 is placed below sign type F1.	**Message load** 1 destination maximum	**Message load** 1 destination maximum

Sign Type G1

Special Purpose

G1 Partnership Recognition

G1 partnership recognition signs consist of a text message, Parks Canada partnership symbol (combination of Parks Canada signature and the "Canada" wordmark) and other partner symbols.

Their function is to acknowledge partnering or collaborative arrangements as no partner symbol may appear on Parks Canada's signs.

The Partnership Recognition Framework provides guidance for the recognition of Parks Canada's participation in partnering and collaborative arrangements, as well as the extent of recognition to be provided to Parks Canada's partners. It should be consulted as the partnership recognition sign is being developed. It is recommended that a qualified designer be contracted to prepare the layout.

G1 Partnership recognition	Message elements: **Text**	
G1-34 *Layout 34, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
	Message load 1 text message maximum 1 Parks Canada partnership symbol Number of partner symbols determined by partnership agreement	**Message load** 1 text message maximum 1 Parks Canada partnership symbol Number of partner symbols determined by partnership agreement

Sign Type G2

Special Purpose

G2 Fees at Work Signs

G2 fees at work signs consist of the beaver symbol and a text message.

Their function is to acknowledge the use of public entry fees for recapitilization work being done at a national park, national historic site or national marine conservation area.

G2 Fees at work signs	Message elements: Text	
G2-27 *Layout 27, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 This is the preferred layout for G2 fees at work signs.	**Message load** 1 beaver symbol per sign 1 standard “Your fees…” message	**Message load** 1 beaver symbol per sign 1 standard “Your fees…” message
G2-28 *Layout 28, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway.	**Message load** 1 beaver symbol per sign 1 standard “Your fees…” message	**Message load** 1 beaver symbol per sign 1 standard “Your fees…” message

Sign Type H1

Information

H1 Emergency

H1 emergency signs consist of a text message.

Their function is to convey an emergency-related message, such as “Emergency exit only” or “Evacuation point”.

Emergency signs must not use Parks Canada Heritage Green background.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

H1 Emergency		Message elements: **Text**	
H1-12 *Layout 12, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for H1 emergency signs.	**Message load** 1 text message maximum	**Message load** 3 text messages maximum
H1-13 *Layout 13, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway.	**Message load** 1 text message maximum	**Message load** 3 text messages maximum

Sign Type J1, J2

Information

J1, J2 Campsite Identification

J1 and J2 campsite identification signs consist of a text message.

Their function is to identify campsites.

J1 Campsite identification		Message elements:	Text
J1-05 *Layout 05, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for campsite identification signs.	**Message load** 1 text message maximum	**Message load** 1 text message maximum

J2 Campsite identification		Message elements:	Text
J2-05 *Layout 05, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Vertical layout is used where horizontal space is limited and a narrow-profile sign is required.	**Message load** 1 text message maximum	**Message load** 1 text message maximum

Sign Type K1
Information

K1 Identification

K1 identification signs consist of text messages.

Their function is to identify a destination by name.

K1 identification signs may also be used to communicate a message.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

K1 Identification		Message elements: **Text**	
K1-01 *Layout 01, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for K1 identification signs.	**Message load** 3 elements maximum 3 destinations maximum	**Message load** 6 destinations maximum
K1-02 *Layout 02, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway.	**Message load** 3 elements maximum 3 destinations maximum	**Message load** 6 destinations maximum

Sign Type K2

Information

K2 Identification

K2 identification signs consist of various combinations of messages, and guidance and information or emergency symbols.

Their function is to identify and confirm arrival at a destination and provide additional information.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

K2 Identification		Message elements: **Text**	
K2-01 *Layout 01, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for K2 identification signs when multiple symbols are required for a single destination. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	**Message load** 6 symbols per destination maximum 6 destinations maximum
K2-02 *Layout 02, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a narrow shoulder on a roadway. When multiple destinations exist, it is not necessary that each destination contain all message elements. This is primarily a vehicular sign.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	**Message load** 6 symbols per destination maximum 6 destinations maximum
K2-03 *Layout 03, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when no more than one symbol is required per destination. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 3 symbols per sign maximum 3 destinations maximum	**Message load** 1 symbol per destination maximum 6 destinations maximum

Sign Type K3

Information

K3 Identification

K3 identification signs consist of a guidance and information or emergency symbol, and a text message.

Their function is to identify and confirm arrival at a destination and provide additional information.

K3 Identification		Message elements: Text	
K3-06 *Layout 06, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used for single destinations only when the primary message element is a single symbol.	**Message load** 1 symbol per sign maximum 1 destination maximum	**Message load** 1 symbol per sign maximum 1 destination maximum

Sign Type K4, K5, K6
Information

K4, K5, K6 Identification
K4, K5, and K6 identification signs consist of guidance and information or emergency symbols or portage route identification numbers.

Their function is to convey information without text.

K4 Identification		Message elements:	
K4-07 *Layout 07, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Post-mounted symbol sign.	**Message load** 4 symbols per sign maximum	**Message load** 6 symbols per sign maximum

K5 Identification		Message elements:	
K5-08 *Layout 08, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when affixing symbol directly to a wall (text message not required). Can be used in conjunction with stand-alone arrow symbol. Must ensure tactile signs are used when required. See Federal Identity Program Manual, Tactile Signage – Sign and System Installation Guide, Interim Guide October 1997, to determine when and where tactile signs are required.	**Message load** 1 symbol per sign maximum	**Message load** 1 symbol per sign maximum

K6 Identification		Message elements:	
K6-09 *Layout 09, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Post-mounted symbol sign. Visible over a long distance. Used to identify and confirm arrival at destination (portage trail head) along a backcountry traverse.	**Message load** 1 symbol per sign maximum	**Message load** 1 symbol per sign maximum

Sign Type L1
Information

L1 Directional

L1 directional signs consist of various combinations of arrows and text messages.

Their function is to direct users to a specific destination within a national park, national historic site or national marine conservation area.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

L1 Directional	Message elements: → **Text**		
L1- 01 *Layout 01, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for L1 directional signs.	**Message load** 6 elements maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	**Message load** 6 destinations maximum **Arrow sequence** No prescribed sequence
L1-02 *Layout 02, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway.	**Message load** 6 elements maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	**Message load** 6 destinations maximum **Arrow sequence** No prescribed sequence

Sign Type L2
Information

L2 Directional

L2 directional signs consist of various combinations of arrows, text messages and distance indicators or other numeric elements such as campsite identification.

Their function is to direct users to a specific destination within a national park, national historic site or national marine conservation area and to communicate the distance to a destination.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

L2 Directional	Message elements: → Text km		
L2-01 *Layout 01, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for L2 directional signs. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	**Message load** 6 destinations maximum **Arrow sequence** No prescribed sequence
L2-02 *Layout 02, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a narrow shoulder on a roadway. When multiple destinations exist, it is not necessary that each destination contain all message elements. This is primarily a vehicular sign.	**Message load** 9 elements maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	Recommended for vehicular use only, following MUTCD convention
L2-04 *Layout 04, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign. When multiple destinations exist, it is not necessary that each destination contain all message elements. This is primarily a pedestrian sign.	Recommended for pedestrian use only	**Message load** 6 destinations maximum **Arrow sequence** No prescribed sequence

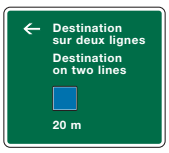
Sign Type L3
Information

L3 Directional

L3 directional signs consist of various combinations of arrows, text messages, guidance and information or emergency symbols, and distance indicators or other numeric elements such as campsite identification.

Their function is to direct users to a specific destination within a national park, national historic site or national marine conservation area and to communicate the distance to a destination.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

L3 Directional	Message elements: → Text ■ ■ km	
L3-01 *Layout 01, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 This is the preferred layout for L3 directional signs when multiple symbols are required for a single destination. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	**Message load** 6 symbols per destination maximum 6 destinations maximum **Arrow sequence** No prescribed sequence
L3-02 *Layout 02, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a narrow shoulder on a roadway. When multiple destinations exist, it is not necessary that each destination contain all message elements. This is primarily a vehicular sign.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	Recommended for vehicular use only, following MUTCD convention
L3-03 *Layout 03, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used when no more than one symbol is required per destination. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 3 symbols per sign maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	**Message load** 1 symbol per destination maximum 6 destinations maximum
L3-04 *Layout 04, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign. When multiple destinations exist, it is not necessary that each destination contain all message elements. This is primarily a pedestrian sign.	Recommended for pedestrian use only	**Message load** 6 symbols per destination maximum 6 destinations maximum **Arrow sequence** No prescribed sequence

Sign Type L4
Information

L4 Directional

L4 directional signs consist of various combinations of arrows, text messages, and guidance and information or emergency symbols.

Their function is to direct users to a specific destination within a national park, national historic site or national marine conservation area.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

L4 Directional		Message elements: → Text ■ ■	
L4-01 *Layout 01, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for L4 directional signs when multiple symbols are required for a single destination. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	**Message load** 6 symbols per destination maximum 6 destinations maximum **Arrow sequence** No prescribed sequence
L4-02 *Layout 02 , Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a narrow shoulder on a roadway. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	**Message load** 6 symbols per destination maximum 6 destinations maximum **Arrow sequence** No prescribed sequence
L4-03 *Layout 03, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when no more than one symbol is required per destination. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 3 symbols per sign maximum 3 destinations maximum **Arrow sequence** Top to bottom: through destination, left, right	**Message load** 1 symbol per destination maximum 6 destinations maximum **Arrow sequence** No prescribed sequence

Sign Type L5
Information

L5 Directional

L5 directional signs consist of a guidance and information or emergency symbol, a text message, distance indicators or other numeric element such as campsite identification, and an arrow.

Their function is to direct users to a specific destination within a national park, national historic site or national marine conservation area and to communicate the distance to a destination.

L5 Directional	Message elements: Text km →	
L5-06 *Layout 06, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Used for single destinations only when the primary message element is a symbol.	**Message load** 1 symbol per sign maximum 1 destination maximum	**Message load** 1 symbol per sign maximum 1 destination maximum

Sign Type L6
Information

L6 Directional
L6 directional signs consist of a guidance and information or emergency symbol, text message, and arrow.

Their function is to direct users to a specific destination within a national park, national historic site or national marine conservation area.

L6 Directional	Message elements: Text →	
L6-06 *Layout 06, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Used for single destinations only when the primary message element is a single symbol.	**Message load** 1 symbol per sign maximum 1 destination maximum	**Message load** 1 symbol per sign maximum 1 destination maximum

Sign Type L7
Information

L7 Directional

L7 directional signs consist of guidance and information or emergency symbols, distance indicators or other numeric elements such as campsite identification, and arrows.

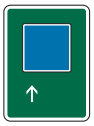
Their function is to direct users to a specific destination within a national park, national historic site or national marine conservation area and to communicate distance to a destination.

L7 Directional		Message elements:	
L7-07 *Layout 07, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when the primary message element is a symbol. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	**Message load** 6 symbols per destination maximum 6 destinations maximum

Sign Type L8
Information

L8 Directional
L8 directional signs consist of guidance and information or emergency symbols, and arrows.

Their function is to direct users to a specific destination within a national park, national historic site or national marine conservation area.

L8 Directional		Message elements:	
L8-07 *Layout 07, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when the primary message element is a symbol.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	**Message load** 6 symbols per destination maximum 6 destinations maximum

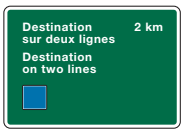
Sign Type M1
Information

M1 Distance

M1 distance signs consist of various combinations of text messages, guidance and information or emergency symbols, and distance indicators.

Their function is to communicate the distance to a specific destination within a national park, national historic site or national marine conservation area.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

M1 Distance		Message elements: **Text** ■ ■ **km**	
M1-01 *Layout 01, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for M1 distance signs when multiple symbols are required for a single destination. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	**Message load** 6 symbols per destination maximum 6 destinations maximum
M1-02 *Layout 02, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a narrow shoulder on a roadway. When multiple destinations exist, it is not necessary that each destination contain all message elements. This is primarily a vehicular sign.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	Recommended for vehicular use only, following MUTCD conventions
M1-03 *Layout 03, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when no more than one symbol is required per destination. When multiple destinations exist, it is not necessary that each destination contain all message elements.	**Message load** 9 elements maximum 3 symbols per sign maximum 3 destinations maximum	**Message load** 1 symbol per destination maximum 6 destinations maximum
M1-04 *Layout 04, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign. When multiple destinations exist, it is not necessary that each destination contain all message elements. This is primarily a pedestrian sign.	Recommended for pedestrian use only	**Message load** 6 symbols per destination maximum 6 destinations maximum

Sign Type M2

Information

M2 Distance

M2 distance signs consist of various combinations of text messages and distance indicators.

Their function is to communicate the distance to a specific destination within a national park, national historic site or national marine conservation area.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

M2 Distance		Message elements: **Text km**	
M2-01 *Layout 01, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for M2 distance signs.	**Message load** 6 elements maximum 3 destinations maximum	**Message load** 6 destinations maximum
M2-02 *Layout 02, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a narrow shoulder on a roadway. This is primarily a vehicular sign.	**Message load** 6 elements maximum 3 destinations maximum	Recommended for vehicular use only, following MUTCD convention
M2-04 *Layout 04, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign. This is primarily a pedestrian sign.	Recommended for pedestrian use only	**Message load** 6 destinations maximum

Sign Type M3
Information

M3 Distance
M3 distance signs consist of a guidance and information or emergency symbol, a text message, and distance indicator.

Their function is to communicate the distance to a specific destination within a national park, national historic site or national marine conservation area.

M3 Distance		Message elements: Text km	
M3-06 *Layout 06, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used for single destinations only when the primary message element is a single symbol.	**Message load** 1 symbol per sign maximum 1 destination maximum	**Message load** 1 symbol per sign maximum 1 destination maximum

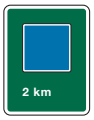
Sign Type M4

Information

M4 Distance

M4 distance signs consist of guidance and information or emergency symbols, and distance indicators.

Their function is to communicate the distance to a specific destination within a national park, national historic site or national marine conservation area.

M4 Distance		Message elements: km	
M4-07 *Layout 07, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when the primary message element is a symbol.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	**Message load** 6 symbols per destination maximum 6 destinations maximum

Sign Type M5

Information

M5 Distance Markers

M5 distance markers consist of a distance indicator.

Their function is to mark distances in support of emergency services, maintenance operations and resource conservation.

Used on Parks Canada roadways only.

M5 Distance marker		Message elements: **km**	
M5-05 *Layout 05, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
286	Since primary use is operational, consider a smaller size than the posted speed limit. This distance does not require a "km" abbreviation.	**Message load** 1 distance indicator maximum	Recommended for vehicular use only

Sign Type N1

Regulatory

N1 Prohibition

N1 prohibition signs consist of text messages and shall be used only in situations where an appropriate symbol does not exist.

Their function is to convey an order forbidding an action. They should be used as sparingly as possible so as not to detract from a positive visitor experience.

The use of a text message hierarchy is encouraged, especially for pedestrian signs, as legibility will be improved. *See Appendix F – Text Message Hierarchy.*

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

N1 Prohibition	Message elements: **Text**		
N1-10 *Layout 10, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for N1 prohibition signs.	**Message load** 1 text message maximum	**Message load** 3 text messages maximum
N1-11 *Layout 11, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway.	**Message load** 1 text message maximum	**Message load** 3 text messages maximum

Sign Type P1

Regulatory

P1 Mandatory

P1 mandatory signs consist of text messages and shall be used only in situations where an appropriate symbol does not exist.

Their function is to convey an obligatory action.

The use of a text message hierarchy is encouraged, especially for pedestrian signs, as legibility will be improved. *See Appendix F – Text Message Hierarchy.*

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

P1 Mandatory		Message elements: **Text**	
P1-12 *Layout 12, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for P1 mandatory signs.	**Message load** 1 text message maximum	**Message load** 3 text messages maximum
P1-13 *Layout 13, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway.	**Message load** 1 text message maximum	**Message load** 3 text messages maximum

Sign Type Q1

Regulatory

Q1 Prohibition and Mandatory

Q1 prohibition and mandatory signs consist of text messages and prohibition and/or mandatory symbols.

Their function is to convey an order forbidding an action (prohibitive) and/or an obligatory action (mandatory).

Prohibition signs should be used sparingly so as not to detract from a positive visitor experience.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

The layout can accommodate text message hierarchy and a combination of prohibitive and mandatory messages communicated with text and symbols.

Q1 Prohibition and mandatory		Message elements: **Text**	
Q1-14 *Layout 14, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for Q1 prohibition and mandatory signs when multiple symbols are required for a message.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 text messages maximum	**Message load** 6 symbols per message maximum 6 text messages maximum
Q1-15 *Layout 15, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a narrow shoulder on a roadway, or a single-post pedestrian sign.	**Message load** 9 elements maximum 4 symbols per sign maximum 3 text messages maximum	**Message load** 6 symbols per message maximum 6 text messages maximum
Q1-16 *Layout 16, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when no more than one symbol is required per message.	**Message load** 9 elements maximum 3 symbols per sign maximum 3 text messages maximum	**Message load** 1 symbol per message maximum 6 text messages maximum

Sign Type Q2, Q3, Q4

Regulatory

Q2, Q3, Q4 Prohibition and Mandatory

Q2, Q3, and Q4 prohibition and mandatory signs consist of prohibition and/or mandatory symbols and text messages.

Their function is to convey an order forbidding an action (prohibitive) and/or an obligatory action (mandatory).

Prohibition signs should be used sparingly so as not to detract from a positive visitor experience.

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

The layout can accommodate text message hierarchy and a combination of prohibitive and mandatory messages communicated with text and symbols.

Q2 Prohibition and mandatory	Message elements: Text		
Q2-06 *Layout 06, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used for single message only when the primary message element is a symbol.	**Message load** 1 symbol per sign maximum 1 text message maximum	**Message load** 1 symbol per sign maximum 1 text message maximum

Q3 Prohibition and mandatory	Message elements:		
Q3-07 *Layout 07, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Post-mounted symbol sign used when the only message element is a symbol.	**Message load** 4 symbols per sign maximum	**Message load** 6 symbols per sign maximum

Q4 Prohibition and mandatory	Message elements:		
Q4-09 *Layout 09, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Wall-mounted sign used when the only message element is a symbol.	**Message load** 1 symbol per sign maximum	**Message load** 1 symbol per sign maximum

Sign Type R1, R2, R3

Regulatory

R1, R2, R3 Parking Control

R1, R2, and R3 parking control signs consist of a Parks Canada signature, and a combination of either a prohibition or guidance and information symbol, text message or arrows.

Their function is to convey information about parking regulations.

When signing an area with multiple parking control signs, it is desirable to have a uniform panel size. For this reason, four standard panel sizes have been developed for the parking control series.

R1 Parking control	Message elements:		
R1-17 *Layout 17, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used for single message only when the primary message element is a symbol.	**Message load** 1 Parks Canada signature 1 symbol per sign maximum 1 text message maximum 2 directional arrows maximum **Standard sizes** Standard 15 mm unit of “x” 3 standard panel sizes	Recommended for vehicular use only in parking lots or areas where vehicle speeds range from very slow to at rest, these signs employ a 3.5:1 symbol to text hierarchy with a symbol viewing distance of approximately 30 m.

R2 Parking control	Message elements:		
R2-17 *Layout 17, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used for single message only when the primary message element is a symbol.	**Message load** 1 Parks Canada signature 1 symbol per sign maximum 1 text message maximum **Standard sizes** Standard 15 mm unit of “x” with a viewing distance of 9 m 4 standard panel sizes	Recommended for vehicular use only in parking lots or areas where vehicle speeds range from very slow to at rest, these signs employ a 3.5:1 symbol to text hierarchy with a symbol viewing distance of approximately 30 m.

R3 Parking control	Message elements:		
R3-17 *Layout 17, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used for single message only when the primary message element is a symbol.	**Message load** 1 Parks Canada signature 1 symbol per sign maximum 2 directional arrows maximum **Standard sizes** Standard 15 mm unit of “x” with a viewing distance of 9 m 1 standard panel size	Recommended for vehicular use only in parking lots or areas where vehicle speeds range from very slow to at rest, these signs employ a 3.5:1 symbol to text hierarchy with a symbol viewing distance of approximately 30 m.

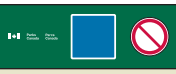
Sign Type R4, R5, R6

Regulatory

R4, R5, R6 Parking Control

R4, R5 and R6 parking control signs consist of the Parks Canada signature and either a prohibition or guidance and information symbol or a text message with or without arrows.

Their function is to convey information about parking regulations.

R4 Parking control	Message elements: 	
R4-17 *Layout 17, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Used for single message only when the primary message element is a symbol.	**Message load** 1 Parks Canada signature 1 symbol per sign maximum **Standard sizes** Standard 15 mm unit of “x” with a viewing distance of 9 m 1 standard panel size	Recommended for vehicular use only in parking lots or areas where vehicle speeds range from very slow to at rest, these signs employ a 3.5:1 symbol to text hierarchy with a symbol viewing distance of approximately 30 m.

R5 Parking control	Message elements: Text ← →	
R5-17 *Layout 17, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Used for single message only when the primary message element is text.	**Message load** 1 Parks Canada signature 1 text message maximum 2 directional arrows maximum **Standard sizes** Standard 15 mm unit of “x” with a viewing distance of 9 m 3 standard panel sizes	Recommended for vehicular use only in parking lots or areas where vehicle speeds range from very slow to at rest.

R6 Parking control	Message elements: Text	
R6-17 *Layout 17, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Used for single message only when the primary message element is text.	**Message load** 1 Parks Canada signature 1 text message maximum **Standard sizes** Standard 15 mm unit of “x” with a viewing distance of 9 m 3 standard panel sizes	Recommended for vehicular use only in parking lots or areas where vehicle speeds range from very slow to at rest.

Sign Type S1

Regulatory

S1 Boundary Markers

S1 boundary markers consist of the Parks Canada signature, a text message and the Parks Canada identifier (combination of beaver symbol and the "Canada" wordmark).

Their function is to identify the perimeter of a national park, national historic site or national marine conservation area.

A limited supply of these signs have been pre-fabricated and are available by contacting the Parks Canada Identity Program Officer at pimpc-pcip@pc.gc.ca.

S1 Boundary marker		Message elements: Text	
S1-18 *Layout 18, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used for single message only. There is no legal requirement to install boundary markers around the perimeter of a national park, national historic site, national marine conservation area, or portion thereof, especially in cases where visitors are clearly aware that they are on Parks Canada property. Site managers should use their own discretion in determining whether and where to install these markers. Boundary markers may be useful at strategic locations to help deter inappropriate or illegal public activities, where it is not obvious to visitors that they are entering federal property, or where their entry and inappropriate use of the land may pose a threat to sensitive cultural and natural resources. In such circumstances, the markers may assist Parks Canada in taking legal action. In cases where sensitive resources are located some distance away from the boundary of a site, it may be more appropriate to consider an additional or alternative form of signage.	**Message load** 1 Parks Canada signature 1 text message maximum 1 Parks Canada identifier	**Message load** 1 Parks Canada signature 1 text message maximum 1 Parks Canada identifier

Sign Type T1

Warning

T1 Caution

T1 caution signs consist of text messages and should be used only in situations where an appropriate symbol does not exist.

Their function is to identify a potential hazard.

The use of a text message hierarchy is encouraged. The word "Caution!" should always be followed by a warning message. *See Appendix F – Text Message Hierarchy.*

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

T1 Caution		Message elements: **Text**	
T1-10 *Layout 10, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for T1 caution signs. In situations where the primary text message is equivalent in both spelling and meaning in English and French, Attention for example, that term can be presented once, introducing the secondary text message in both languages.	**Message load** 1 primary text message maximum 1 secondary text message maximum	**Message load** 1 primary text message maximum 3 secondary text messages maximum
T1-11 *Layout 11, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway. In situations where the primary text message is equivalent in both spelling and meaning in English and French, that term can be presented once, introducing the secondary text message in both languages.	**Message load** 1 primary text message maximum 1 secondary text message maximum	**Message load** 1 primary text message maximum 3 secondary text messages maximum

See *Section 3.4 – Duty of Care* for more information on use and placement of warning signs

Sign Type U1

Warning

U1 Danger

U1 danger signs consist of text messages and should be used only in situations where an appropriate symbol does not exist.

Their function is to identify a definite hazard.

The use of a text message hierarchy is mandatory. The word "Danger!" is always followed by a warning message. *See Appendix F – Text Message Hierarchy.*

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

U1 Danger		Message elements: **Text**	
U1-12 *Layout 12, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	This is the preferred layout for U1 danger signs. In situations where the primary text message is equivalent in both spelling and meaning in English and French, Danger for example, that term can be presented once, introducing the secondary text message in both languages.	**Message load** 1 primary text message maximum 1 secondary text message maximum	**Message load** 1 primary text message maximum 3 secondary text messages maximum
U1-13 *Layout 13, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used when space is inadequate for side-by-side language format, or when there is sufficient functional reason for an over-under message, such as a single-post pedestrian sign or a narrow shoulder on a roadway. In situations where the primary text message is equivalent in both spelling and meaning in English and French, Danger for example, that term can be presented once, introducing the secondary text message in both languages.	**Message load** 1 primary text message maximum 1 secondary text message maximum	**Message load** 1 primary text message maximum 3 secondary text messages maximum

See *Section 3.4 – Duty of Care* for more information on use and placement of warning signs

Sign Type V1

Warning

V1 Warning

V1 warning signs consist of a warning symbol and text message.

Their function is to identify a potential (caution) or definite (danger) hazard.

The use of a text message hierarchy is encouraged. The word "Danger!" or "Caution!" is always followed by a warning message. *See Appendix F – Text Message Hierarchy.*

The message must be bilingual and appear in a side-by-side format unless space is inadequate or there is sufficient functional reason for an over-under message.

V1 Warning		Message elements: ▲ ▲ **Text**	
V1-06 *Layout 06, Appendix E*		**Vehicular specifications**	**Pedestrian specifications**
	Used for single messages only when the primary message element is a single symbol. In situations where the primary text message is equivalent in both spelling and meaning in English and French, Attention or Danger for example, that term can be presented once, introducing the secondary text message in both languages.	**Message load** 1 symbol per sign maximum 1 text message maximum	**Message load** 1 symbol per sign maximum 1 text message maximum

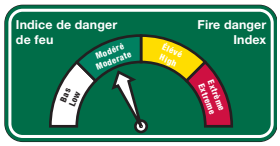
Sign Type W1, W2

Warning

W1-19 Wave Hazard & W2-20 Fire Hazard

Special purpose sign designed with a changeable component to identify various levels of risk. Must be used in accordance with *Section 3.4 – Duty of Care* requirements.

W1 Wave Hazard	Message elements:	
W1-19 *Layout 19, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
Under development	Under development	Under development

W2 Fire Hazard	Message elements:	
W2-20 *Layout 20, Appendix E*	**Vehicular specifications**	**Pedestrian specifications**
 Under development	Under development	Under development

E

Appendix E – Layout Specifications

Appendix E – Layout Specifications

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Introduction

Appendix E provides detailed sign layout instructions to assist the designer when preparing general assembly drawings.

It is organized by layout number. Each layout, and associated specifications, can support several sign types that share the same layout instructions.

The sign schedule provided by a sign planner includes detailed information on each sign required within a sign plan. The layout number indicated in the sign schedule will direct the designer to the selected sign layout.

Layout 01

Sign types: K1, K2, L1, L2, L3, L4, M1, M2

Identification

K1

K2

Directional

L1

L2

L3

L4

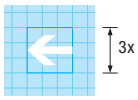
Distance

M1

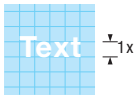
M2

Size and colour

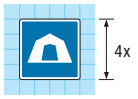
Arrow
Size: 3x
Colour: White



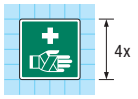
Text message and distance
Size: 1x
Colour: White
Tracking: 50/1000 of an em



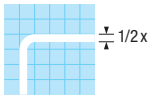
Guidance and information symbol
Size: 4x
Colour: White (symbol image, symbol border);
CSA Blue (symbol background)



Emergency symbol
Size: 4x
Colour: White (symbol image, symbol border);
CSA Green (symbol background)



Border
Size: 1/2x
Colour: White



Panel background
Colour: PC Heritage Green

Vehicular	Pedestrian
Message load 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	**Message load** 6 symbols per destination maximum 6 destinations maximum
Arrow sequence Top to bottom: ↑ through destination ← left → right	**Arrow sequence** No prescribed sequence

Layout 01 (CONTINUED)

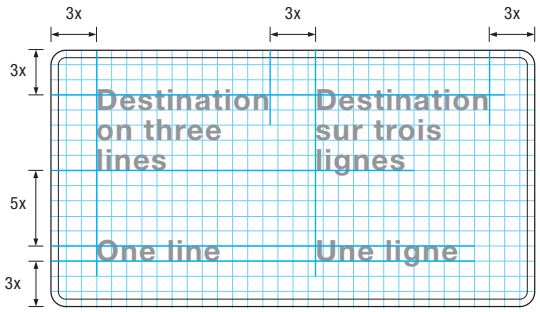
Layout 01 – Spacing and Alignment

A Top of arrow (key line) is placed 1x above x-height of first line of text message.

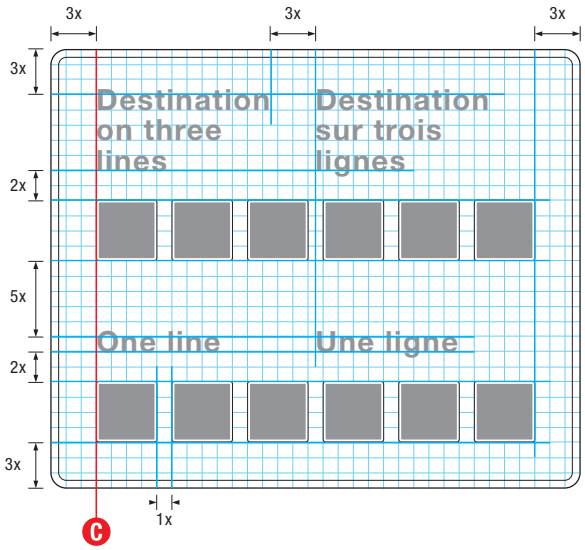
B Baseline of distance indicator is aligned with baseline of first line of text message.

C Symbol is left aligned with text message.

K1-01 Identification

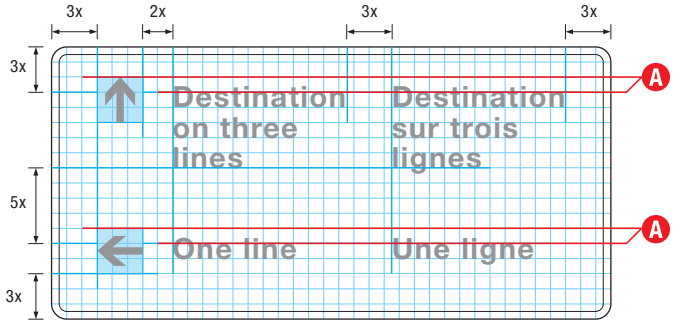


K2-01 Identification

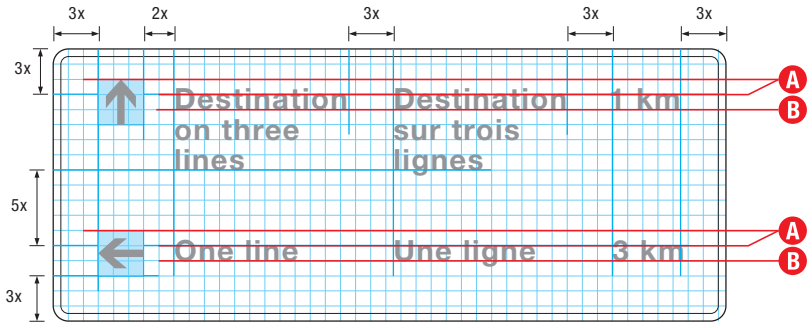


Layout 01 (CONTINUED)

L1-01 Directional



L2-01 Directional



Layout 01 (CONTINUED)

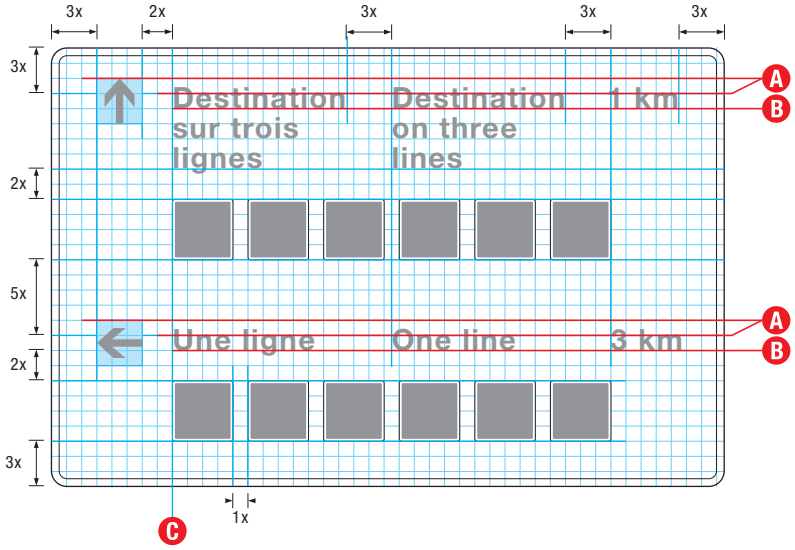
Layout 01 – Spacing and Alignment

A Top of arrow (key line) is placed 1x above x-height of first line of text message.

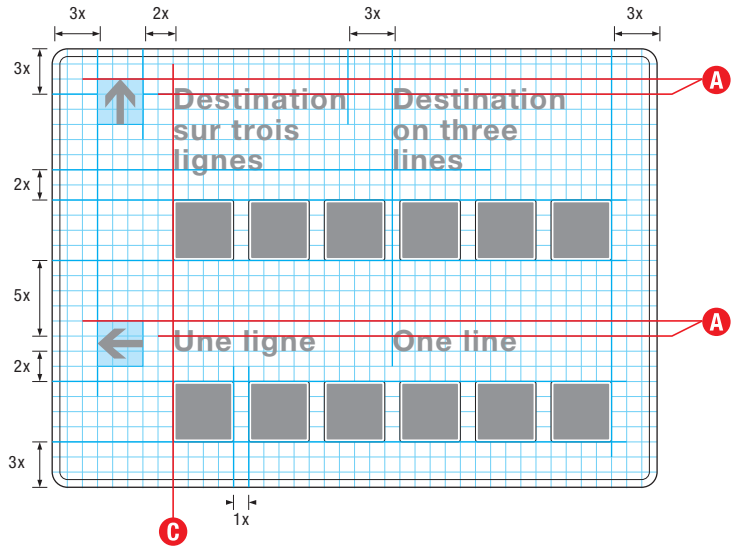
B Baseline of distance indicator is aligned with baseline of first line of text message.

C Symbol is left aligned with text message.

L3-01 Directional

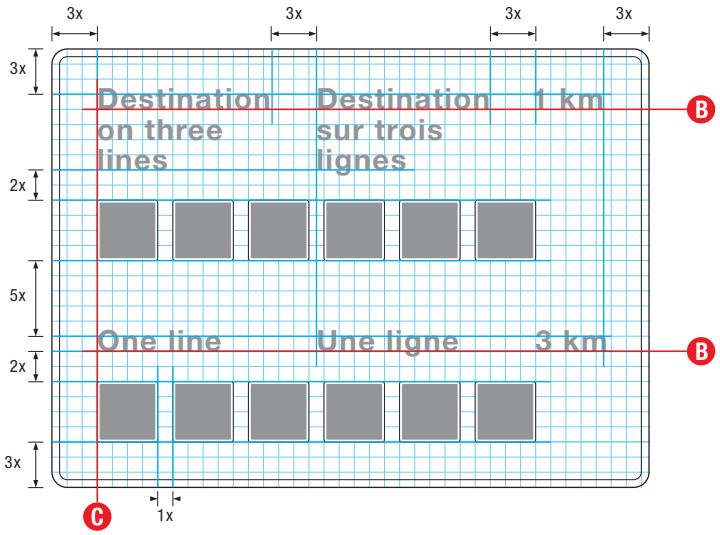


L4-01 Directional

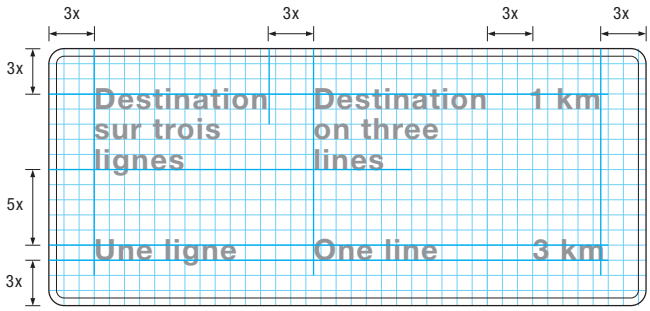


Layout 01 (CONTINUED)

M1-01 Distance

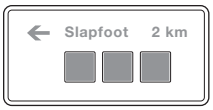


M2-01 Distance

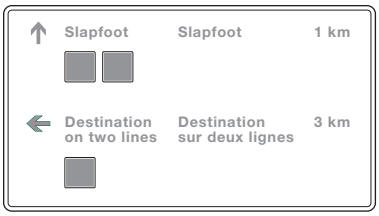


Layout 01 – Exceptions and Special Cases

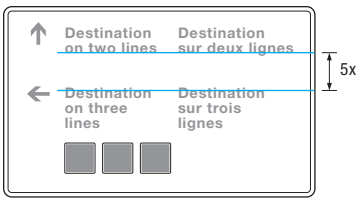
Sign Types: K1, K2, L1, L2, L3, L4, M1, M2
Messages composed exclusively of specific geographic names do not require translation, and do not have to be repeated on a single-destination sign.



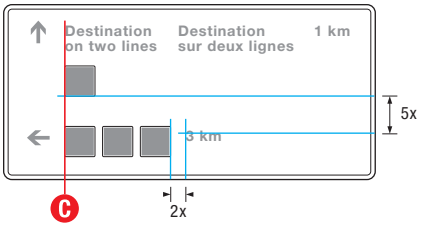
Sign Types: K1, K2, L1, L2, L3, L4, M1, M2
Messages composed exclusively of specific geographic names that do not require translation must be repeated on a multiple-destination sign, unless all destination names are specific and do not require translation.



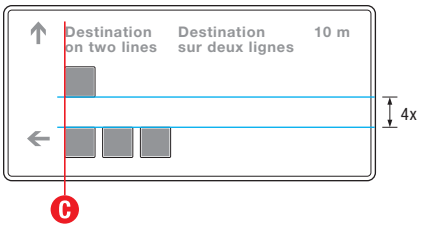
Sign Types: K2, L3, L4, M1
When a symbol is not required in one destination, standard 5x spacing is maintained between message units, measured from baseline to x-height.



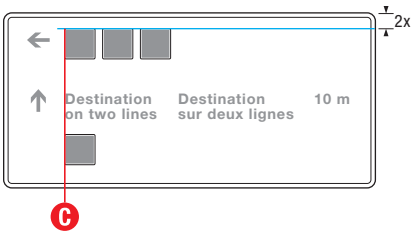
Sign Types: L3, M1
When the text message is not required in one destination, the symbol is left aligned with the text message in the preceding (or following) destination. Standard 5x spacing is maintained between destinations measured from the symbol to the x-height of the distance indicator (distance). Standard 2x spacing is maintained between the symbol and the distance indicator.



Sign Types: L4, M1
When the text message is not required in one destination on a multiple destination sign, the symbol is left aligned with the text message in the preceding (or following) destination; 4x spacing is maintained between symbols.



Sign Types: L3, L4, M1
When the text message is not required in the top destination on a multiple destination sign, the symbol is left aligned with the text message in the following destination; space above symbol is 2x.



Layout 02

Sign types: K1, K2, L1, L2, L3, L4, M1, M2

Identification

K1

K2

Directional

L1

L2

L3

L4

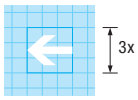
Distance

M1

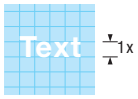
M2

Size and colour

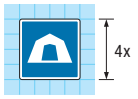
Arrow
Size: 3x
Colour: White



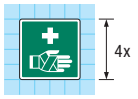
Text message and distance
Size: 1x
Colour: White
Tracking: 50/1000 of an em



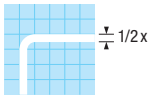
Guidance and information symbol
Size: 4x
Colour: White (symbol image, symbol border);
CSA Blue (symbol background)



Emergency symbol
Size: 4x
Colour: White (symbol image, symbol border);
CSA Green (symbol background)



Border
Size: 1/2x
Colour: White



Panel background
Colour: PC Heritage Green

Vehicular	Pedestrian
Message load 9 elements maximum 4 symbols per sign maximum 3 destinations or text messages maximum	**Message load (K1, K2, L1, L4)** 6 symbols per destination maximum 6 destinations maximum **Message load (L2, L3, M1, M2)** Recommended for vehicular use only, following MUTCD convention.
Arrow sequence Top to bottom: ↑ through destination ← left → right	**Arrow sequence** No prescribed sequence

Layout 02 (CONTINUED)

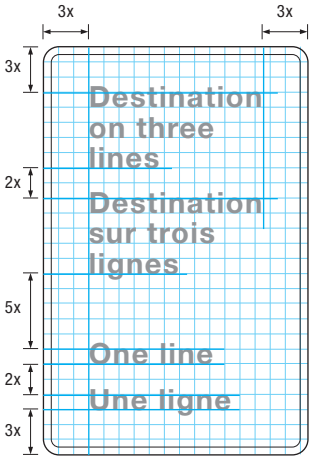
Layout 02 – Spacing and Alignment

A Top of arrow (key line) is placed 1x above x-height of first line of text message.

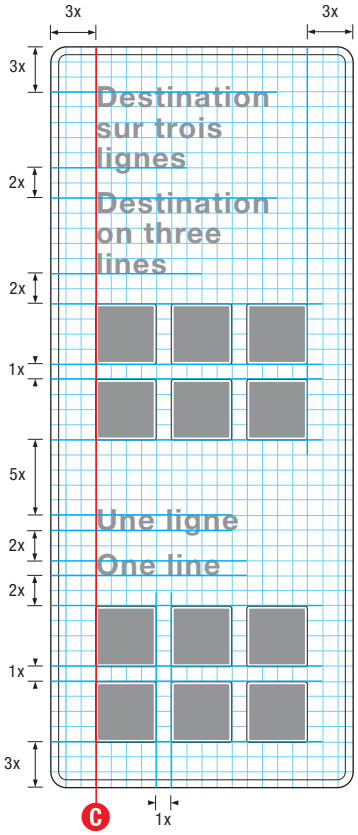
B Baseline of distance indicator is aligned with baseline of first line of text message.

C Symbol is left aligned with text message.

K1-02 Identification

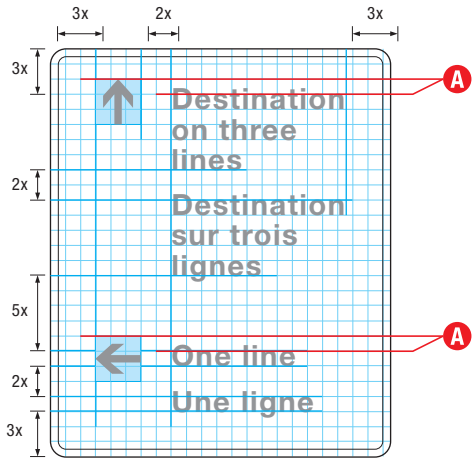


K2-02 Identification

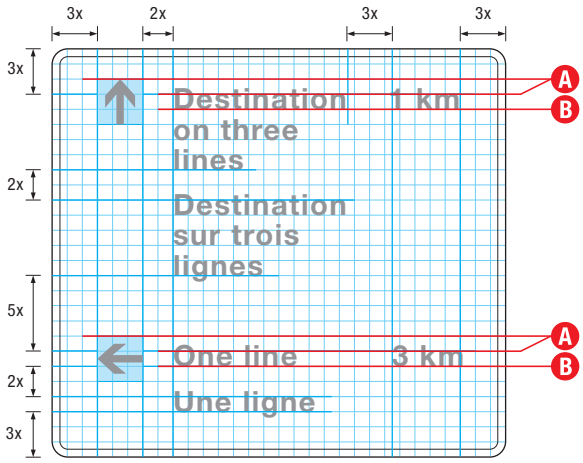


Layout 02 (CONTINUED)

L1-02 Directional



L2-02 Directional



Layout 02 (CONTINUED)

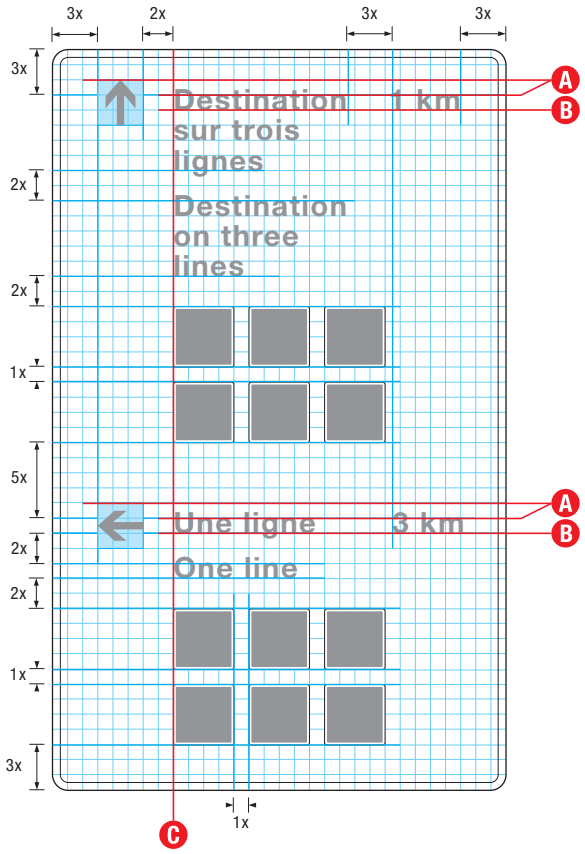
Layout 02 – Spacing and Alignment

A Top of arrow (key line) is placed 1x above x-height of first line of text message.

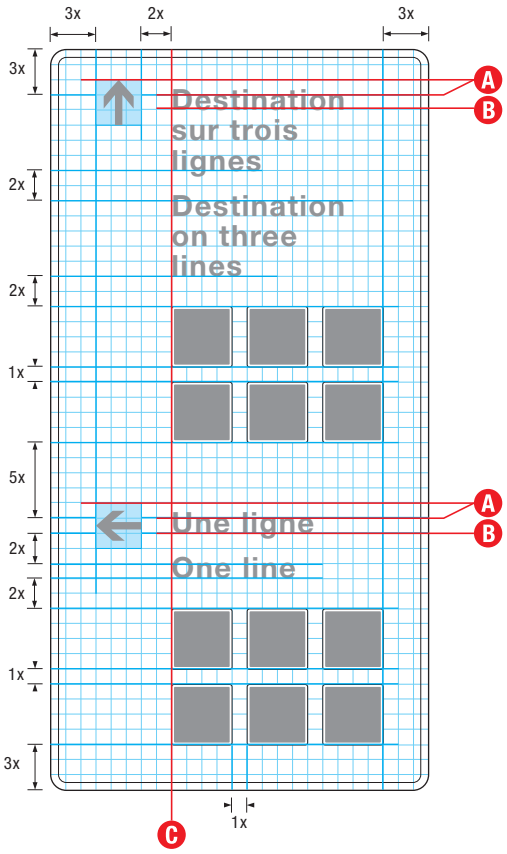
B Baseline of distance indicator is aligned with baseline of first line of text message.

C Symbol is left aligned with text message.

L3-02 Directional

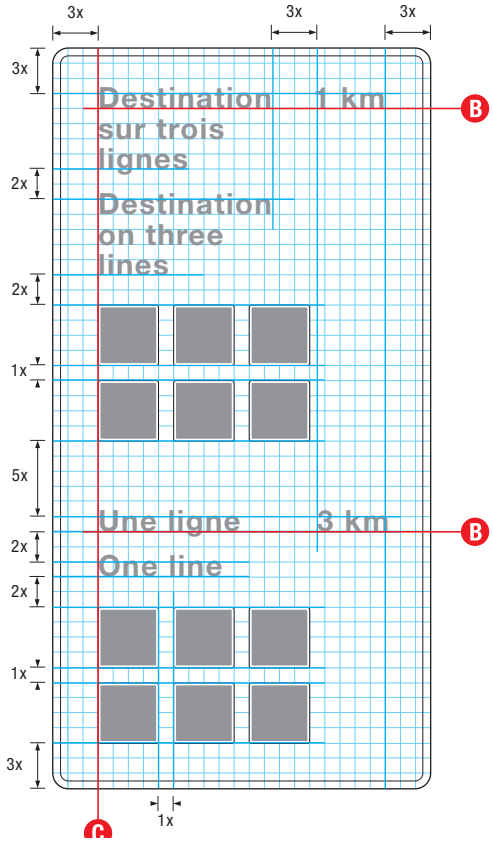


L4-02 Directional

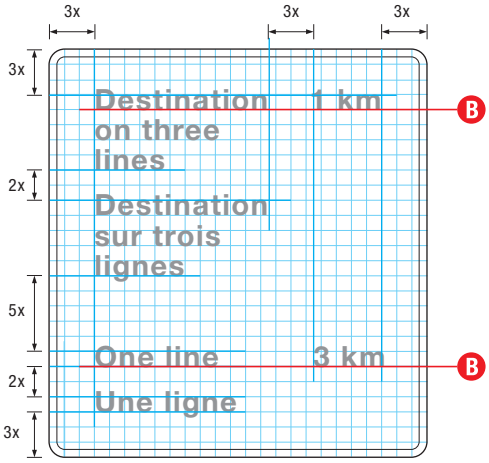


Layout 02 (CONTINUED)

M1-02 Distance

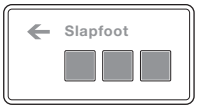


M2-02 Distance

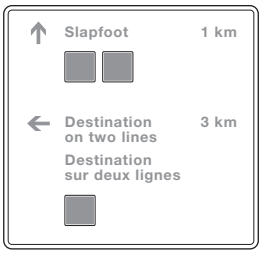


Layout 02 – Exceptions and Special Cases

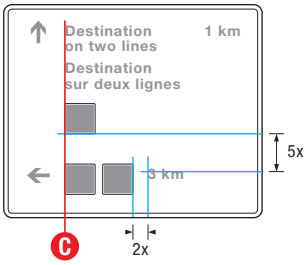
Sign Types: K1, K2, L1, L2, L3, L4, M1, M2
Messages composed exclusively of specific geographic names do not require translation, and do not have to be repeated on a single-destination sign.



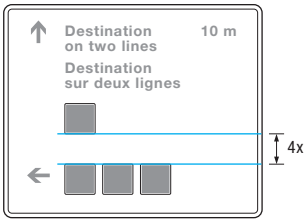
Sign Types: K1, K2, L1, L2, L3, L4, M1, M2
Messages composed exclusively of specific geographic names that do not require translation do not have to be repeated on an over-under format multiple-destination sign.



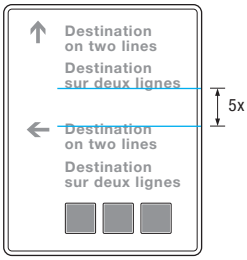
Sign Types: K2, L3, L4, M1
When the text message is not required in one destination, the symbol is left aligned with the text message in the preceding (or following) destination. Standard 5x spacing is maintained between destinations measured from the symbol to the x-height of the distance indicator (distance). Standard 2x spacing is maintained between the symbol and the distance indicator.



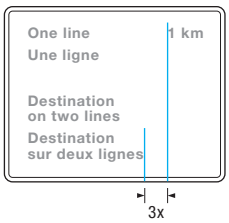
Sign Types: L3, L4, M1
When the text message and distance indicator are not required in one destination, the symbol is left aligned with the text message in the preceding (or following) destination; 4x spacing is maintained between symbols.



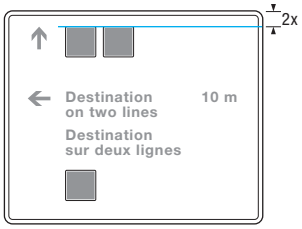
Sign Types: L3, L4, M1
When symbols are not required in one destination, standard 5x spacing is maintained between message units, measured from baseline to x-height.



Sign Types: L2, L3, M1, M3
When the distance indicator is not required in one destination, standard 3x spacing is maintained between the longest line of text on the sign and the distance indicator.



Sign Types: L3, L4, M1
When the text message is not required in the top destination on a multiple destination sign, the symbol is left aligned with the text message in the following destination; space above symbol is 2x.



Layout 03

Sign types: K2, L3, L4, M1

Identification

K2

Distance

M1

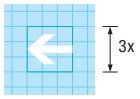
Directional

L3

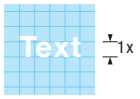
L4

Size and colour

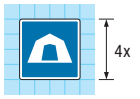
Arrow
Size: 3x
Colour: White



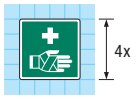
Text message and distance
Size: 1x
Colour: White
Tracking: 50/1000 of an em



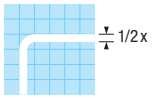
Guidance and information symbol
Size: 4x
Colour: White (symbol image, symbol border);
CSA Blue (symbol background)



Emergency symbol
Size: 4x
Colour: White (symbol image, symbol border);
CSA Green (symbol background)



Border
Size: 1/2x
Colour: White



Panel background
Colour: PC Heritage Green

Vehicular	Pedestrian
Message load 9 elements maximum 3 symbols per sign maximum 3 destinations maximum	**Message load** 1 symbol per destination maximum 6 destinations maximum
Arrow sequence Top to bottom: ↑ through destination ← left → right	**Arrow sequence** No prescribed sequence

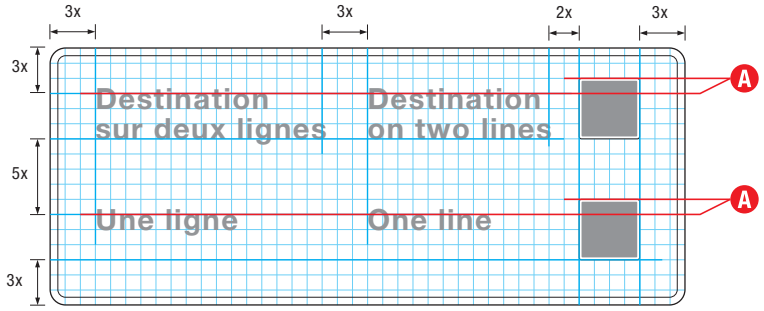
Layout 03 (CONTINUED)

Layout 03 – Spacing and Alignment

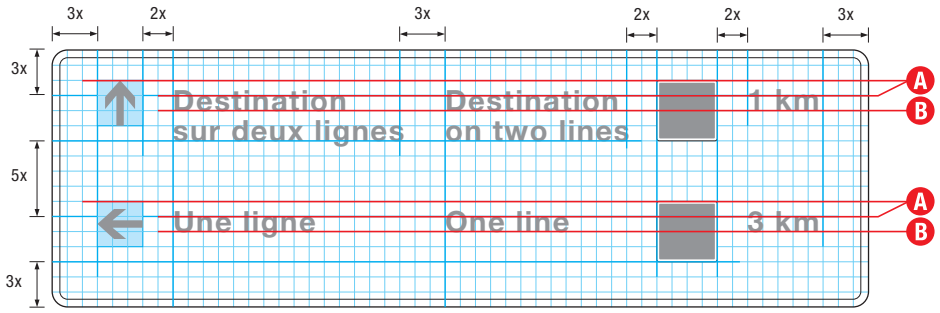
A Top of arrow (key line) is placed 1x above x-height of first line of text message.

B Baseline of distance indicator is aligned with baseline of first line of text message.

K2-03 Identification

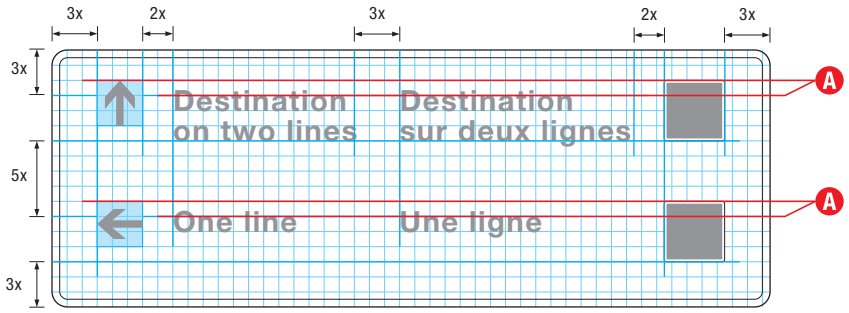


L3-03 Directional

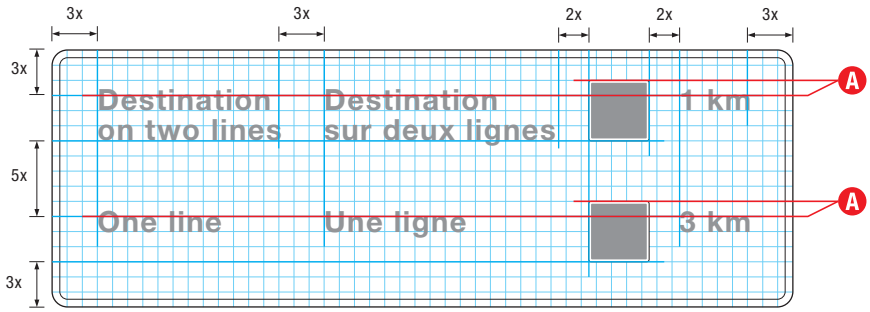


Layout 03 (CONTINUED)

L4-03 Directional



M1-03 Distance



Layout 03 (CONTINUED)

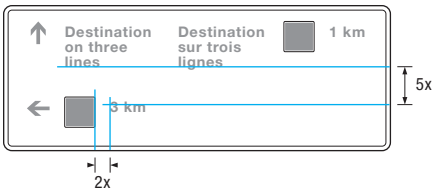
Layout 03 – Exceptions and Special Cases

Sign Types: K2, L3, L4, M1
Messages composed exclusively of specific geographic names do not require translation, and do not have to be repeated on a single-destination sign.

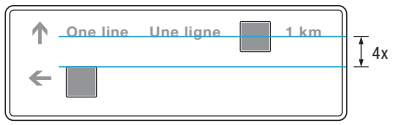
Sign Types: K2, L3, L4, M1
Messages composed exclusively of specific geographic names that do not require translation must be repeated on a multiple-destination sign, unless all destination names are specific and do not require translation.

Sign Types: L3, L4, M1
When a symbol is not required in one destination, standard 5x spacing is maintained between message units, measured from baseline to x-height.

Sign Types: L3, M1
When the text message is not required in one destination, the symbol is left aligned with the text message in the preceding (or following) destination. Standard 5x spacing is maintained between destinations measured from the symbol to the x-height of the distance indicator (distance). Standard 2x spacing is maintained between the symbol and the distance indicator.



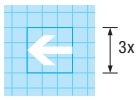
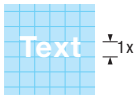
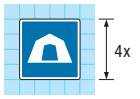
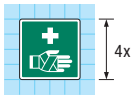
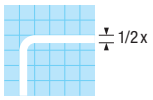
Sign Types: L3, L4, M1
When the text message is not required in one destination on a multiple-destination sign, the symbol is left aligned with the text message in the preceding (or following) destination; 4x spacing is maintained between text and symbol.



Sign Types: L3, L4, M1
When the text message is not required in the top destination on a multiple-destination sign, the symbol is left aligned with the text message in the following destination; space above symbol is 2x.

Layout 04

Sign types: L2, L3, M1, M2	
Directional	**Distance**
L2 L3 	M1 M2

Size and colour	
Arrow Size: 3x Colour: White	
Text message and distance Size: 1x Colour: White Tracking: 50/1000 of an em	
Guidance and information symbol Size: 4x Colour: White (symbol image, symbol border); CSA Blue (symbol background)	
Emergency symbol Size: 4x Colour: White (symbol image, symbol border); CSA Green (symbol background)	
Border Size: 1/2x Colour: White	
Panel background Colour: PC Heritage green	

Vehicular	Pedestrian
Message load Recommended for pedestrian use only.	**Message load** 6 symbols per destination maximum 6 destinations maximum
Arrow sequence Recommended for pedestrian use only.	**Arrow sequence** No prescribed sequence

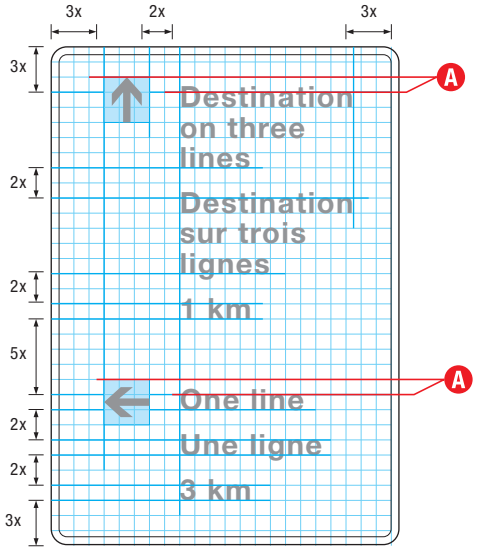
Layout 04 (CONTINUED)

Layout 04 – Spacing and Alignment

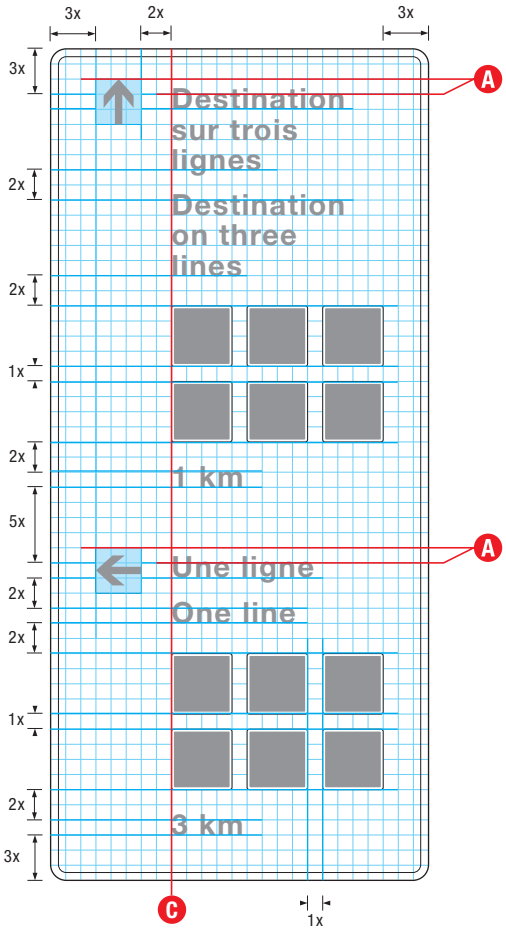
A Top of arrow (key line) is placed 1x above x-height of first line of text message.

C Symbol is left aligned with text message.

L2-04 Directional

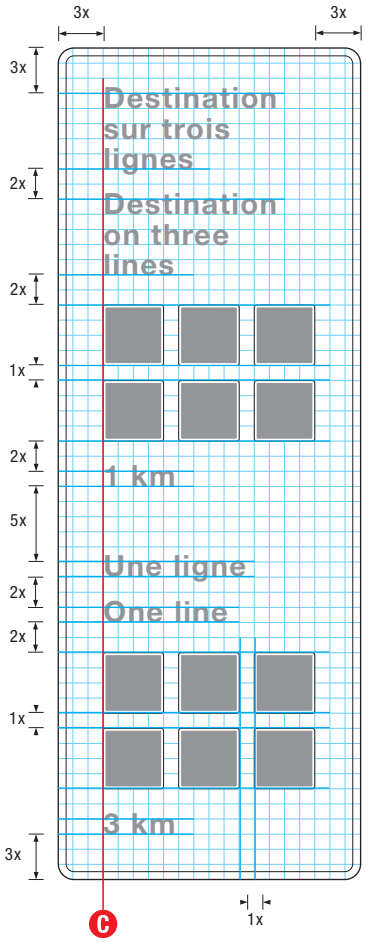


L3-04 Directional

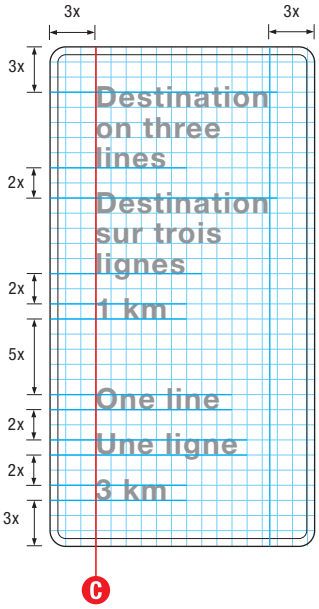


Layout 04 (CONTINUED)

M1-04 Distance

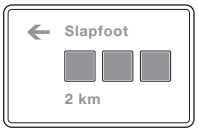


M2-04 Distance



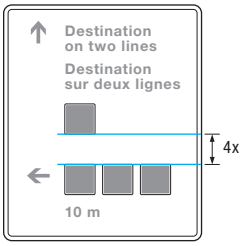
Layout 04 – Exceptions and Special Cases

Sign Types: L2, L3, M1, M2
Messages composed exclusively of specific geographic names do not require translation, and do not have to be repeated on a single-destination sign.

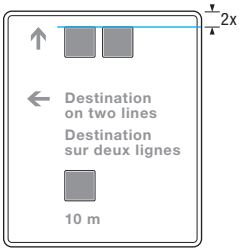


Sign Types: L2, L3, M1, M2
Messages composed exclusively of specific geographic names that do not require translation do not have to be repeated on an over-under format multiple-destination sign.

Sign Types: L3, M1
When the text message and distance indicator are not required in one destination, the symbol is left aligned with the text message in the preceding (or following) destination; 4x spacing is maintained between symbols.

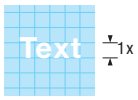
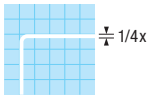


Sign Types: L3, M1
When the text message is not required in the top destination on a multiple-destination sign, the symbol is left aligned with the text message in the following destination; space above symbol is 2x.



Layout 05

Sign types: J1, J2, M5	
Campsite identification	**Distance indicator**
J1 J2	M5

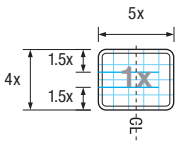
Size and colour	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/4x Colour: White	
Panel background Colour: PC Heritage Green	

Vehicular	Pedestrian
Message load 1 text message maximum	**Message load (J1, J2)** 1 text message maximum **Message load (M5)** Recommended for vehicular use only.

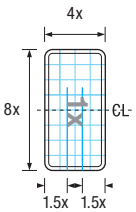
Layout 05 – Spacing and Alignment

If necessary, the horizontal dimension (J1, M5) can be increased to accommodate 4 or more digits.

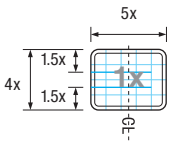
J1-05 Campsite Identification



J2-05 Campsite Identification



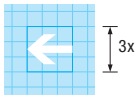
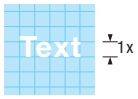
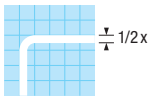
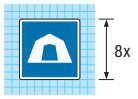
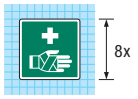
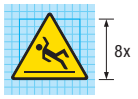
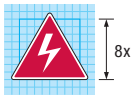
M5-05 Distance Indicator



Layout 06

Sign types: K3, L5, L6, M3, Q2, V1		
Identification	**Directional**	**Distance**
K3 	L5 L6 	M3
Prohibition and mandatory	**Caution and danger**	
Q2 	V1 	

Layout 06 (CONTINUED)

Size and colour	
Arrow Size: 3x Colour: White	
Text message and distance Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Panel background Colour: PC Heritage Green	
Guidance and information symbol Size: 8x Colour: White (symbol image, symbol border); CSA Blue (symbol background)	
Emergency symbol Size: 8x Colour: White (symbol image, symbol border); CSA Green (symbol background)	
Prohibition symbol Size: 8x Colour: White (symbol background, symbol border); Black (symbol image); CSA Red (prohibition slash)	
Mandatory symbol Size: 8x Colour: White (symbol image, symbol border); Black (symbol background)	
Caution symbol Size: 8x Colour: CSA Yellow (symbol background); Black (symbol image, symbol border)	
Danger symbol Size: 8x Colour: White (symbol image, symbol border); CSA Red (symbol background)	

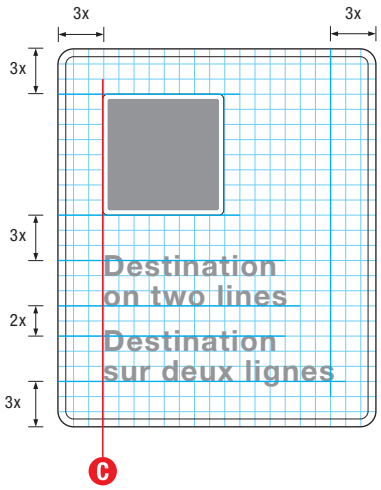
Vehicular	Pedestrian
Message load 1 symbol per sign maximum 1 destination or text message maximum	**Message load** 1 symbol per sign maximum 1 destination or text message maximum

Layout 06 (CONTINUED)

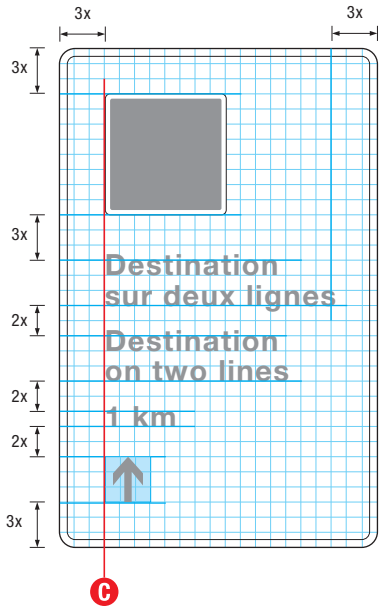
Layout 06 – Spacing and Alignment

C Symbol is left aligned with text message.

K3-06 Identification

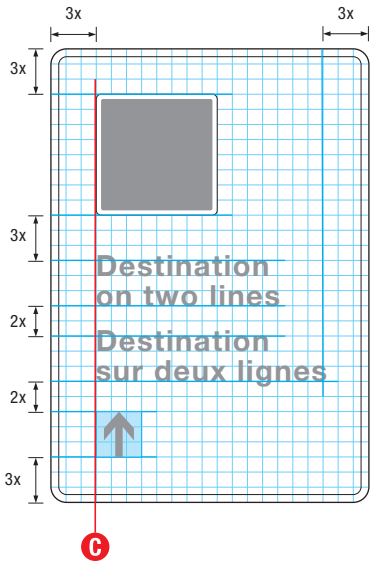


L5-06 Directional

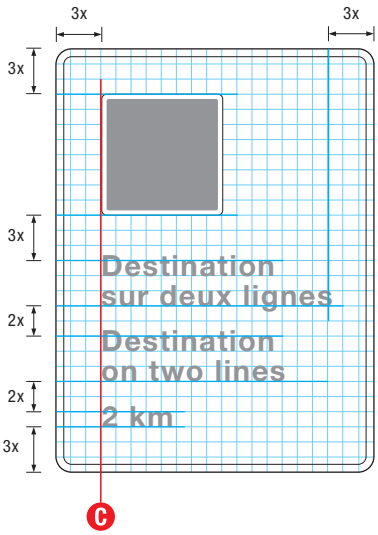


Layout 06 (CONTINUED)

L6-06 Directional



M3-06 Distance



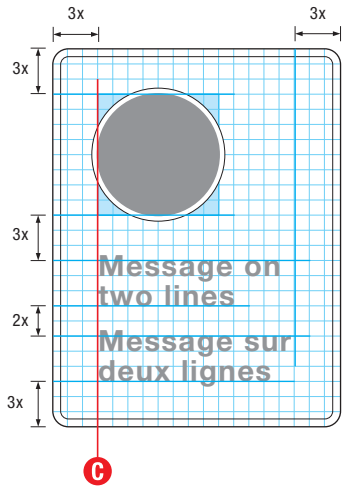
Layout 06 (CONTINUED)

Layout 06 – Spacing and Alignment

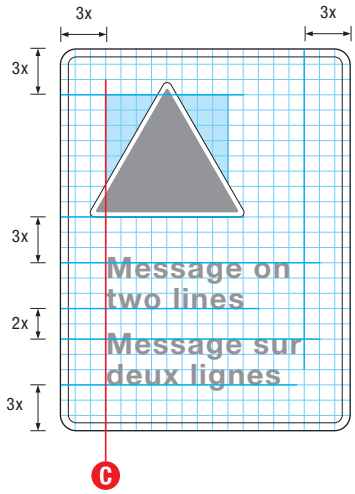
C Symbol is left aligned with text message.

D Spacing for margins, gutters and rows is variable, dependent upon the primary text message size. See *Appendix F – Text Message Hierarchy.*

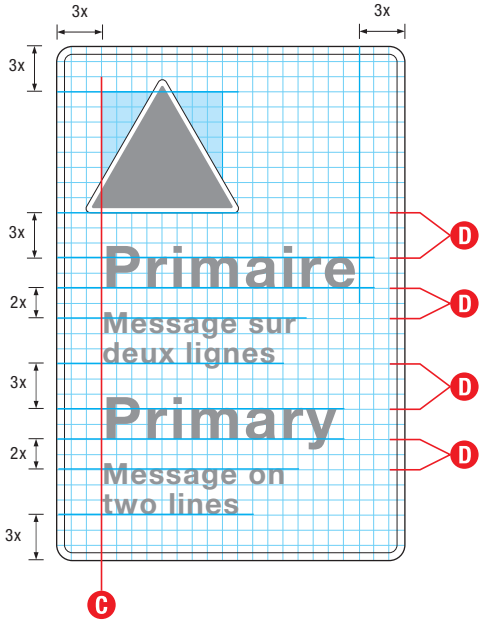
Q2-06 Prohibition and Mandatory



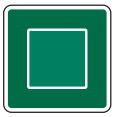
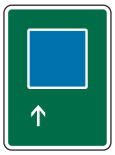
V1-06 Caution and Danger

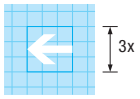
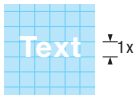
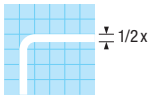
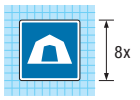
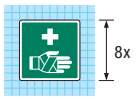


V1-06 Caution and Danger



Layout 07

Sign types: K4, L7, L8, M4, Q3			
Identification	**Directional**	**Distance**	**Prohibition and mandatory**
K4 	L7 L8 	M4	Q3

Size and colour	
Arrow Size: 3x Colour: White	
Text message and distance Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Panel background Colour: PC Heritage green	
Guidance and information symbol Size: 8x Colour: White (symbol image, symbol border); CSA Blue (symbol background)	
Emergency symbol Size: 8x Colour: White (symbol image, symbol border); CSA Green (symbol background)	
Prohibition symbol Size: 8x Colour: White (symbol background, symbol border); Black (symbol image); CSA Red (prohibition slash)	
Mandatory symbol Size: 8x Colour: White (symbol image, symbol border); Black (symbol background)	

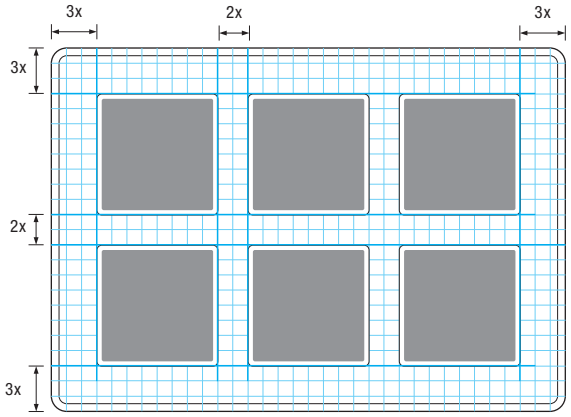
Vehicular	Pedestrian
Message load 9 elements maximum 4 symbols per sign maximum 3 destinations maximum	**Message load** 6 symbols per destination maximum 6 destinations maximum

Layout 07 (CONTINUED)

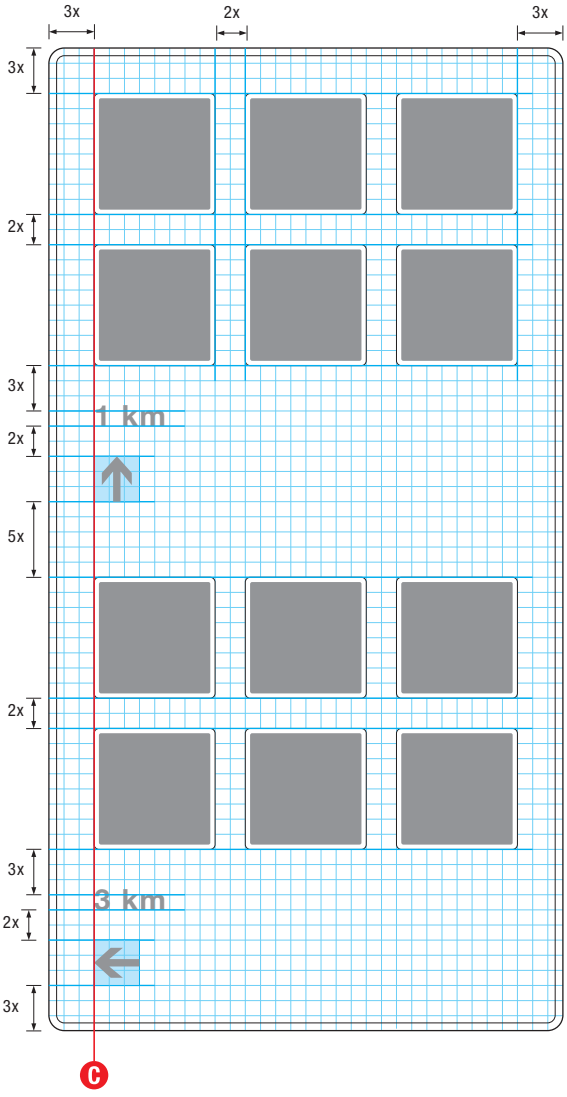
Layout 07 – Spacing and Alignment

C Symbol is left aligned with text message.

K4-07 Identification

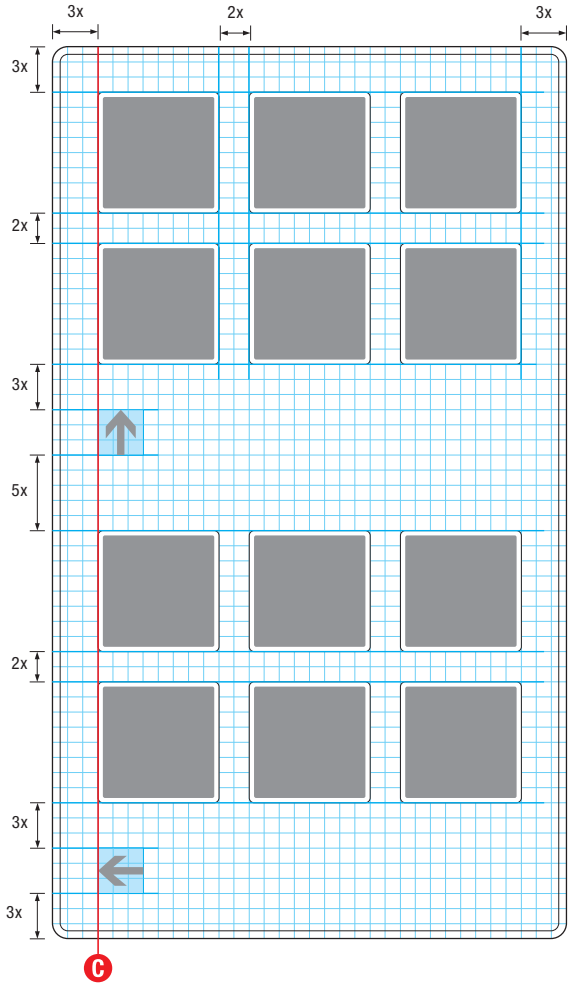


L7-07 Directional

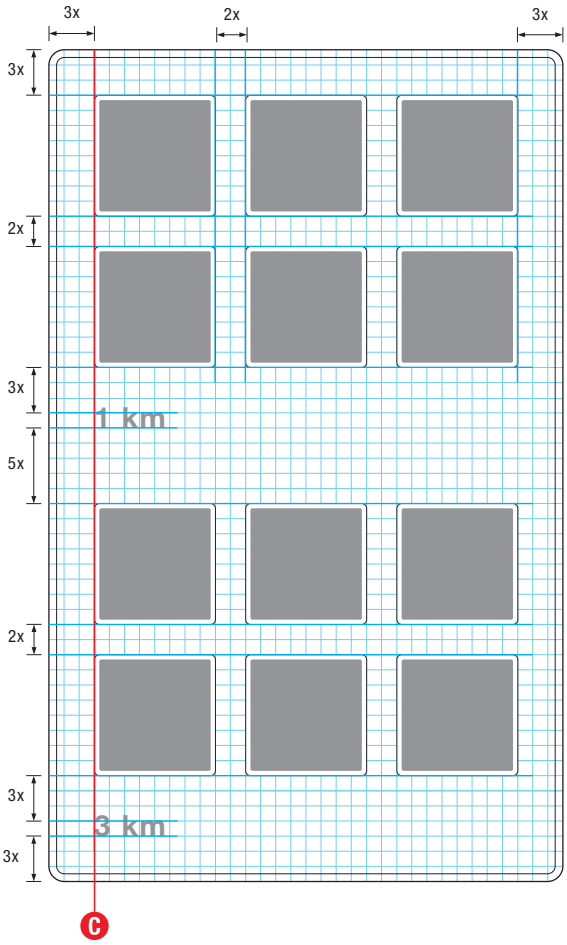


Layout 07 (CONTINUED)

L8-07 Directional

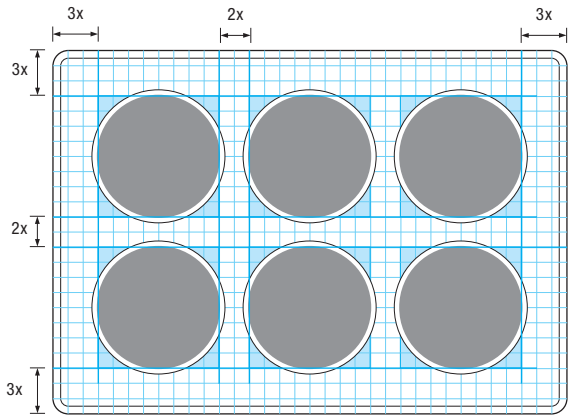


M4-07 Distance



Layout 07 – Spacing and Alignment

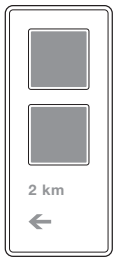
Q3-07 Prohibition and Mandatory



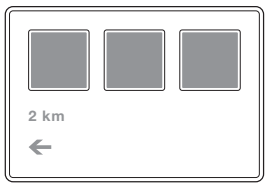
Layout 07 (CONTINUED)

Layout 07 – Exceptions and Special Cases

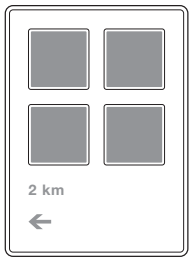
Sign Types: K4, L7, L8, M4, Q3
Preferred layout for a two-symbol sign is to align the symbols vertically.



Sign Types: K4, L7, L8, M4, Q3
Preferred layout for a three-symbol sign is to align the symbols horizontally.

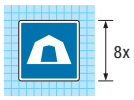
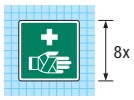
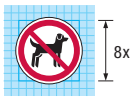


Sign Types: K4, L7, L8, M4, Q3
Preferred layout for a four-symbol sign is to have two rows of two symbols.



Layout 08

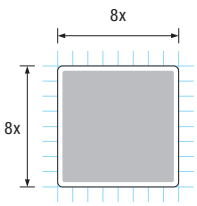
Sign types: K5, Q4	
Identification	**Prohibition and mandatory**
K5	Q4

Size and colour	
Guidance and information symbol Size: 8x Colour: White (symbol image, symbol border); CSA Blue (symbol background)	
Emergency symbol Size: 8x Colour: White (symbol image, symbol border); CSA Green (symbol background)	
Prohibition symbol Size: 8x Colour: White (symbol background, symbol border); Black (symbol image); CSA Red (prohibition slash)	
Mandatory symbol Size: 8x Colour: White (symbol image, symbol border); Black (symbol background)	

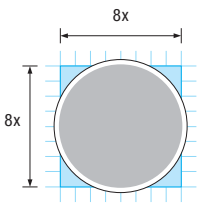
Vehicular	Pedestrian
Message load 1 symbol per sign maximum	**Message load** 1 symbol per sign maximum

Layout 08 – Spacing and Alignment

K5-08 Identification

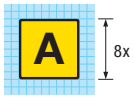
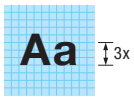
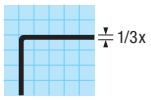
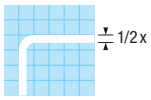


Q4-09 Prohibition and Mandatory



Layout 09

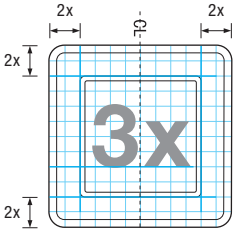
Sign type: K6
Identification
K6

Size and colour	
Portage symbol Size: 8x Colour: Black (symbol text, symbol border); CSA Yellow (symbol background)	
Text Size: 3x Colour: Black Tracking: 50/1000 of an em	
Symbol Border Size: 1/3x Colour: Black	
Sign Border Size: 1/2x Colour: White	
Panel background Colour: PC Heritage Green	

Vehicular	Pedestrian
Message load 1 symbol per sign maximum	**Message load** 1 symbol per sign maximum

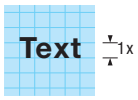
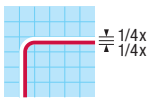
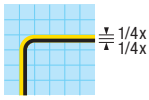
Layout 09 – Spacing and Alignment

K6-09 Identification



Layout 10

Sign types: N1, T1	
Prohibition	**Caution**
N1	T1

Size and colour	
Text message Size: 1x Colour: Black Tracking: 50/1000 of an em	
Prohibition borders Size: 1/4x Colour: CSA Red interior border White exterior border	
Prohibition panel background Colour: White	
Warning borders Size: 1/4x Colour: Black interior border CSA Yellow exterior border	
Warning panel background Colour: CSA Yellow	

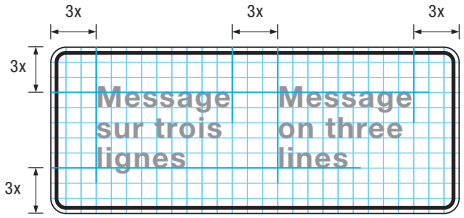
Vehicular	Pedestrian
Message load 1 text message maximum	**Message load** 3 text messages maximum

Layout 10 (CONTINUED)

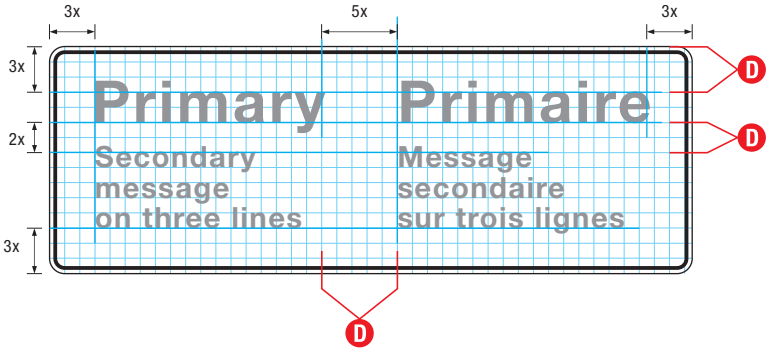
Layout 10 – Spacing and Alignment

D Spacing for margins, gutters and rows is variable, dependent upon the primary text message size. See *Appendix F – Text Message Hierarchy.*

N1-10 Prohibition



T1-10 Caution



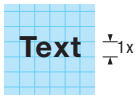
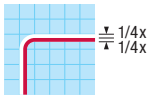
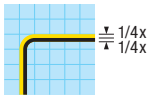
Layout 10 – Exceptions and Special Cases

Sign Type: T1

In situations where the primary text message is equivalent in both spelling and meaning in English and French, Attention for example, that term can be presented once, in a larger size.

Layout 11

Sign types: N1, T1	
Prohibition	**Caution**
N1	T1

Size and colour	
Text message Size: 1x Colour: Black Tracking: 50/1000 of an em	
Prohibition border Size: 1/4x Colour: CSA Red interior border White exterior border	
Prohibition panel background Colour: White	
Caution border Size: 1/4x Colour: Black interior border CSA Yellow exterior border	
Caution panel background Colour: CSA Yellow	

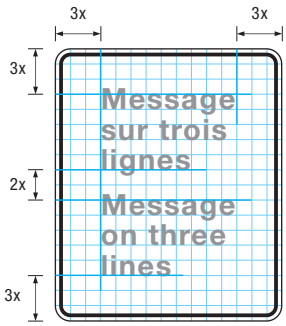
Vehicular	Pedestrian
Message load 1 text message maximum	**Message load** 3 text messages maximum

Layout 11 (CONTINUED)

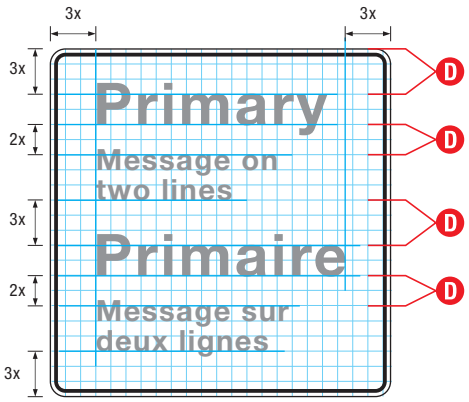
Layout 11 – Spacing and Alignment

D Spacing for margins, gutters and rows is variable, dependent upon the primary text message size.
See *Appendix F – Text Message Hierarchy.*

N1-11 Prohibition



T1-11 Caution

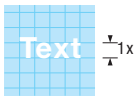
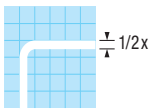


Layout 11 – Exceptions and Special Cases

Sign Types: N1, T1
In situations where the primary text message is equivalent in both spelling and meaning in English and French, Attention for example, that term can be presented once, in a larger size.

Layout 12

Sign types: H1, P1, U1		
Emergency	**Mandatory**	**Danger**
H1	P1	U1

Size and colour	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Emergency panel background Colour: CSA Green	
Mandatory panel background Colour: Black	
Danger panel background Colour: CSA Red	

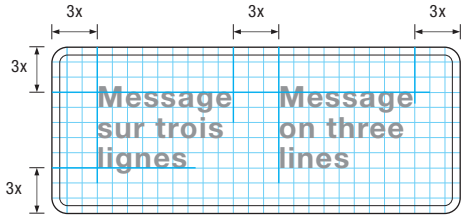
Vehicular	Pedestrian
Message load 1 text message maximum	**Message load** 3 text messages maximum

Layout 12 (CONTINUED)

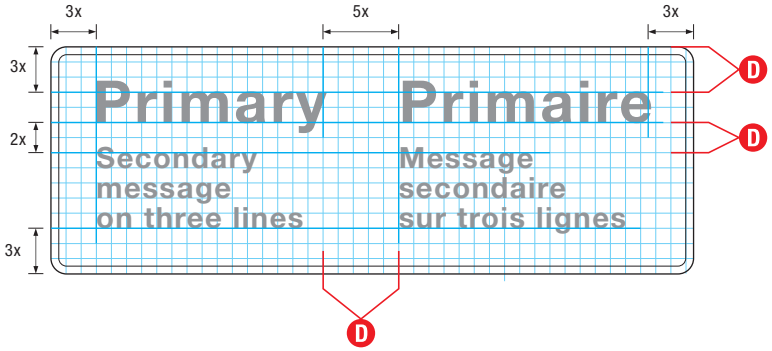
Layout 12 – Spacing and Alignment

D Spacing for margins, gutters and rows is variable, dependent upon the primary text message size.
See *Appendix F – Text Message Hierarchy.*

H1-12 Emergency, P1-12 Mandatory



H1-12 Emergency, P1-12 Mandatory, U1-12 Danger

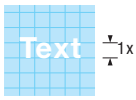
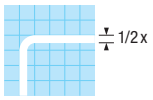


Layout 12 – Exceptions and Special Cases

Sign Type: U1
In situations where the primary text message is equivalent in both spelling and meaning in English and French, Danger for example, that term can be presented once, in a larger size.

Layout 13

Sign types: H1, P1, U1		
Emergency	**Mandatory**	**Danger**
H1	P1	U1

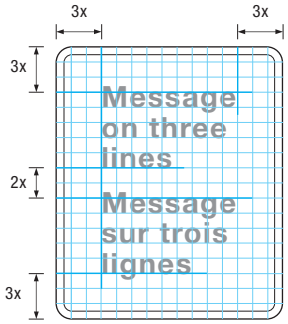
Size and colour	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Emergency panel background Colour: CSA Green	
Mandatory panel background Colour: Black	
Danger panel background Colour: CSA Red	

Vehicular	Pedestrian
Message load 1 text message maximum	**Message load** 3 text messages maximum

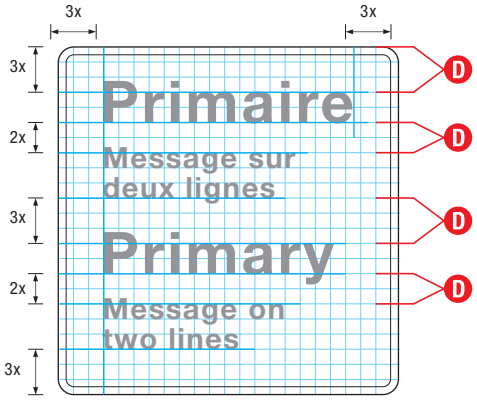
Layout 13 – Spacing and Alignment

D Spacing for margins, gutters and rows is variable, dependent upon the primary text message size.
See *Appendix F – Text Message Hierarchy.*

H1-13 Emergency, P1-13 Mandatory



H1-13 Emergency, P1-13 Mandatory, U1-13 Danger

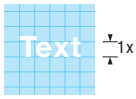
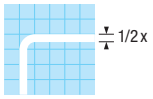


Layout 13 – Exceptions and Special Cases

Sign Type: U1
In situations where the primary text message is equivalent in both spelling and meaning in English and French, Danger for example, that term can be presented once, in a larger size.

Layout 14

Sign type: Q1
Prohibition and mandatory
Q1

Size and colour	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Prohibition symbol Size: 4x Colour: White (symbol background, symbol border); Black (symbol image); CSA Red (prohibition slash)	
Mandatory symbol Size: 4x Colour: White (symbol image, symbol border); Black (symbol background)	
Panel background Colour: PC Heritage green	

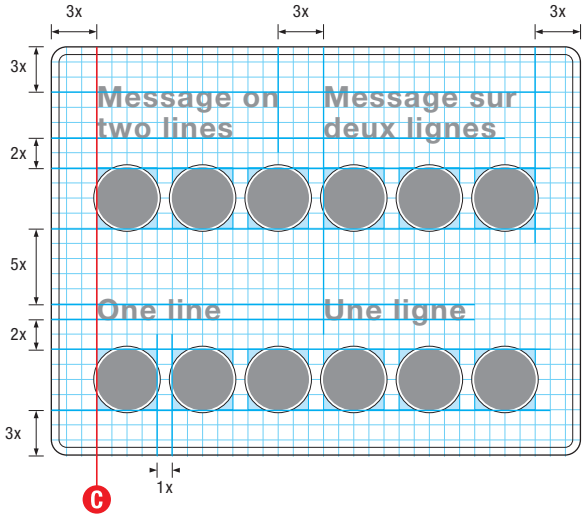
Vehicular	Pedestrian
Message load 9 elements maximum 4 symbols per sign maximum 3 text messages maximum	**Message load** 6 symbols per message maximum 6 text messages maximum

Layout 14 (CONTINUED)

Layout 14 – Spacing and Alignment

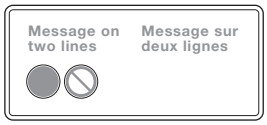
C Symbol is left aligned with text message.

Q1-14 Prohibition and Mandatory



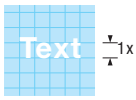
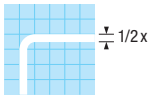
Layout 14 – Exceptions and Special Cases

Sign Type: Q1
When appropriate, both prohibition and mandatory symbols can be used with the same message.



Layout 15

Sign type: Q1
Prohibition and mandatory
Q1

Size and colour	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Prohibition symbol Size: 4x Colour: White (symbol background, symbol border); Black (symbol image); CSA Red (prohibition slash)	
Mandatory symbol Size: 4x Colour: White (symbol image, symbol border); Black (symbol background)	
Panel background Colour: PC Heritage green	

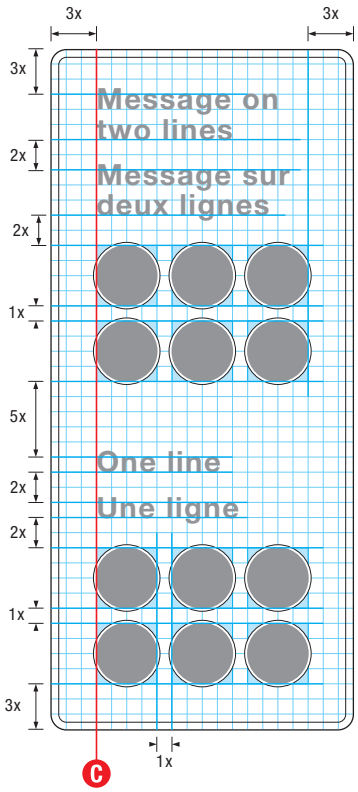
Vehicular	Pedestrian
Message load 9 elements maximum 4 symbols per sign maximum 3 text messages maximum	**Message load** 6 symbols per message maximum 6 text messages maximum

Layout 15 (CONTINUED)

Layout 15 – Spacing and Alignment

C Symbol is left aligned with text message.

Q1-15 Prohibition and mandatory

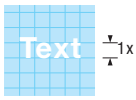
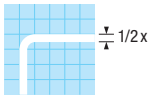


Layout 15 – Exceptions and Special Cases

Sign Type: Q1
When appropriate, both prohibition and mandatory symbols can be used with the same message.

Layout 16

Sign type: Q1
Prohibition and mandatory
Q1

Size and colour	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Prohibition border Size: 1/2x Colour: White	
Prohibition symbol Size: 4x Colour: White (symbol background, symbol border); Black (symbol image); CSA Red (prohibition slash)	
Mandatory symbol Size: 4x Colour: White (symbol image, symbol border); Black (symbol background)	
Panel background Colour: PC Heritage G reen	

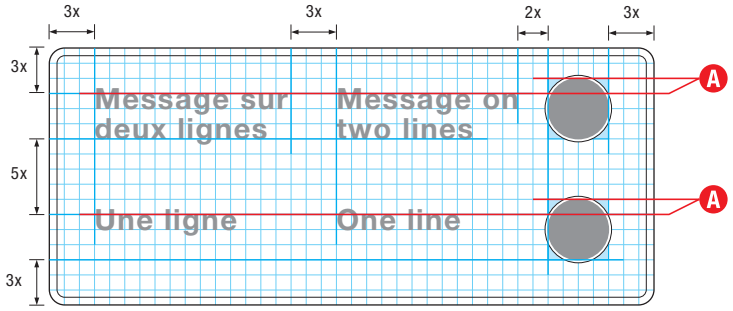
Vehicular	Pedestrian
Message load 9 elements maximum 3 symbols per sign maximum 3 text messages maximum	**Message load** 1 symbol per message maximum 6 text messages maximum

Layout 16 (CONTINUED)

Layout 16 – Spacing and Alignment

A Top of symbol (key line) is placed 1x above x-height of first line of text message.

Q1-16 Prohibition and Mandatory



Layout 16 – Exceptions and Special Cases

Sign Type: Q1
When appropriate, a prohibition message and symbol can be combined with a mandatory message and symbol on the same sign.

Layout 17

Sign types: R1, R2, R3, R4, R5, R6

Parking control

R1

R2

R3

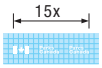
R4

R5

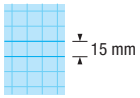
R6

Size and colour

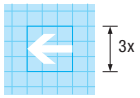
Parks Canada signature
Size: 1x, flag height
Colour: White (flag symbol, applied title)



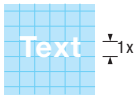
Standard unit "x"
Size: 15 mm



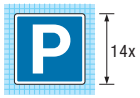
Arrow
Size: 3x
Colour: White



Text message
Size: 1x
Colour: White
Tracking: 50/1000 of an em

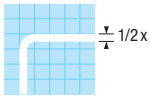


Guidance and information symbol
Size: 14x
Colour: White (symbol image, symbol border);
CSA Blue (symbol background)



Prohibition symbol
Size: 14x
Colour: White (symbol background, symbol border);
Black (symbol image);
CSA Red (prohibition slash)

Border
Size: 1/2x
Colour: White



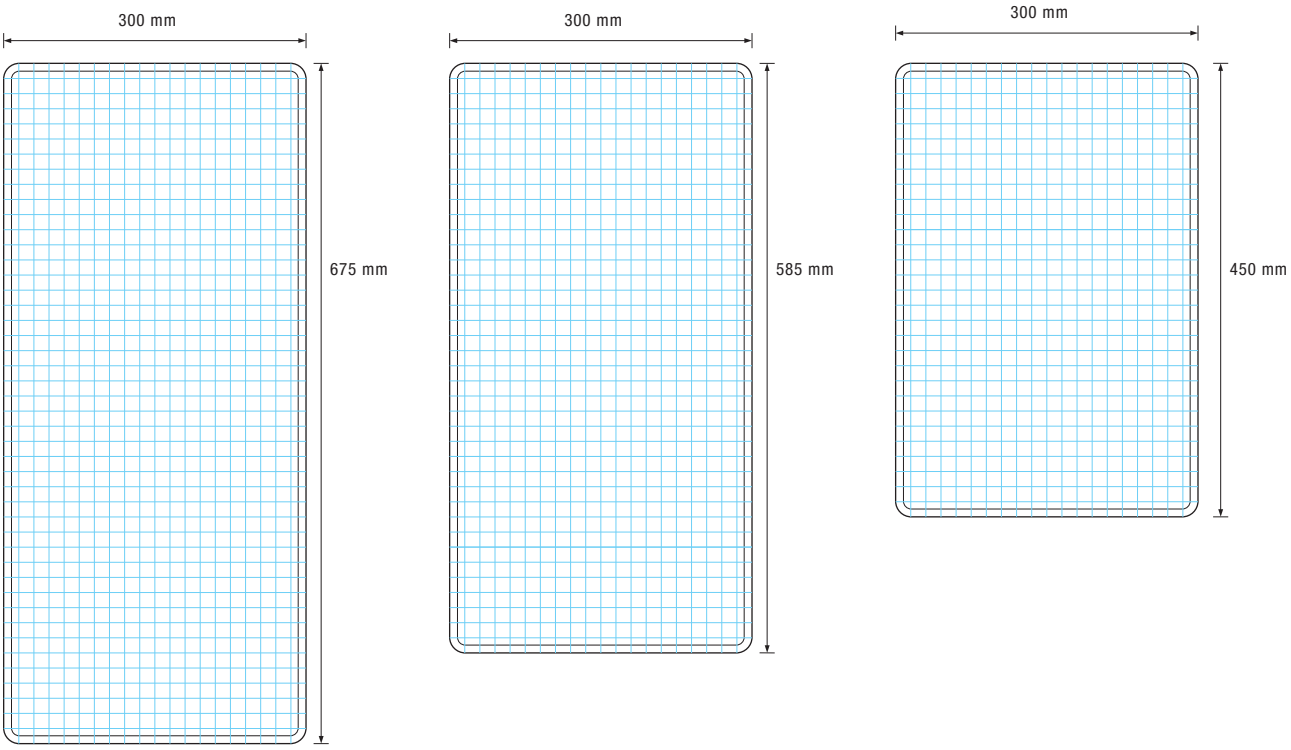
Panel background
Colour: PC Heritage Green

Vehicular	Pedestrian
Message load 1 Parks Canada signature 1 symbol per sign maximum 1 text message maximum	**Message load** Recommended for vehicular use only

Templates containing approved artwork for the Parks Canada signature as applied in Layout 17 are available in the three standard panel sizes.

Layout 17 (CONTINUED)

Layout 17 – Standard Sizes

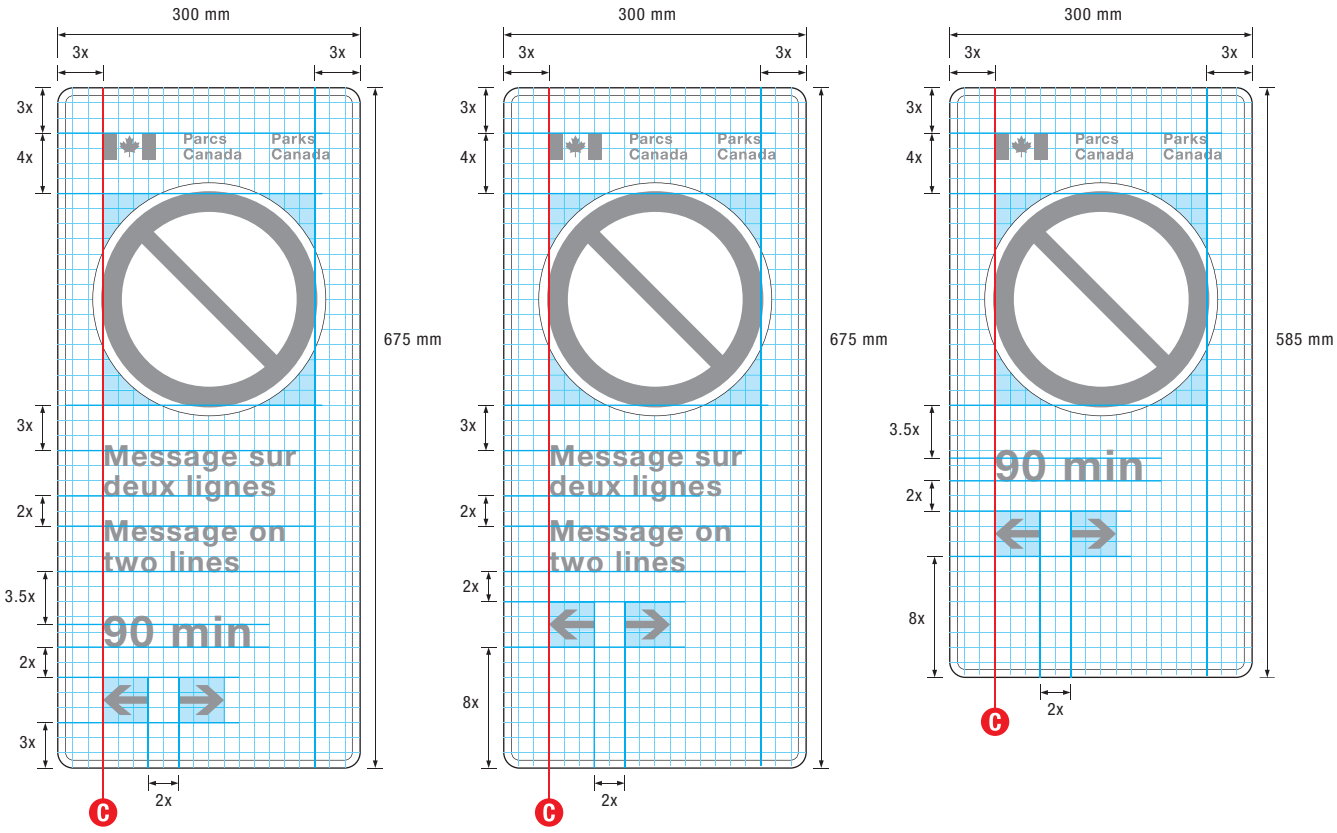


Layout 17 (CONTINUED)

Layout 17 – Spacing and Alignment

C Symbol is left aligned with text message.

R1-17 Parking Control

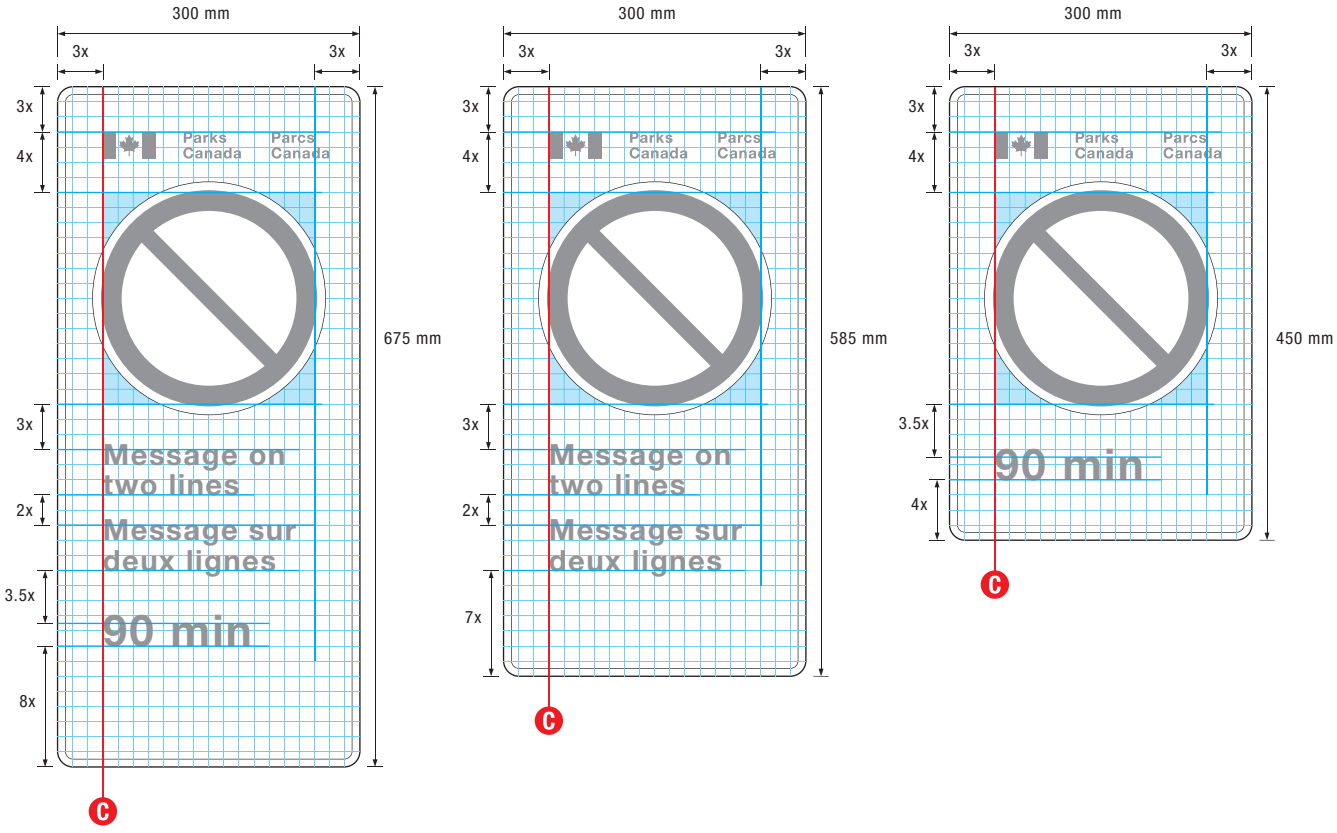


Layout 17 (CONTINUED)

Layout 17 – Spacing and Alignment

C Symbol is left aligned with text message.

R2-17 Parking Control

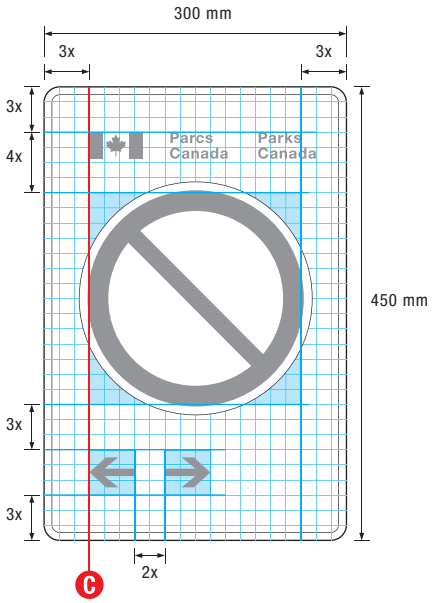


Layout 17 (CONTINUED)

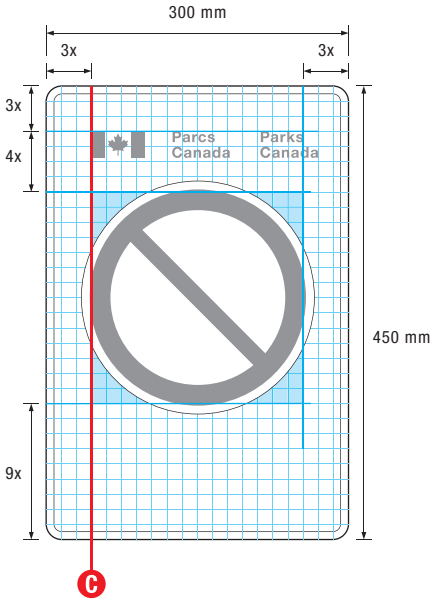
Layout 17 – Spacing and Alignment

C Symbol is left aligned with text message.

R3-17 Parking Control



R4-17 Parking Control

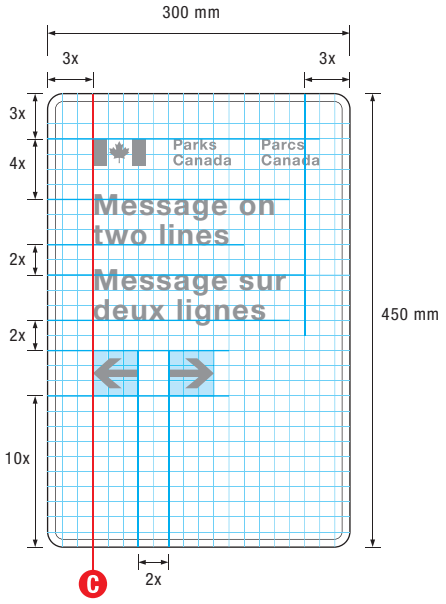


Layout 17 (CONTINUED)

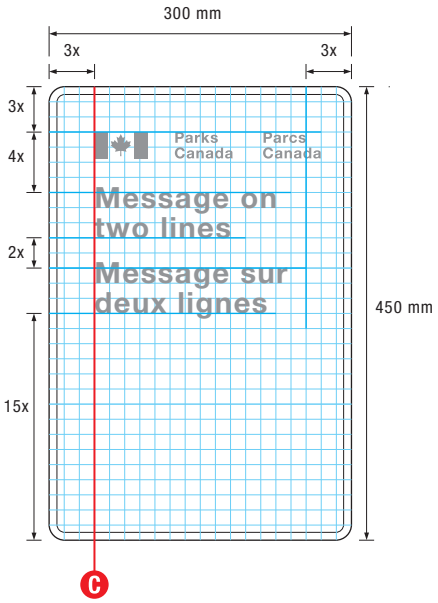
Layout 17 – Spacing and Alignment

C Symbol is left aligned with text message.

R5-17 Parking Control



R6-17 Parking Control



Layout 18

Sign type: S1
Boundary marker
S1

Size and colour	
Parks Canada signature Size: Standard, pre-fabricated Colour: Black (flag symbol, applied title)	Parks Canada Parcs Canada
Text message Size: Standard, pre-fabricated Colour: Black Tracking: 50/1000 of an em	Text
Parks Canada identifier Size: Standard, pre-fabricated Colour: Black	Canada
Border Size: Standard, pre-fabricated Colour: Black	
Panel background Colour: Boundary Marker Yellow	

Vehicular	Pedestrian
Message load 1 Parks Canada signature 1 Parks Canada identifier 1 text message maximum	**Message load** 1 Parks Canada signature 1 Parks Canada identifier 1 text message maximum

Layout 18 – Spacing and Alignment

S1-18 Boundary Marker

Layout 19

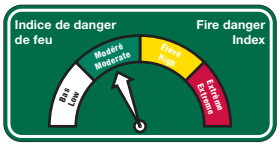
Sign type: W1
Wave hazard
W1

Size and colour	
Under development	

Vehicular	Pedestrian

Layout 19 (CONTINUED)

Layout 20

Sign type: W2
Fire hazard
W2 

Size and colour	
Under development	

Vehicular	Pedestrian

Layout 20 (CONTINUED)

Layout 21

Sign type:
Under development

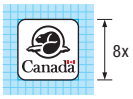
Size and colour	

Vehicular	Pedestrian

Layout 21 (CONTINUED)

Layout 22

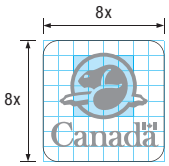
Sign type: A1
TODs patch
A1

Size and colour	
Parks Canada identifier Size: 8x Colour: Black (Parks Canada beaver symbol, "Canada" wordmark); FIP Red (flag symbol)	

Vehicular	Pedestrian
Message load 1 symbol per sign maximum	**Message load** For vehicular use only

Layout 22 – Spacing and Alignment

A1-23 Special Purpose



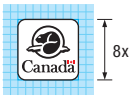
Layout 23

Sign type: B1
Highway advance

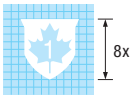
B1

Size and colour

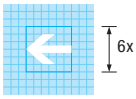
Parks Canada identifier
Size: 8x
Colour: Black (Parks Canada beaver symbol, "Canada" wordmark);
FIP Red (flag symbol)



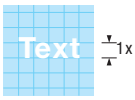
Additional symbols
Size: Not to exceed 8x in height or width
Colour: White



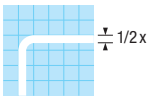
Arrow
Size: 6x
Colour: White



Text message and distance
Size: 1x
Colour: White
Tracking: 50/1000 of an em



Border
Size: 1/2x
Colour: White



Panel background
Colour: PC Heritage Green

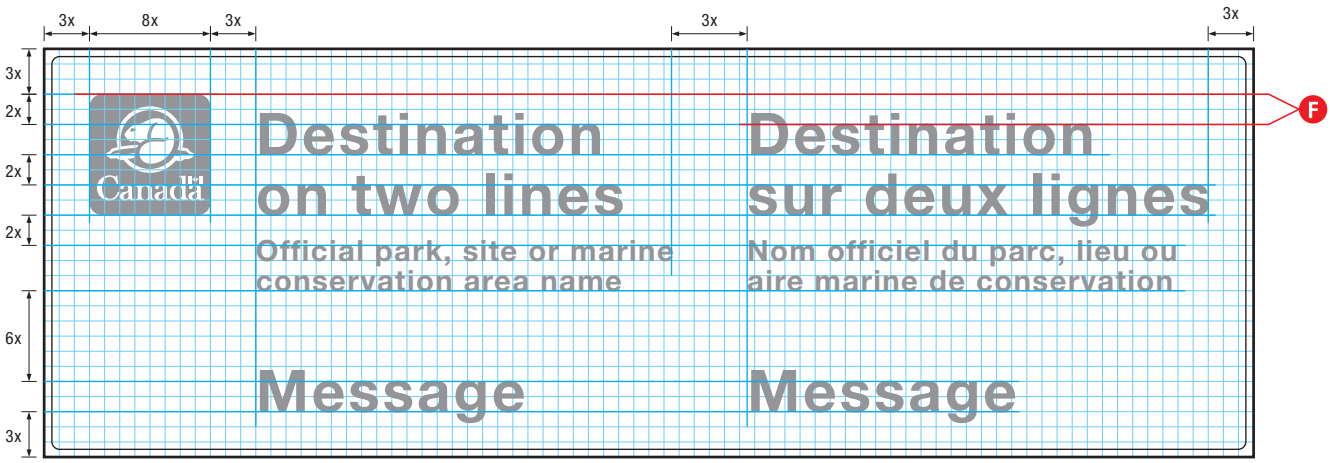
Vehicular	Pedestrian
Message load The ideal highway advance sign would include the Parks Canada identifier, name of park/site, Parks Canada Heritage Green background, bilingual message and layout. However, it is recognized that these content specifications are subject to provincial/municipal approval and must be negotiated at the Field Unit level.	**Message load** For vehicular use only

Layout 23 (CONTINUED)

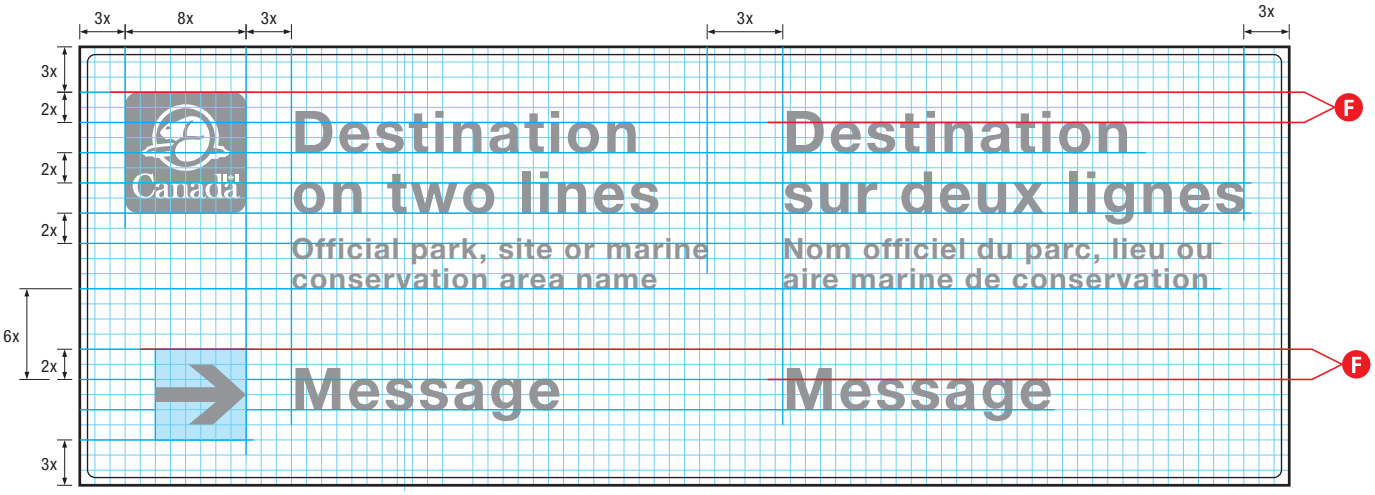
Layout 23 – Spacing and Alignment

F Top of Parks Canada identifier symbol and arrow (key line) is placed 2x above x-height of first line of text message.

B1-23 Highway Advance



B1-23 Highway Advance

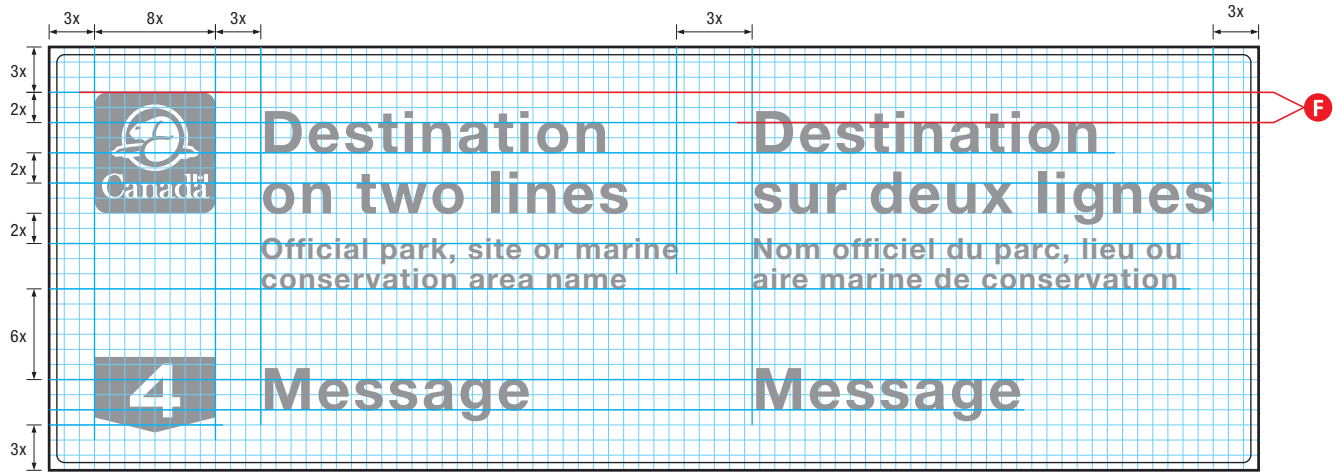


Layout 23 (CONTINUED)

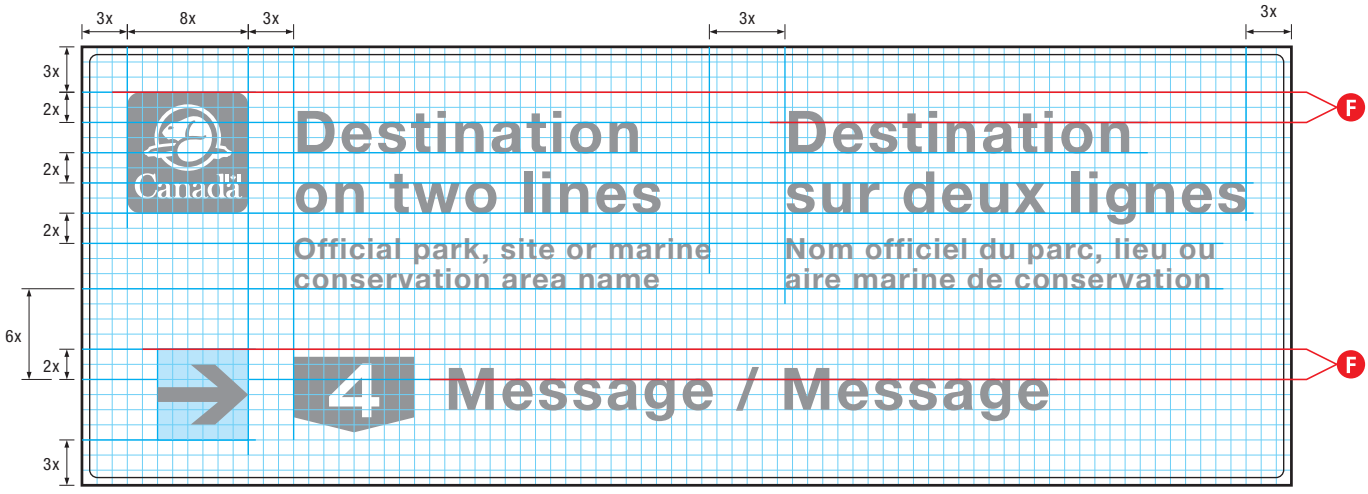
Layout 23 – Spacing and Alignment

F Top of Parks Canada identifier symbol and arrow (key line) is placed 2x above x-height of first line of text message.

B1-23 Highway Advance



B1-23 Highway Advance



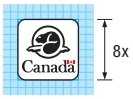
Layout 24

Sign type: B1
Highway advance

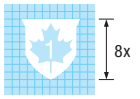
B1

Size and colour

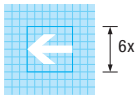
Parks Canada identifier
Size: 8x
Colour: Black (Parks Canada beaver symbol, "Canada" wordmark); FIP Red (flag symbol)



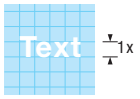
Additional symbols
Size: Not to exceed 8x in height or width
Colour: White



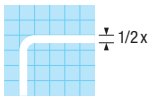
Arrow
Size: 6x
Colour: White



Text message and distance
Size: 1x
Colour: White
Tracking: 50/1000 of an em



Border
Size: 1/2x
Colour: White



Panel background
Colour: PC Heritage Green

Vehicular	Pedestrian
Message load 9 elements maximum 3 symbols per sign maximum 3 text messages maximum	**Message load** For vehicular use only

Layout 24 – Spacing and Alignment

F Top of Parks Canada identifier symbol is placed 2x above x-height of first line of text message.

B1-24 Highway Advance

Layout 24 (CONTINUED)

Layout 24 – Spacing and Alignment

F Top of Parks Canada identifier symbol and arrow (key line) is placed 2x above x-height of first line of text message.

B1-24 Highway Advance

Layout 24 (CONTINUED)

Layout 24 – Spacing and Alignment

F Top of Parks Canada identifier symbol is placed 2x above x-height of first line of text message.

B1-24 Highway Advance

Layout 24 (CONTINUED)

Layout 24 – Spacing and Alignment

F Top of Parks Canada identifier symbol and arrow (key line) is placed 2x above x-height of first line of text message.

B1-24 Highway Advance

Layout 25

Sign types: B2, B3, B4, B5

Trailblazer

B2

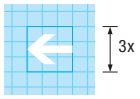
B3

B4

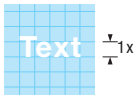
B5

Size and colour

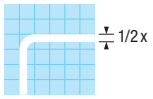
Arrow
Size: 3x
Colour: White



Text message and distance
Size: 1x
Colour: White
Tracking: 50/1000 of an em

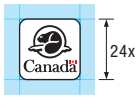


Border
Size: 1/2x
Colour: White



Panel background
Colour: PC Heritage Green

Parks Canada identifier
Size: 24x
Colour: Black (Parks Canada beaver symbol, "Canada" wordmark);
FIP Red (flag symbol)



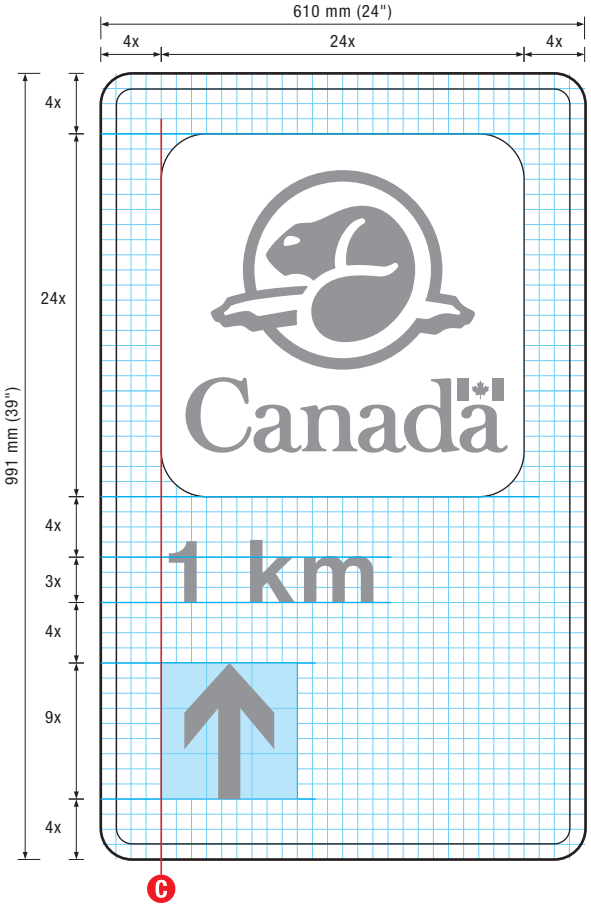
Vehicular	Pedestrian
Message load 1 symbol per sign maximum 1 destination maximum	**Message load** Recommended for vehicular use only

Layout 25 (CONTINUED)

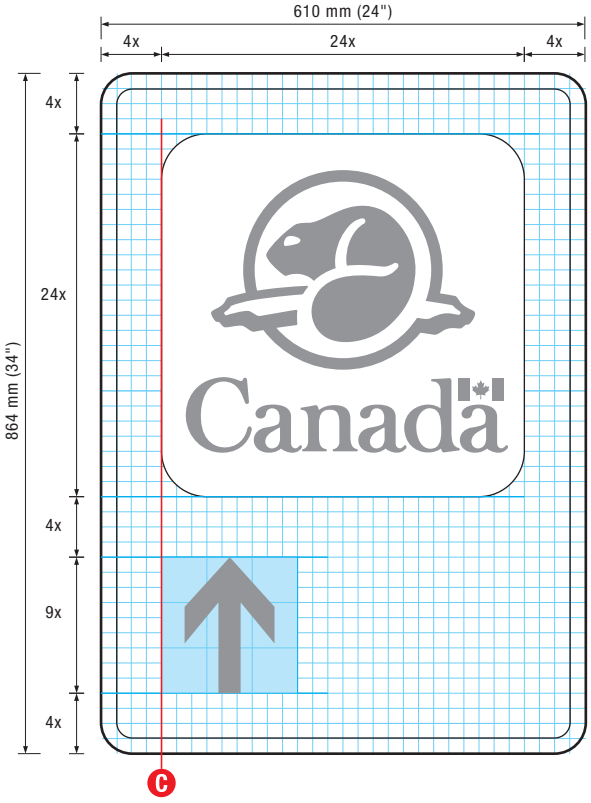
Layout 25 – Spacing and Alignment

C Symbol is left aligned with text message.

B2-25 Trailblazer



B3-25 Trailblazer

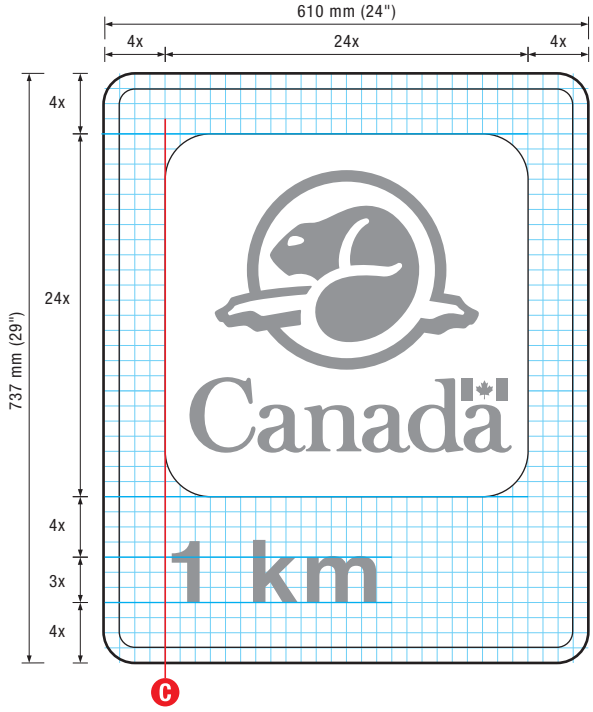


Layout 25 (CONTINUED)

Layout 25 – Spacing and Alignment

C Symbol is left aligned with text message.

B4-25 Trailblazer



B5-25 Trailblazer

Layout 26

Sign types: C1, C2

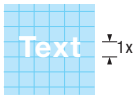
Primary Identification

C1

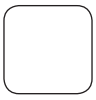
C2

Size and colour

Text message
Size: 1x
Colour: White
Tracking: 50/1000 of an em



Panel background (top and bottom)
Colour: White



Panel background (middle)
Colour: PC Heritage Green

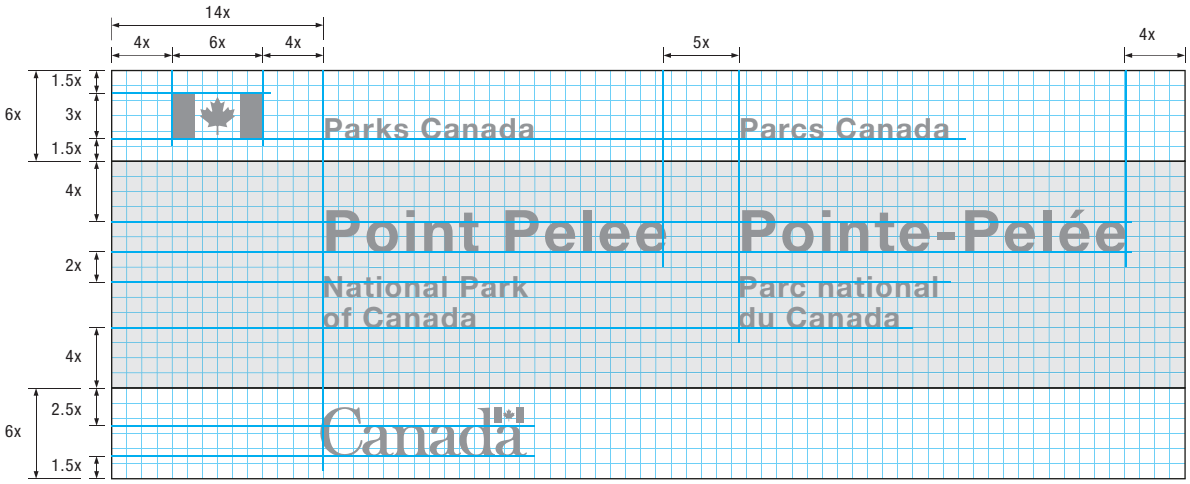
Parks Canada signature
Size: 3x (flag height); 1x applied title
Colour: Black (Parks Canada signature);
FIP Red (flag symbol)

The "Canada" wordmark
Size: 2x (x-height)
Colour: Black ("Canada" wordmark);
FIP Red (flag symbol)

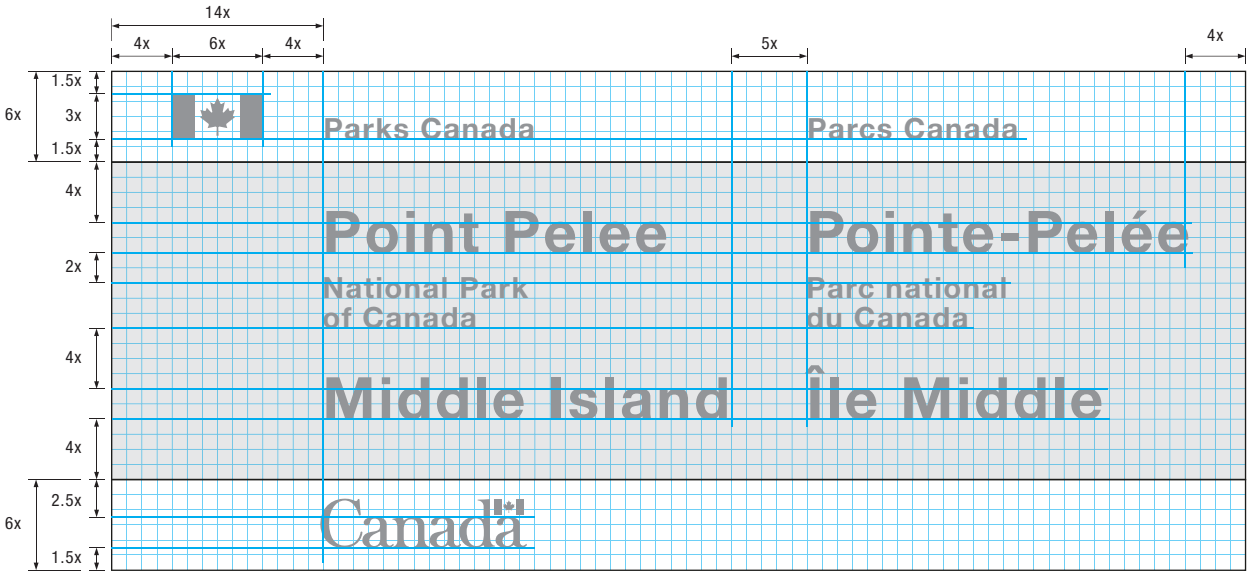
Vehicular	Pedestrian
Message load 1 Parks Canada signature 1 Canada wordmark 1 destination maximum	**Message load** 1 Parks Canada signature 1 Canada wordmark 1 destination maximum

Layout 26 – Spacing and Alignment

C1-26 Primary Identification

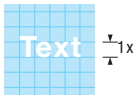
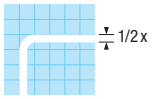


C2-26 Primary Identification



Layout 27

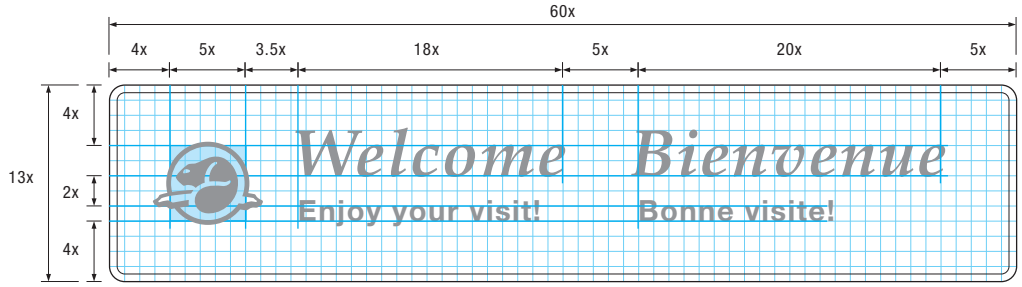
Sign types: D1, D2, G2	
Welcome	**Fees at work**
D1 D2 	G2

Size and colour	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Parks Canada beaver symbol Size: 5x Colour: White	
Panel background Colour: PC Heritage Green	

Vehicular	Pedestrian
Message load 1 beaver symbol per sign 1 text message maximum	**Message load** 1 beaver symbol per sign 1 text message maximum

Layout 27 – Spacing and Alignment

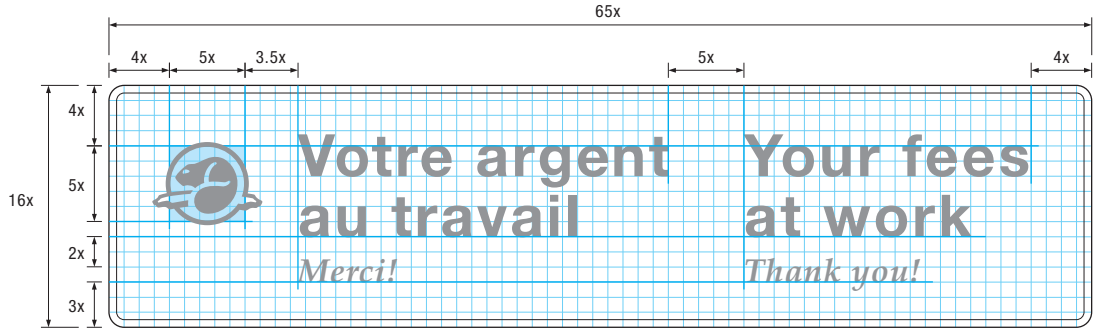
D1-27 Welcome



D2-27 Welcome

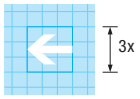
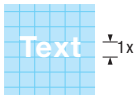
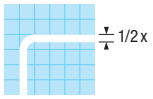
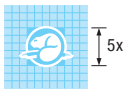
D2 is the same height and width as D1 when placed back to back.

G2-27 Fees at Work



Layout 28

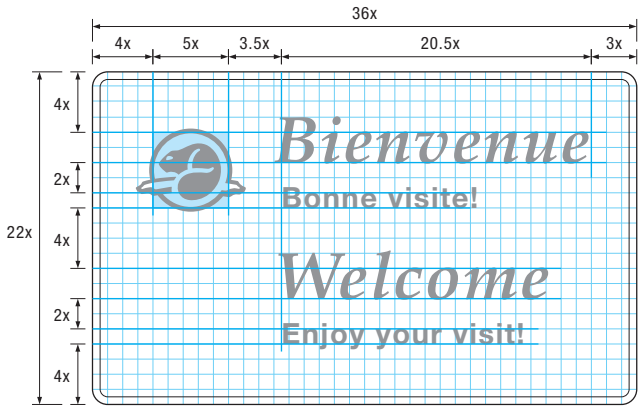
Sign types: D1, D2, D3, G2		
Welcome	**Day-use area**	**Fees at work**
D1 D2 	D3 	G2

Size and colour	
Arrow Size: 3x Colour: White	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Parks Canada beaver symbol Size: 5x Colour: White	
Panel background Colour: PC Heritage Green	

Vehicular	Pedestrian
Message load 1 beaver symbol per sign 1 text message maximum	**Message load** 1 beaver symbol per sign 1 text message maximum

Layout 28 – Spacing and Alignment

D1-28 Welcome

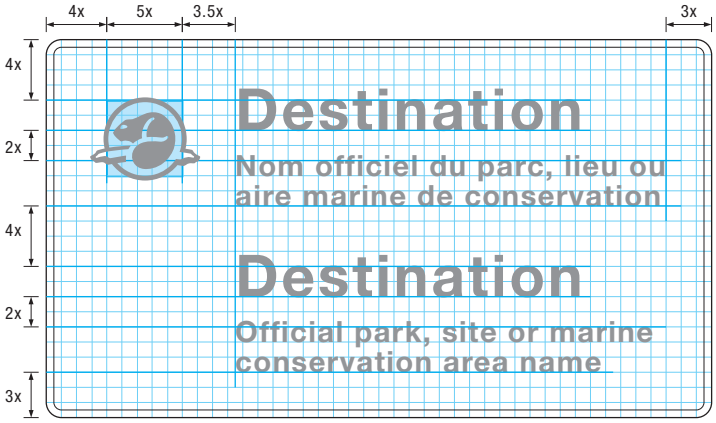


D2-28 Welcome
D2 is the same height and width as D1 when placed back to back.

Layout 28 (CONTINUED)

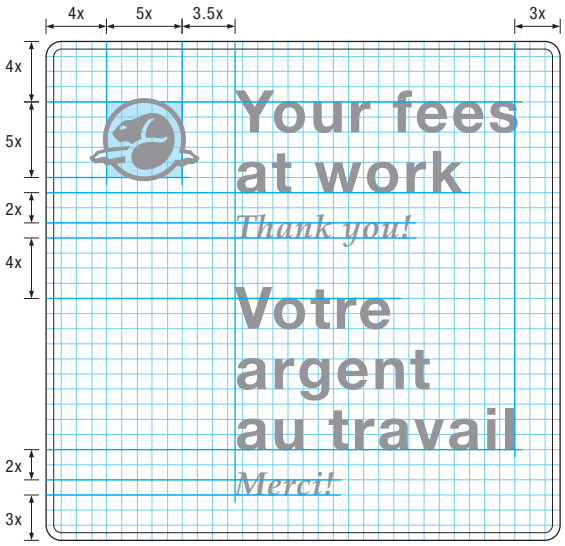
Layout 28 – Spacing and Alignment

D3-28 Day-use Area



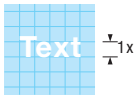
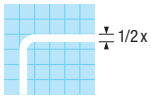
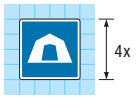
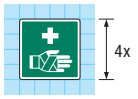
Layout 28 (CONTINUED)

G2-28 Fees at Work



Layout 29

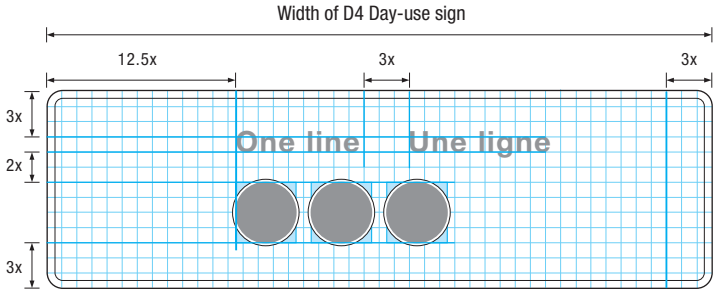
Sign types: D4, E3	
Day-use area	**Fees**
D4 	E3

Size and colour	
Text message Size: 1x Colour: White Tracking: 50/1000 of an em	
Border Size: 1/2x Colour: White	
Panel background Colour: PC Heritage green	
Guidance and information symbol Size: 4x Colour: White (symbol image, symbol border); CSA Blue (symbol background)	
Emergency symbol Size: 4x Colour: White (symbol image, symbol border); CSA Green (symbol background)	
Prohibition symbol Size: 4x Colour: White (symbol background, symbol border); Black (symbol image); CSA Red (prohibition slash)	
Mandatory symbol Size: 4x Colour: White (symbol image, symbol border); Black (symbol background)	

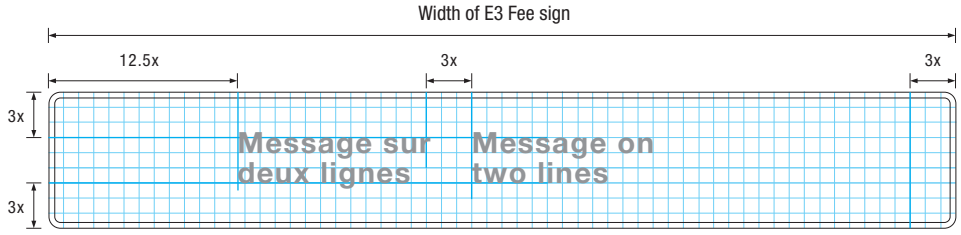
Vehicular	Pedestrian
Message load (D4) 9 elements maximum 4 symbols per sign maximum 3 text messages maximum **Message load (E3)** Must not exceed 1/2 the height of E1	**Message load (D4)** 6 symbols per sign maximum 6 text messages maximum **Message load (E3)** Must not exceed 1/2 the height of E1

Layout 29 – Spacing and Alignment

D4-29 Day-use Area



E3-29 Fees

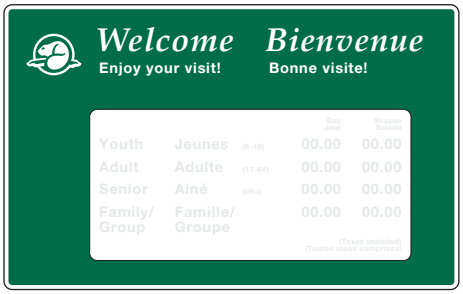


Layout 30

Sign types: E1, E4, E7, E8

Fees

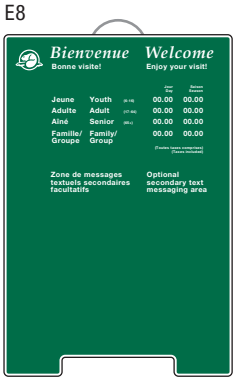
E1



E4

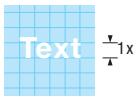
E7

E8

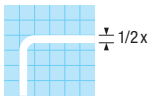


Size and colour

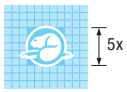
Text message
Size: 1x
Colour: White
Tracking: 50/1000 of an em



Border
Size: 1/2x
Colour: White



Parks Canada beaver symbol
Size: 5x
Colour: White



Panel background
Colour: PC Heritage Green

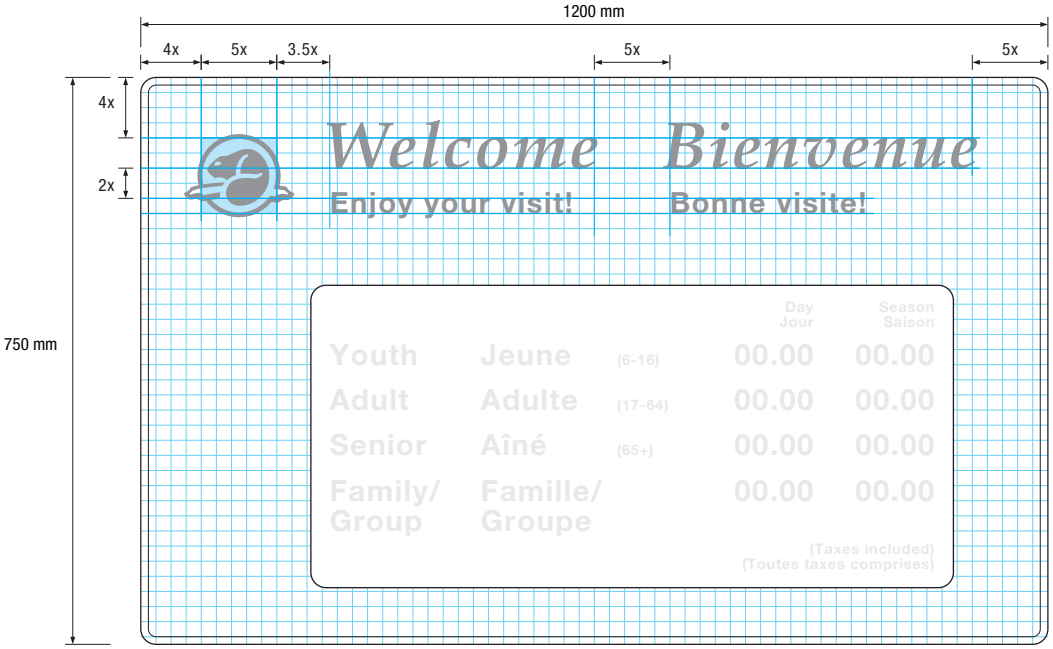
Vehicular	Pedestrian
Message load (E1, E4) 1 beaver symbol per sign 1 welcome/fee message **Message load (E7, E8)** 1 beaver symbol per sign 1 welcome/fee message Number of supporting text messages determined by available space	**Message load (E1, E4)** 1 beaver symbol per sign 1 welcome/fee message **Message load (E7, E8)** 1 beaver symbol per sign 1 welcome/fee message Number of supporting text messages determined by available space

The sizes of sign types E1 and E7 have been standardized to accommodate interchangeable insert panels (E2). Templates for these sign types containing approved artwork for the Parks Canada beaver symbol and welcome message are available.

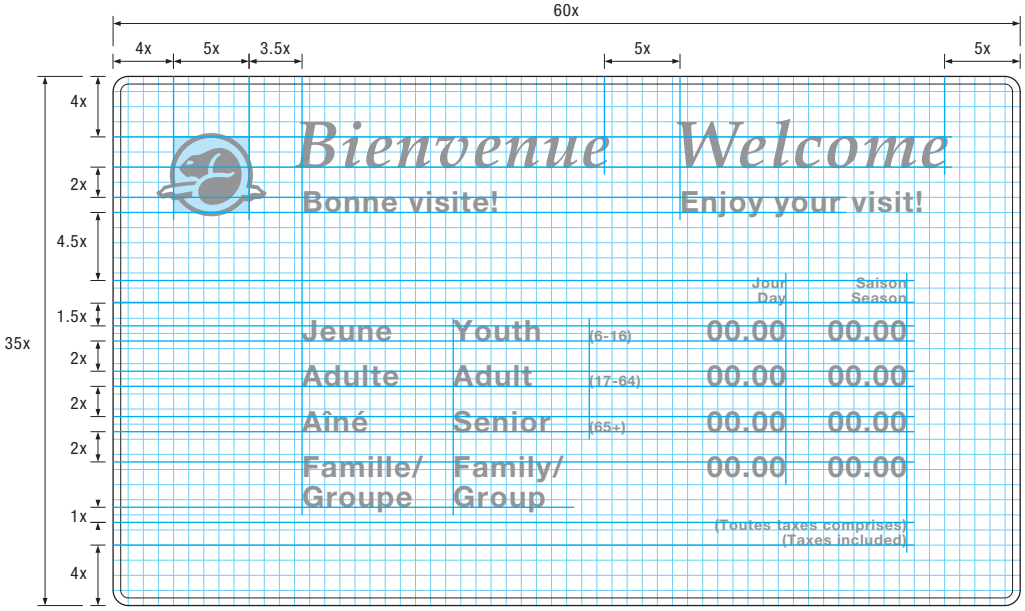
Layout 30 (CONTINUED)

Layout 30 – Spacing and Alignment

E1-30 Fees

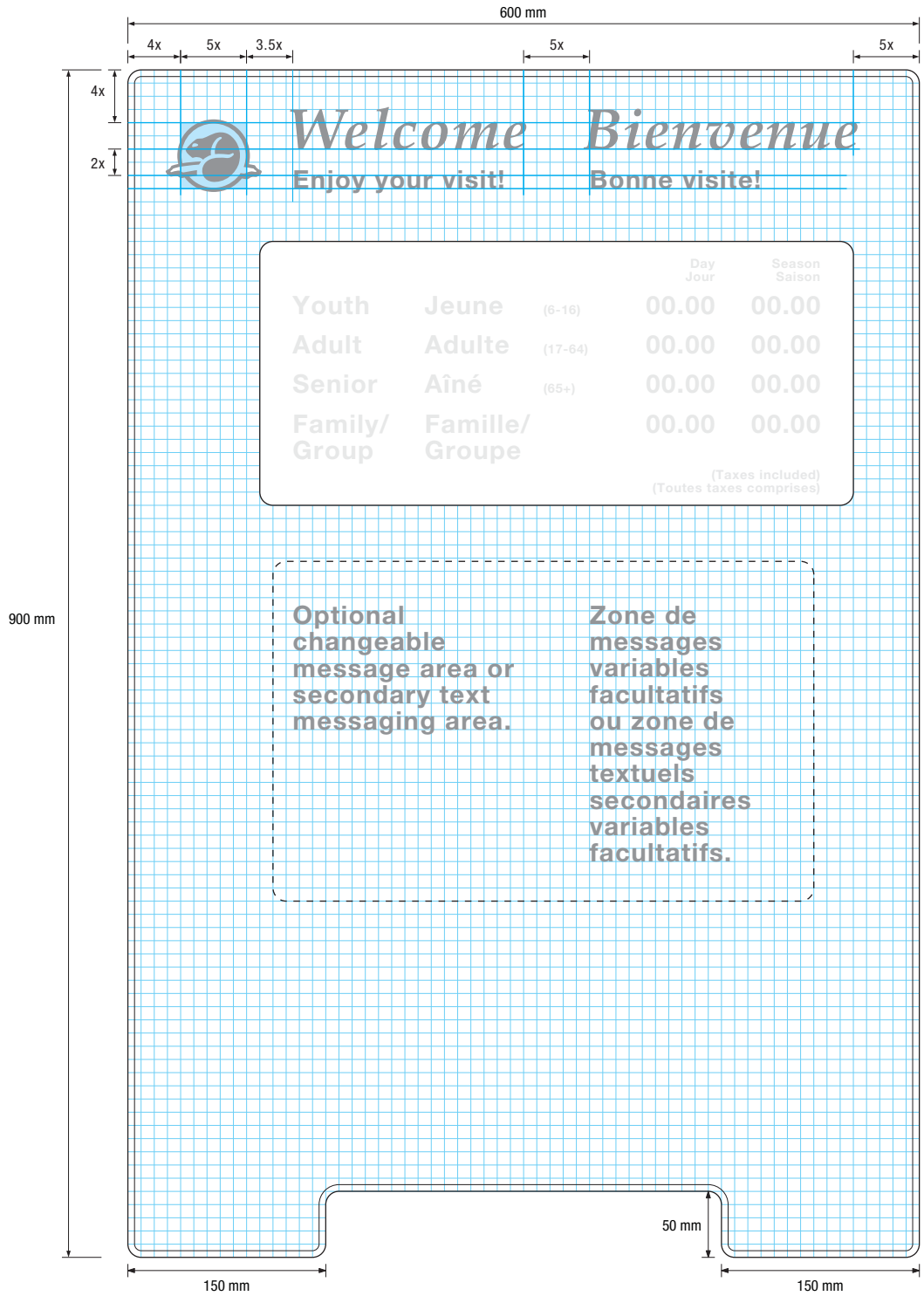


E4-30 Fees



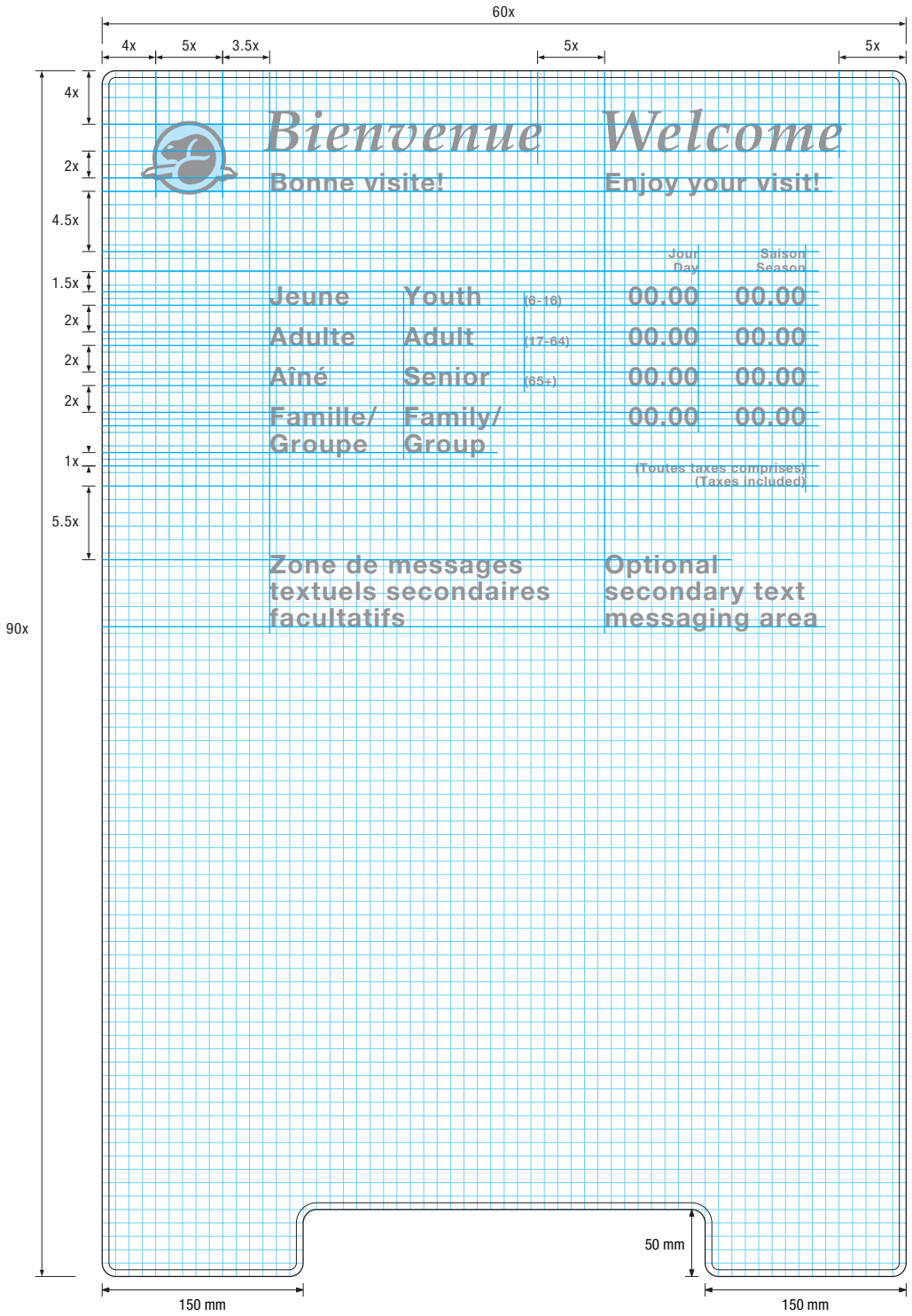
Layout 30 (CONTINUED)

E7-30 Fees



Layout 30 (CONTINUED)

E8-30 Fees

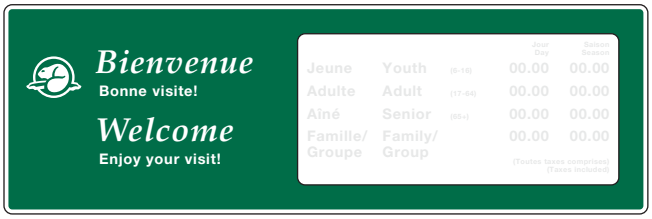


Layout 31

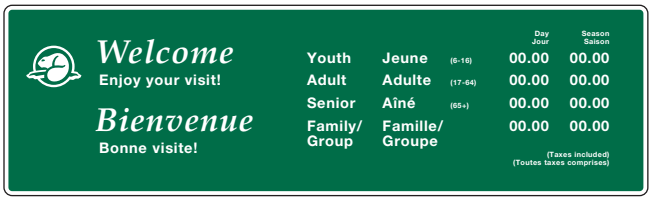
Sign types: E1, E4

Fees

E1

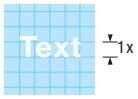


E4

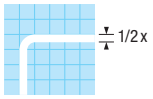


Size and colour

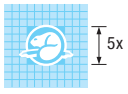
Text message
Size: 1x
Colour: White
Tracking: 50/1000 of an em



Border
Size: 1/2x
Colour: White



Parks Canada beaver symbol
Size: 5x
Colour: White



Panel background
Colour: PC Heritage Green

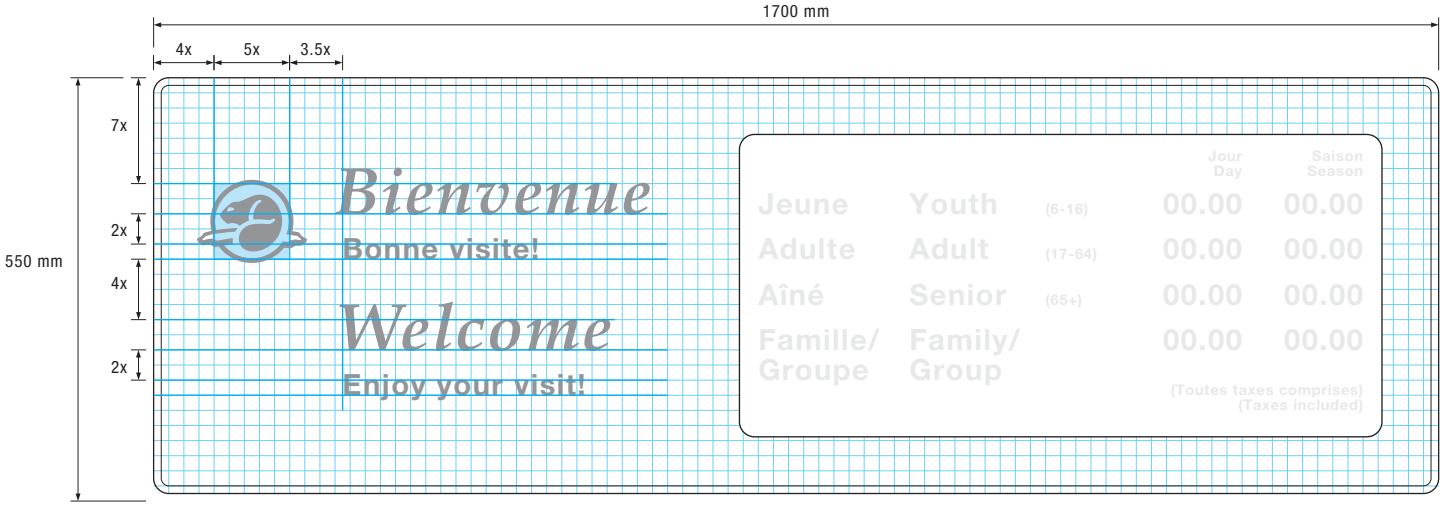
Vehicular	Pedestrian
Message load 1 beaver symbol per sign 1 welcome/fee message	**Message load** 1 beaver symbol per sign 1 welcome/fee message

The size of sign type E1 has been standardized to accommodate interchangeable insert panels (E2). A template for this sign type containing approved artwork for the Parks Canada beaver symbol and welcome message is available.

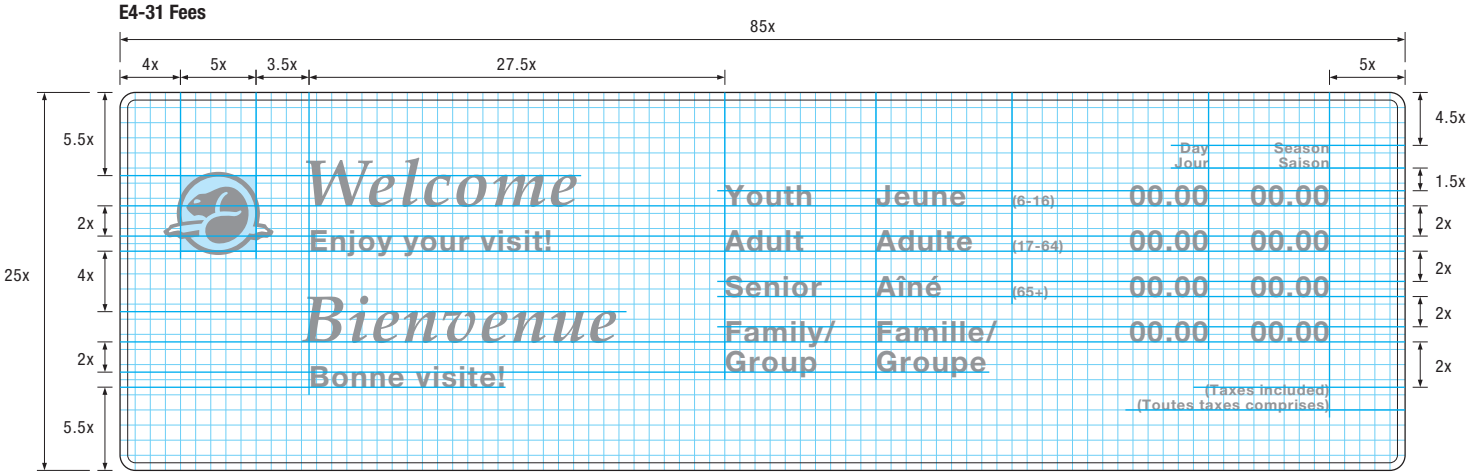
Layout 31 (CONTINUED)

Layout 31 – Spacing and Alignment

E1-31 Fees



E4-31 Fees



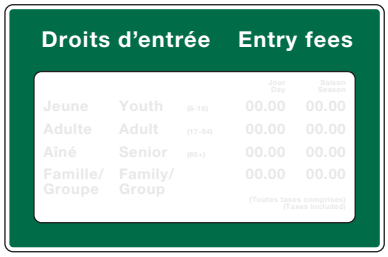
Layout 32

Sign types: E2, E5, E6

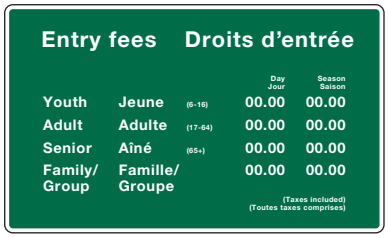
Fees

E2

E5

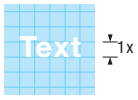


E6

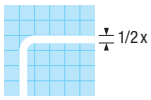


Size and colour

Text message
Size: 1x
Colour: White
Tracking: 50/1000 of an em



Border
Size: 1/2x
Colour: White



Panel background
Colour: PC Heritage Green

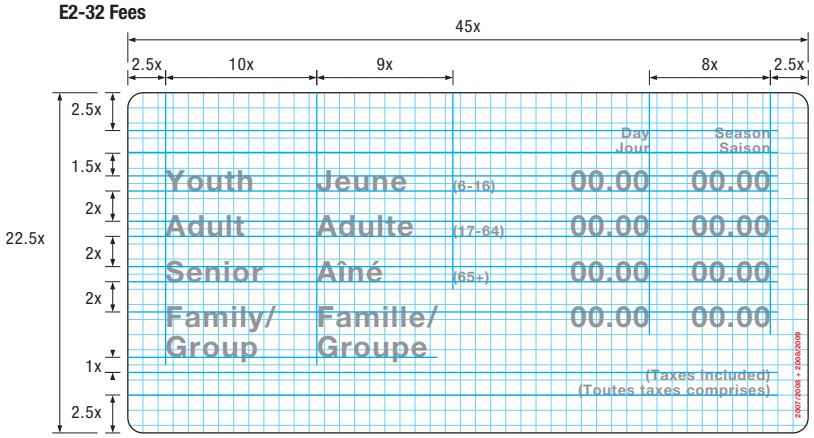
Vehicular	Pedestrian
Message load 1 approved fee message, as per National Pricing Compendium	**Message load** 1 approved fee message, as per National Pricing Compendium

The size of sign type E5 has been standardized to accommodate interchangeable insert panels (E2). A template for this sign type is available.

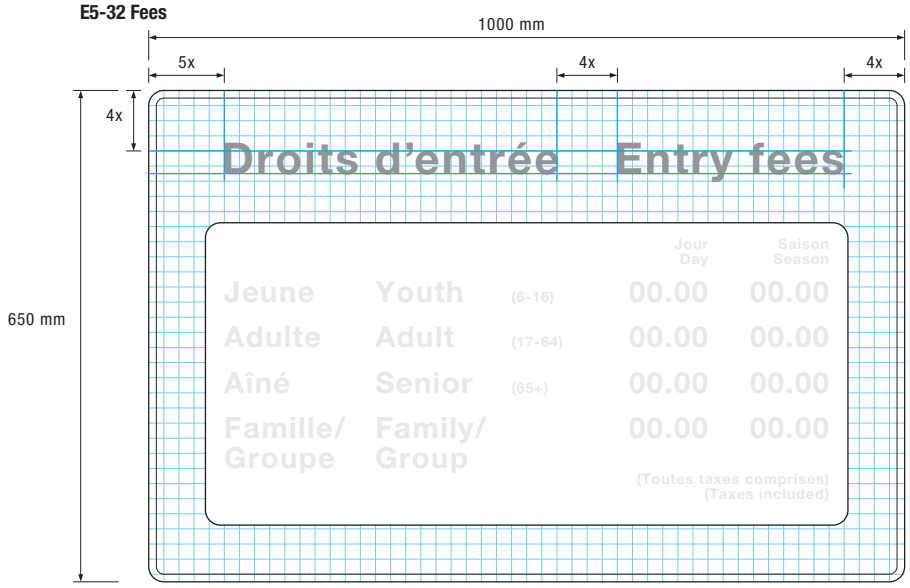
Layout 32 (CONTINUED)

Layout 32 – Spacing and Alignment

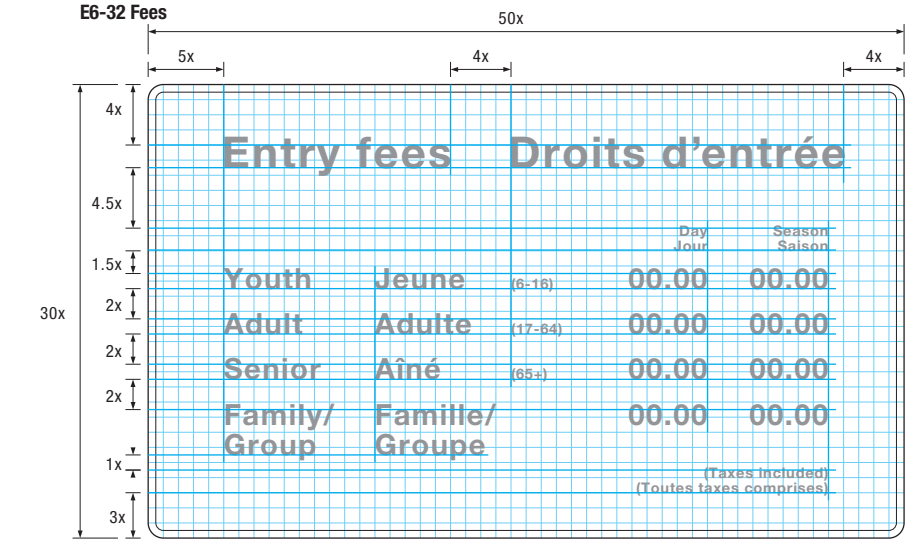
E2-32 Fees



E5-32 Fees



E6-32 Fees



Layout 33

Sign types: F1, F2

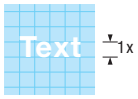
Street identification

F1

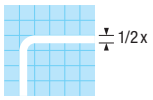
F2

Size and colour

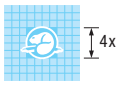
Text message
Size: 1x
Colour: White
Tracking: 50/1000 of an em



Border
Size: 1/2x
Colour: White



Parks Canada beaver symbol
Size: 4x
Colour: White



Panel background
Colour: PC Heritage Green

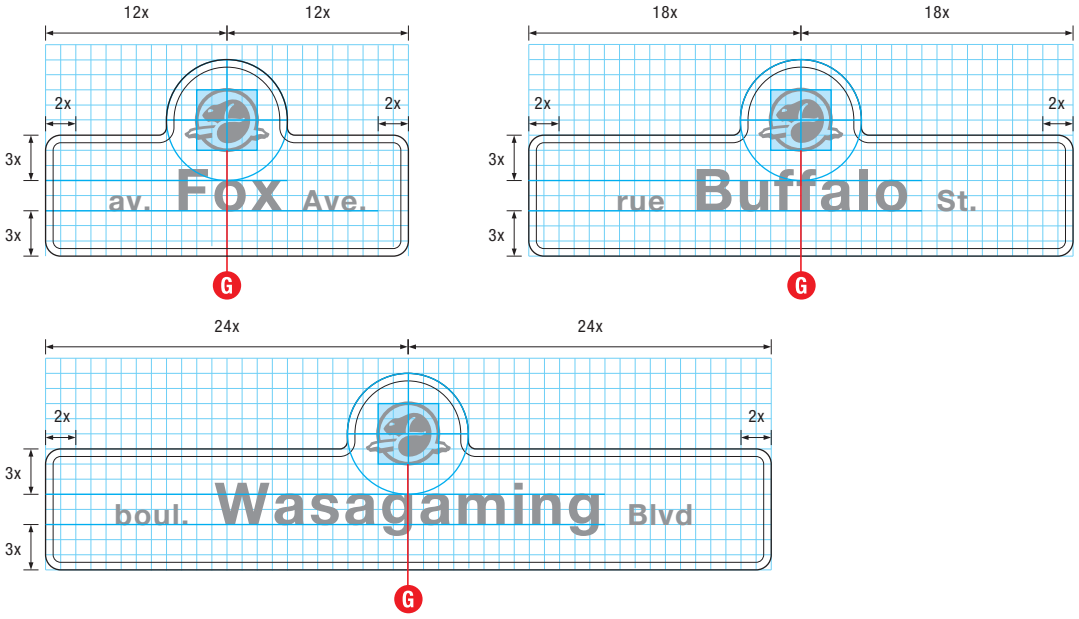
Vehicular	Pedestrian
Message load (F1) 1 beaver symbol 1 destination maximum **Message load (F2)** 1 destination maximum	**Message load** 1 destination maximum

Layout 33 (CONTINUED)

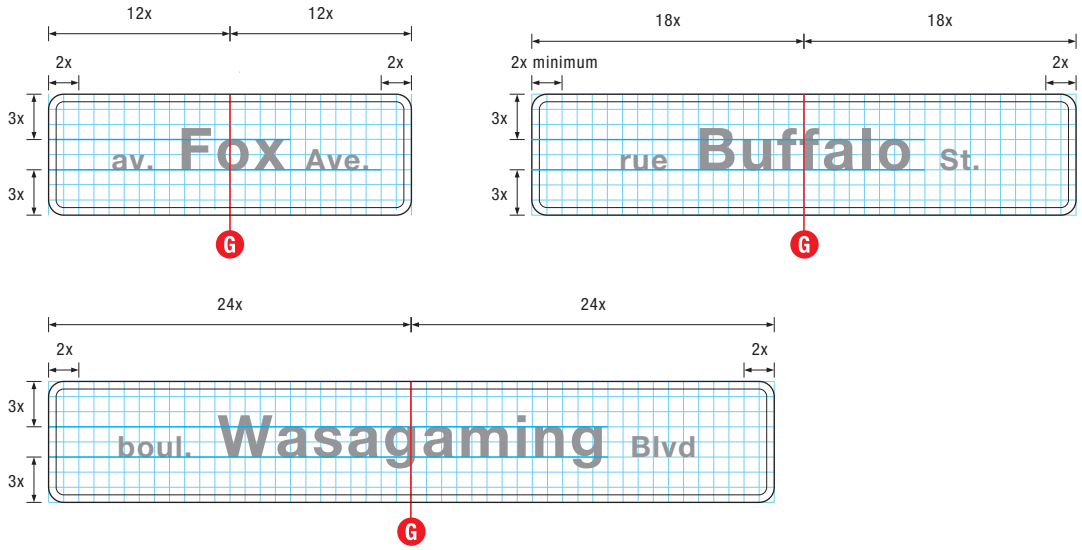
Layout 33 – Spacing and Alignment

G Text and beaver symbol are centred to width of sign.

F1-33 Street Identification



F2-33 Street Identification



Layout 34

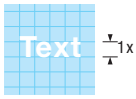
Sign type: G1

Partnership recognition

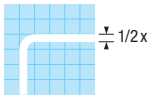
G1

Size and colour

Text message
Size: 1x
Colour: White
Tracking: 50/1000 of an em



Border
Size: 1/2x
Colour: White



Panel background (Top)
Colour: PC Heritage Green

Panel background (bottom)
Colour: White

Parks Canada partnership symbol
The Parks Canada partnership symbol ensures federal presence and visibility when Parks Canada enters into partnerships with non-federal organizations to deliver programs and services.

This partnership symbol consists of the Parks Canada signature combined with the "Canada" wordmark in a single block.

Size: 3x

Parks Canada Signature
Size: 1x (flag height)
Colour: Black (Parks Canada signature);
FIP Red (flag symbol)

The "Canada" wordmark
Size: 1x
Colour: Black ("Canada" wordmark);
FIP Red (flag symbol)

Vehicular	Pedestrian
Message load 1 text message maximum Number of symbols determined by number of partners	**Message load** 1 text message maximum Number of symbols determined by number of partners

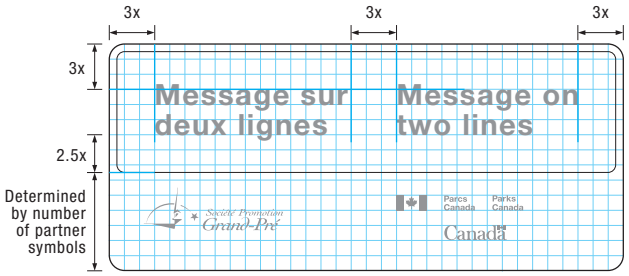
The Partnership Recognition Framework provides guidance for the recognition of Parks Canada's participation in partnering and collaborative arrangements, as well as the extent of recognition to be provided to Parks Canada's partners. It should be consulted as the partnership recognition sign is being developed.

It is recommended that a qualified designer be contracted to prepare the layout.

Layout 34 (CONTINUED)

Layout 34 – Spacing and Alignment

G1-34 Partnership Recognition



F

Appendix F – Text Message Hierarchy

Appendix F – Text Message Hierarchy

Overview	2
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3 to 1 Text Message Hierarchy	8

Overview

Hierarchy is a technique used to highlight or give emphasis to a sign element. A hierarchy is created by enlarging a primary text message—"Notice", "Attention!", "Danger!"—and contrasting this against a secondary text message configured in a smaller character size. This appendix specifies the spacing adjustments required when building text message hierarchy on signs.

In view of the many possible combinations of text message hierarchy, symbols and arrows, judgment is required when applying the 1.5:1, 2:1 and 3:1 hierarchy spacing standards.

Creating Text Message Hierarchy

Building Signs with Text Message Hierarchy

Building a sign with text message hierarchy requires adjustments to the spacing standards. The table below lists all the modifications required when building a sign with a 1.5:1, 2:1 and 3:1 ratio.

Table F.1 – Size ratios

Size Ratio		1.5:1	2:1	3:1
A	x-height of primary text message	1.5x	2x	3x
B	Margins	3x	3x	4x
C	Gutters (horizontal space between English and French text message)	4x	5x	6x
D	Vertical space between primary and secondary text message (primary text message above)	1.5x	2x	2x
E	Vertical space between two different text messages	5.5x	6x	7x
F	Vertical space between primary and secondary text message (primary text message below)	1.5x	2x	3x
G	Vertical space between symbol and text message	3x	3x	4x
H	Vertical space between English and French text message	3x	3x	4x

1.5 to 1 Text Message Hierarchy

Creating a Ratio of 1.5 to 1

The following layout specifications are applied when developing a text message hierarchy of 1.5 to 1.

Figure F.1
A = x-height of primary text message

Figure F.2
B = Margins

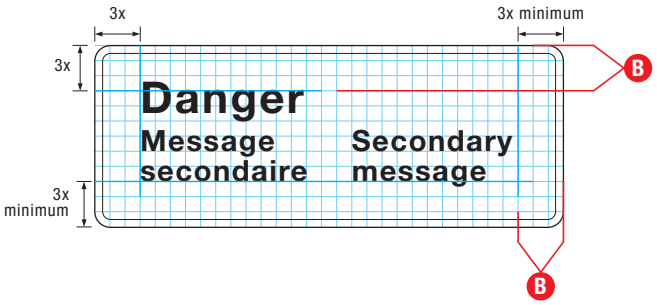
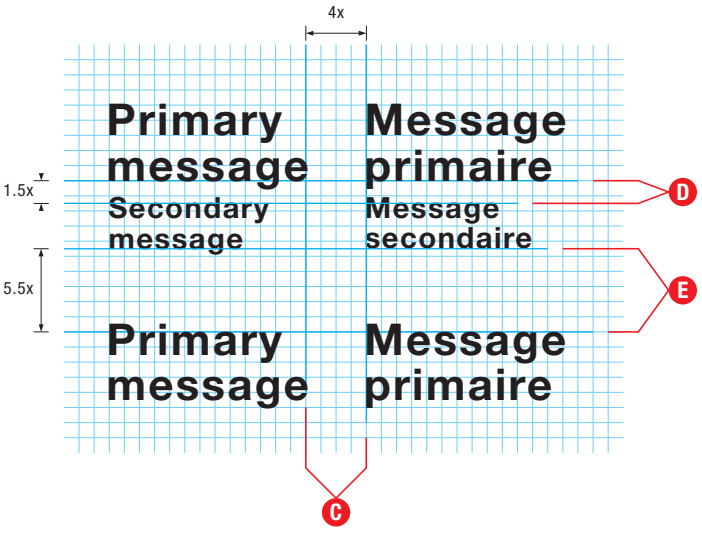


Figure F.3
C = Gutters (horizontal space between English and French text message)
D = Vertical space between primary and secondary text message (primary text message above)
E = Vertical space between two different text messages



1.5 to 1 Text Message Hierarchy (CONTINUED)

Figure F.4
C = Gutters (horizontal space between English and French text message)
F = Vertical space between primary and secondary text message (primary text message below)

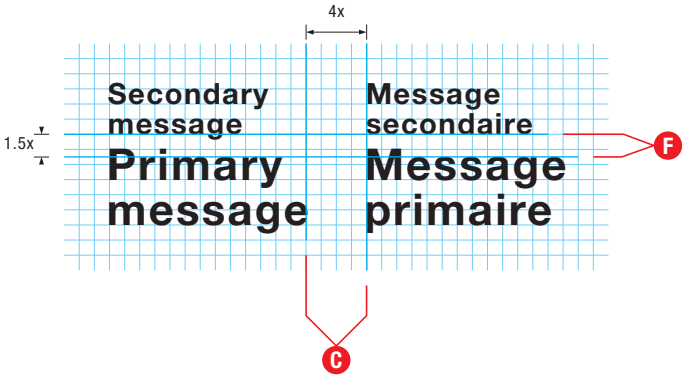
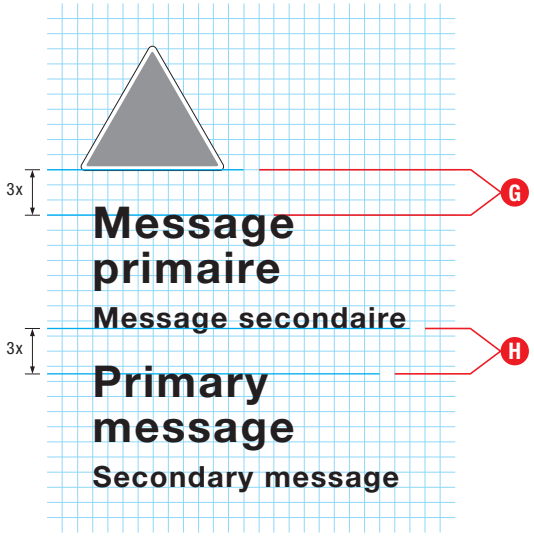


Figure F.5
G = Vertical space between symbol and text message
H = Vertical space between English and French text message



2 to 1 Text Message Hierarchy

Creating a Ratio of 2 to 1

The following layout specifications are applied when developing a text message hierarchy of 2 to 1.

Figure F.6
A = x-height of primary text message

Figure F.7
B = Margins

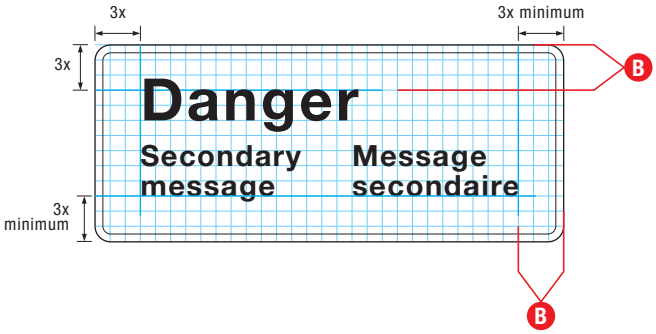
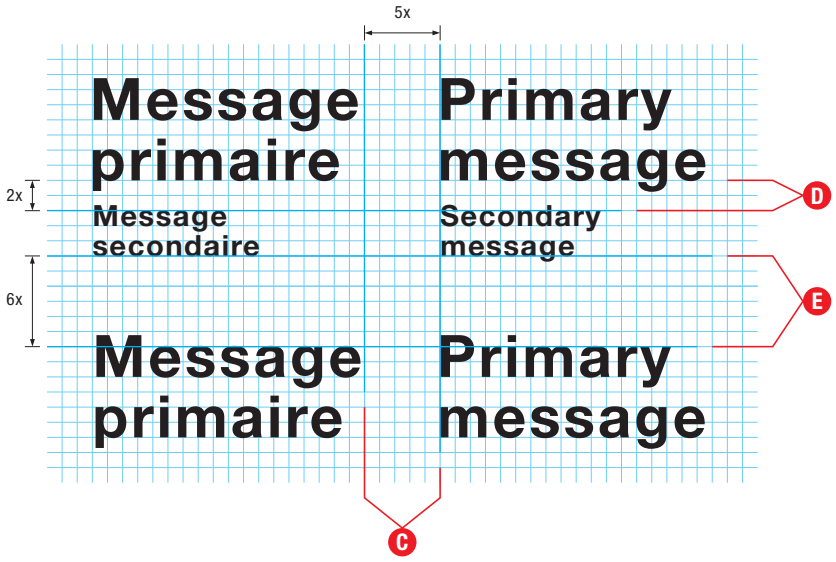


Figure F.8
C = Gutters (horizontal space between English and French text message)
D = Vertical space between primary and secondary text message (primary text message above)
E = Vertical space between two different text messages



2 to 1 Text Message Hierarchy (CONTINUED)

Figure F.9
C = Gutters (horizontal space between English and French text message)
F = Vertical space between primary and secondary message (primary text message below)

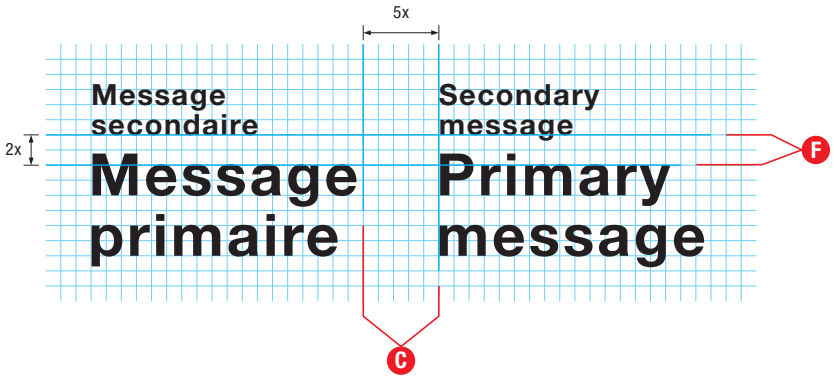
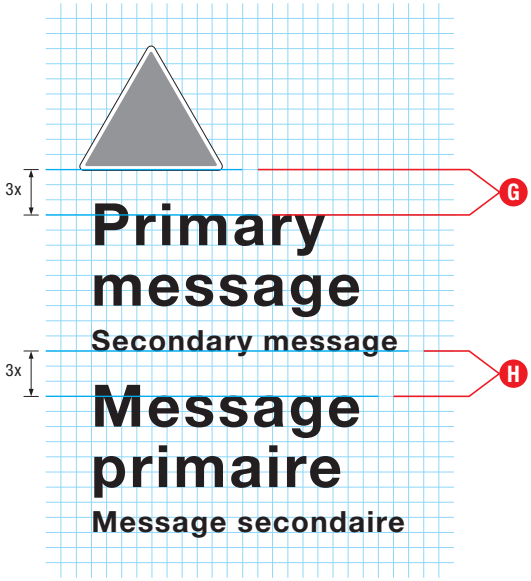


Figure F.10
G = Vertical space between symbol and text message
H = Vertical space between English and French text message



3 to 1 Text Message Hierarchy

Creating a Ratio of 3 to 1

The following layout specifications are applied when developing a text message hierarchy of 3 to 1.

Figure F.11
A = x-height of primary text message

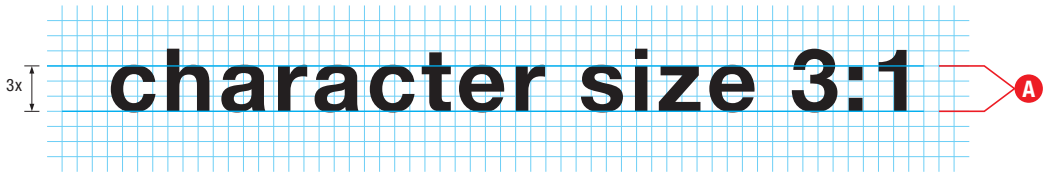


Figure F.12
B = Margins

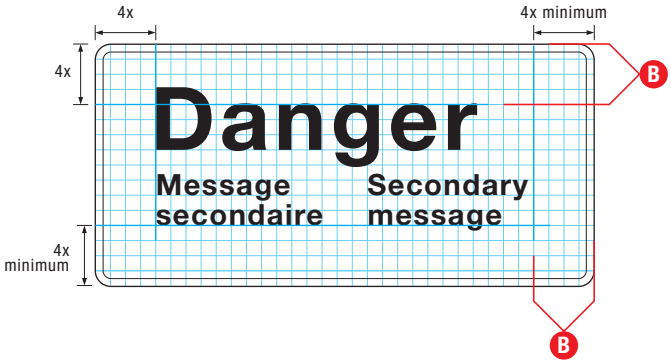
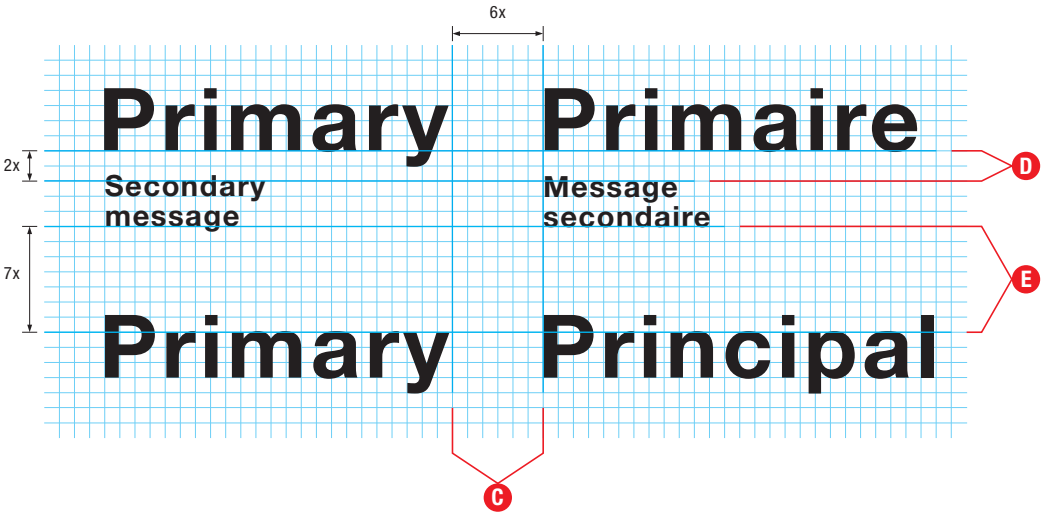


Figure F.13
C = Gutters (horizontal space between English and French text message)
D = Vertical space between primary and secondary message (primary text message above)
E = Vertical space between two different text messages



3 to 1 Text Message Hierarchy (CONTINUED)

Figure F.14
C = Gutters (horizontal space between English and French text message)
F = Vertical space between primary and secondary text message (primary text message below)

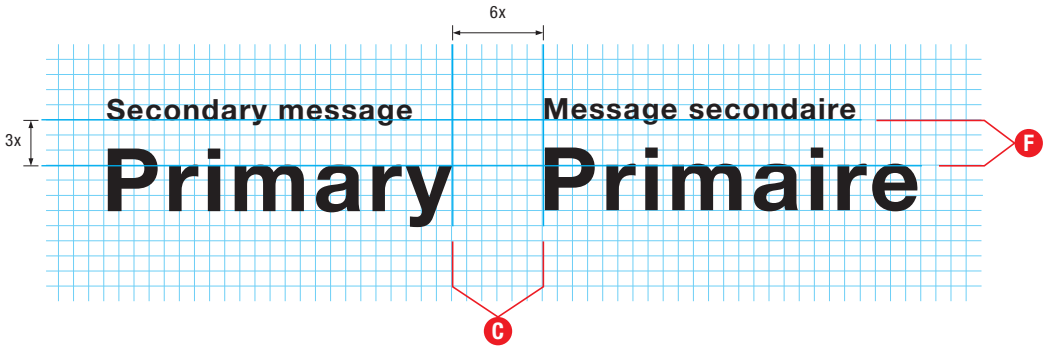
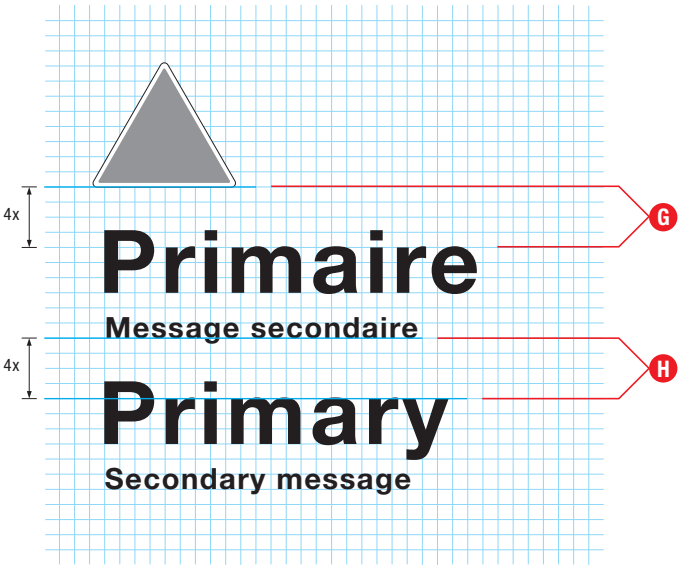


Figure F.15
G = Vertical space between symbol and text message
H = Vertical space between English and French text message



G

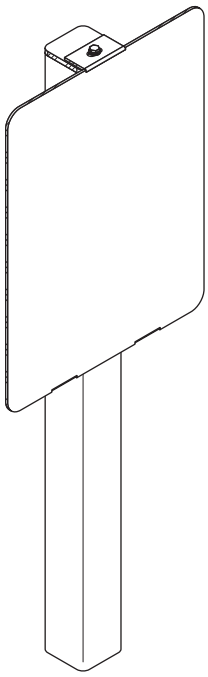
Appendix G – Technical Drawings

Appendix G – Technical Drawings

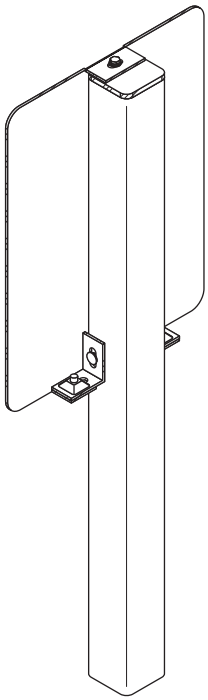
1-post Sign – Aluminum Post	2
2-post Sign – Aluminum Posts	6
3-post Sign – Aluminum Posts	10
1-post Sign – Wood Post	16
2-post Sign – Wood Posts	20
3-post Sign – Wood Posts	24
L-bracket	30
Steel Cap For Wood Posts	31
Fee Sign	32
Security Latch Sub-assembly	40
Fee Sign Mounting Brackets	41

1-post Sign – Aluminum Post

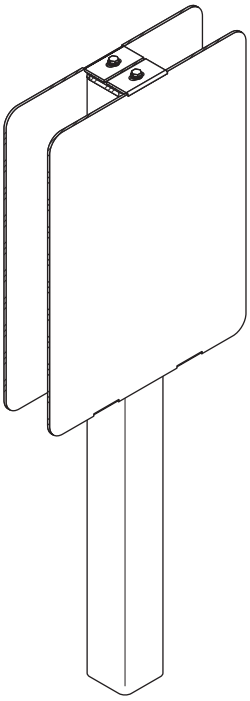
Single-sided Sign
Front Isometric View



Single-sided Sign
Rear Isometric View

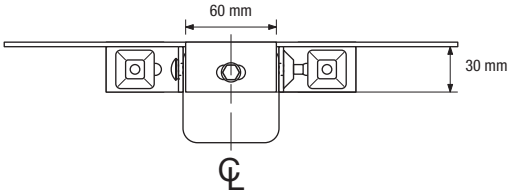


Double-sided Sign
Isometric View

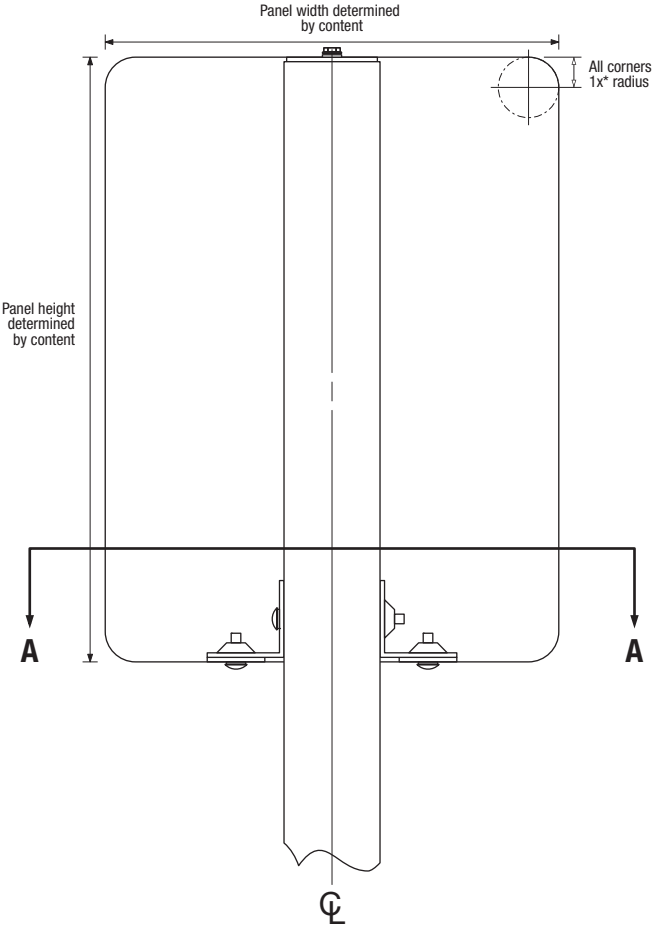


1-post Sign – Aluminum Post (CONTINUED)

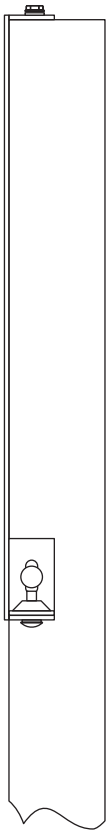
Top Plan View



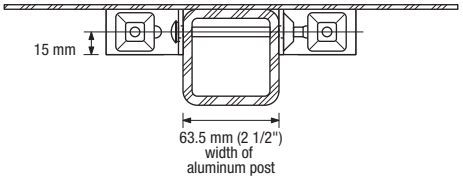
Rear Elevation



Side Elevation

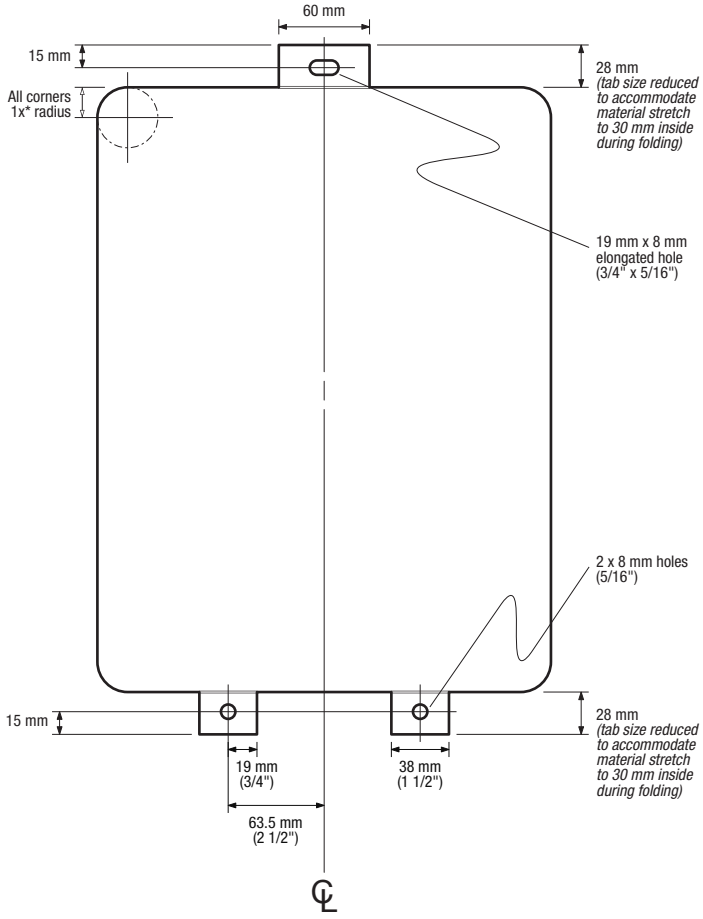


Section A-A



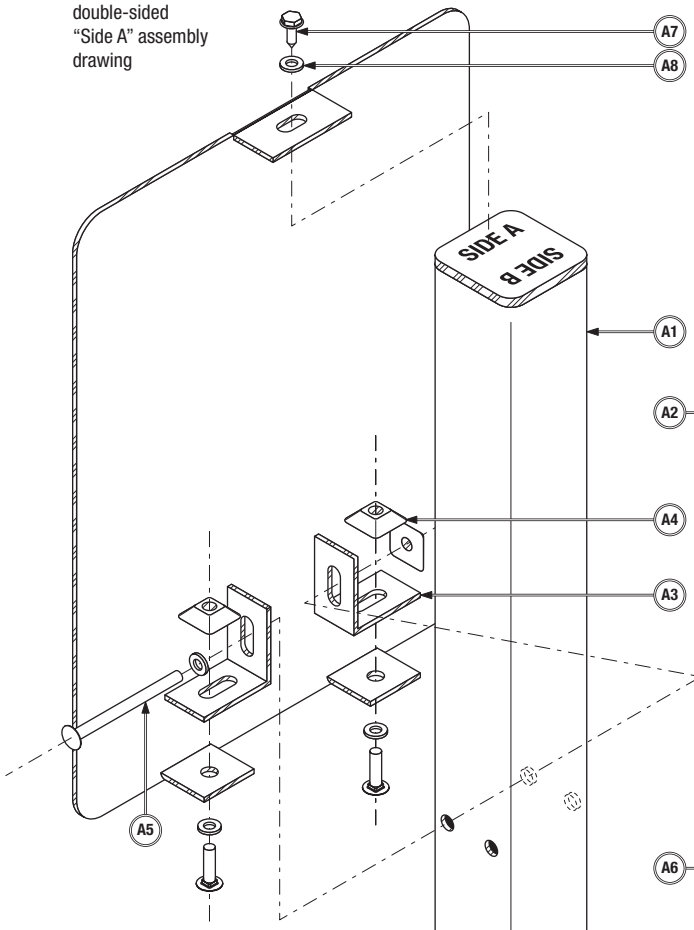
1-post Sign – Aluminum Post (CONTINUED)

1-post Sign: Cut Pattern

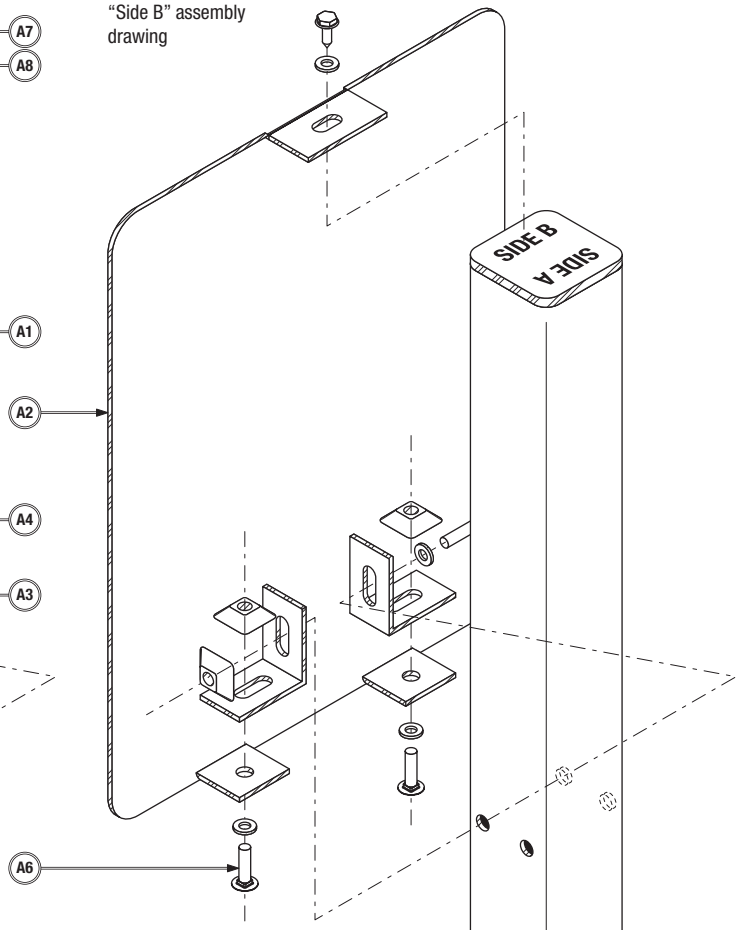


1-post Sign – Aluminum Post (CONTINUED)

1-post Sign:
single-sided or
double-sided
"Side A" assembly
drawing



1-post Sign:
double-sided
"Side B" assembly
drawing

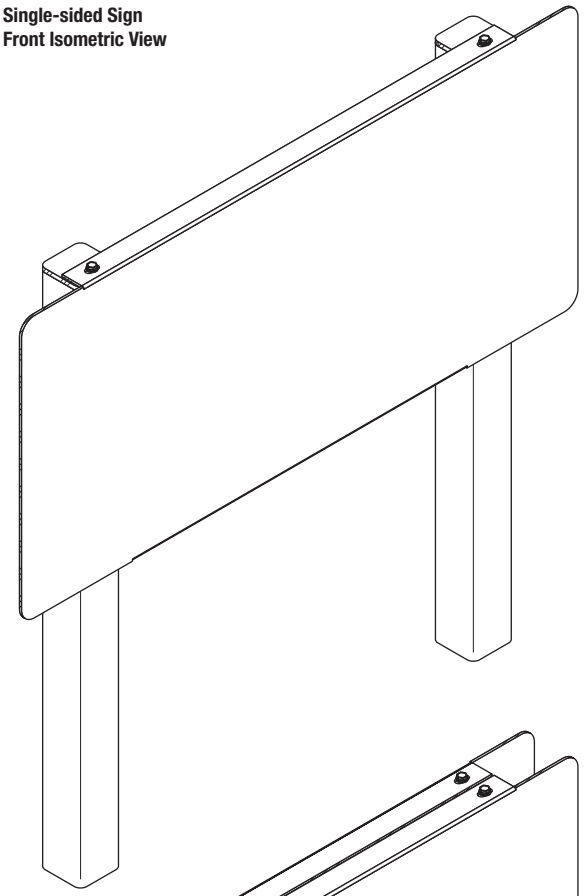


Parts List Per Side
1-post Sign – Aluminum Post

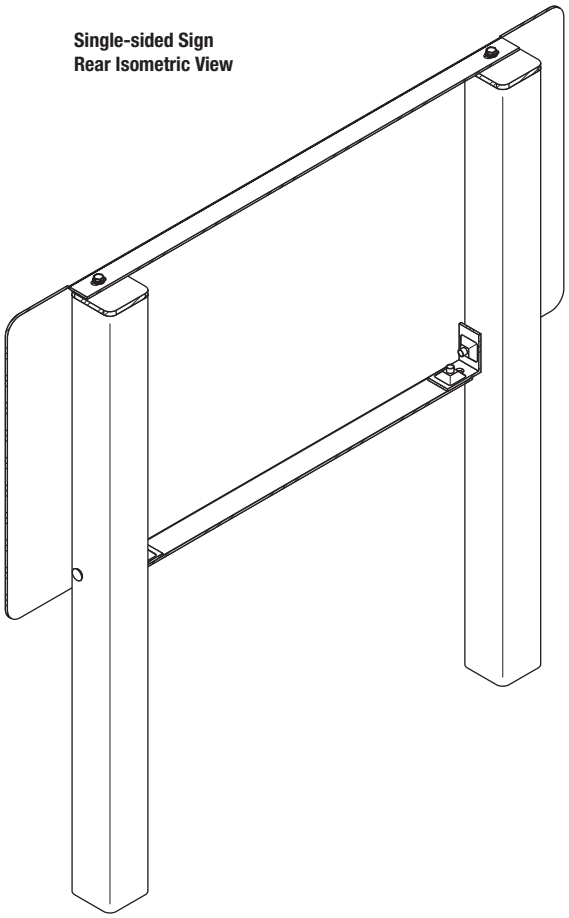
Item #	Description	Qty
A1	2 1/2" x 2 1/2" x 1/4" thick aluminum post	1
A2	3 mm thick aluminum sign face	1
A3	30 mm x 50 mm x 50 mm x 3 mm thick aluminum L-bracket	2
A4	Tamper-resistant nut 1/4"-20 UNC, steel case-hardened, zinc-plated	3
A5	Carriage bolt round head, square neck, 1/4"-20 x 3 1/2"; 18-8 stainless steel	1
A6	Carriage bolt round head, square neck, 1/4"-20 x 1/2"; 18-8 stainless steel	2
A7	Hex screw, self tapping, #14 x 3/4"; 18-8 stainless steel	1
A8	Nylon 6/6, 1/4" flat washer	4

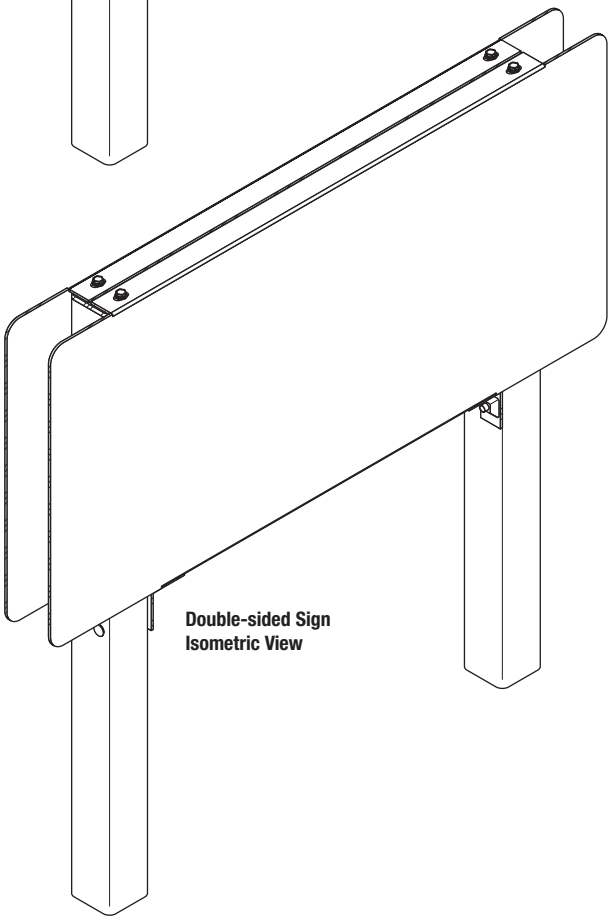
2-post Sign – Aluminum Posts

Single-sided Sign
Front Isometric View



Single-sided Sign
Rear Isometric View

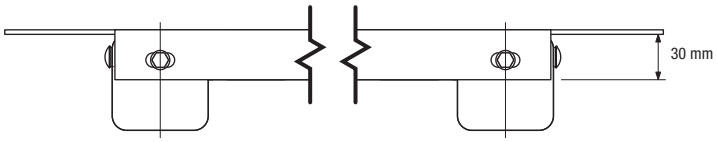




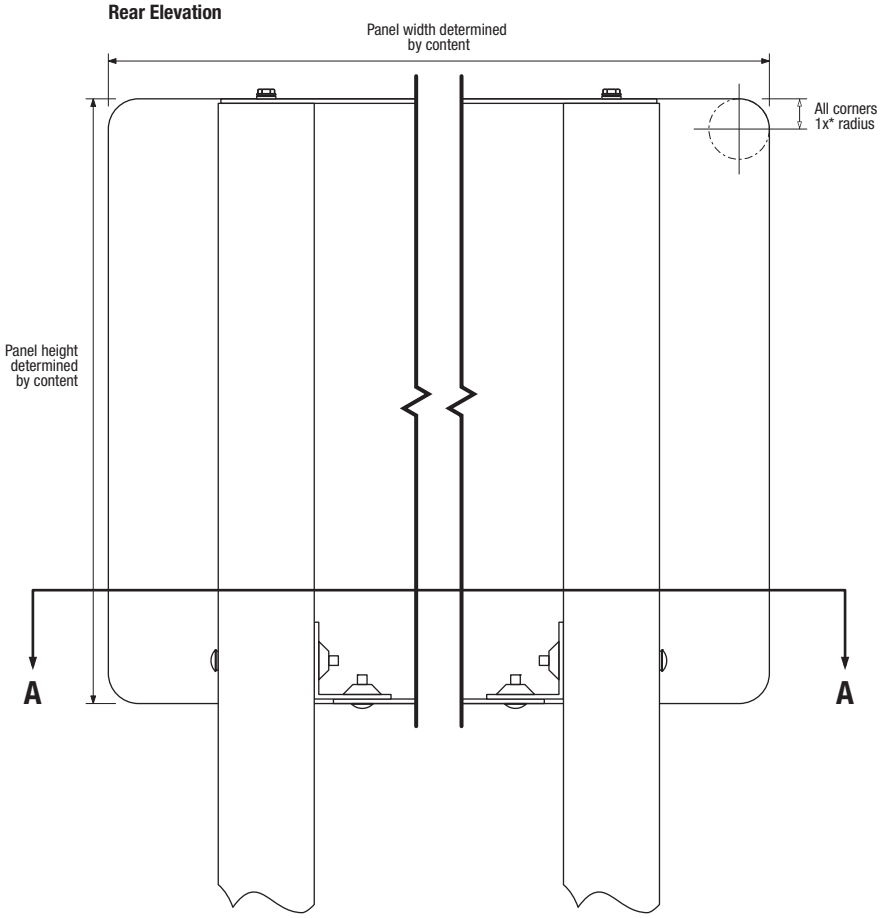
Double-sided Sign
Isometric View

2-post Sign – Aluminum Posts (CONTINUED)

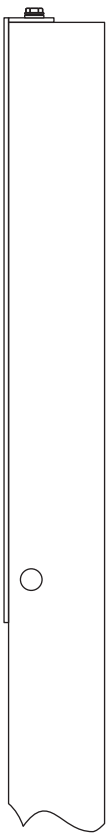
Top Plan View



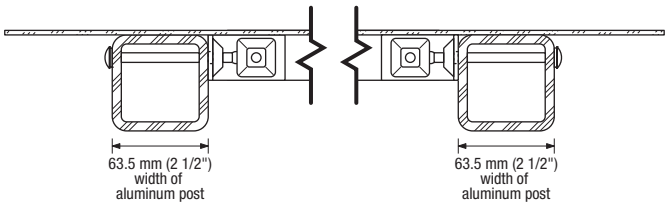
Rear Elevation



Side Elevation

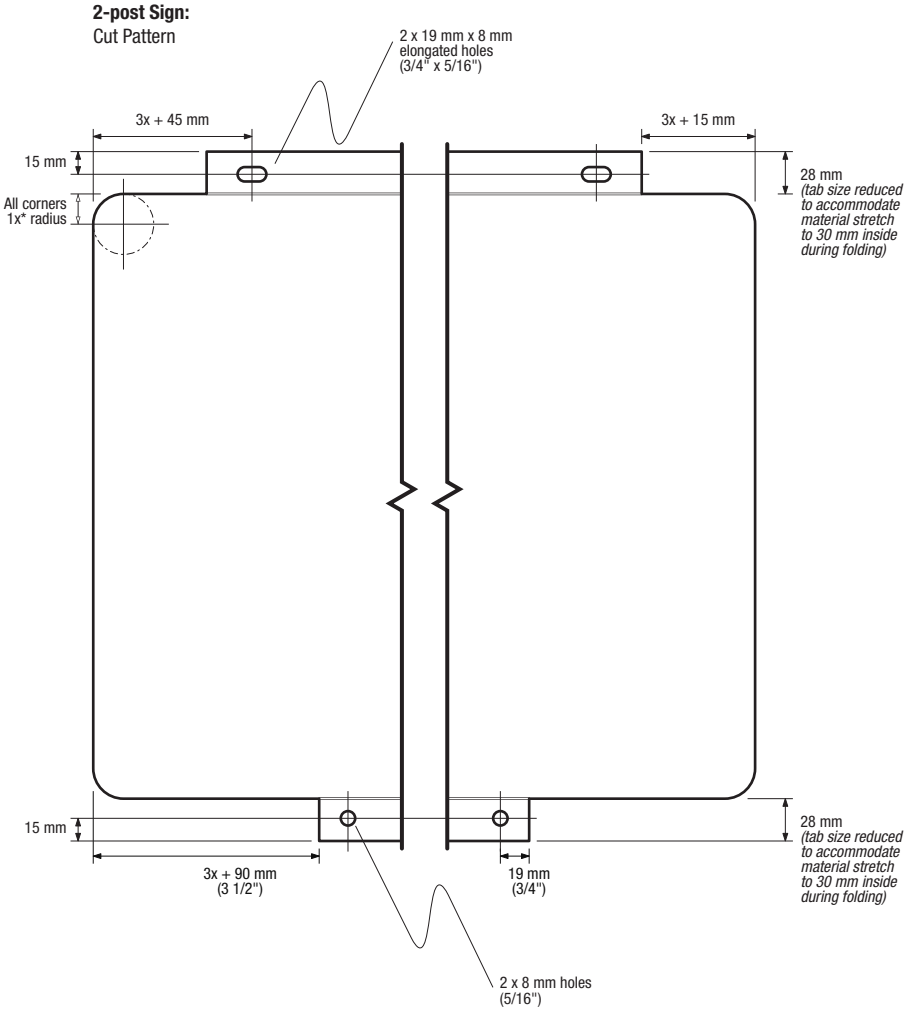


Section A-A



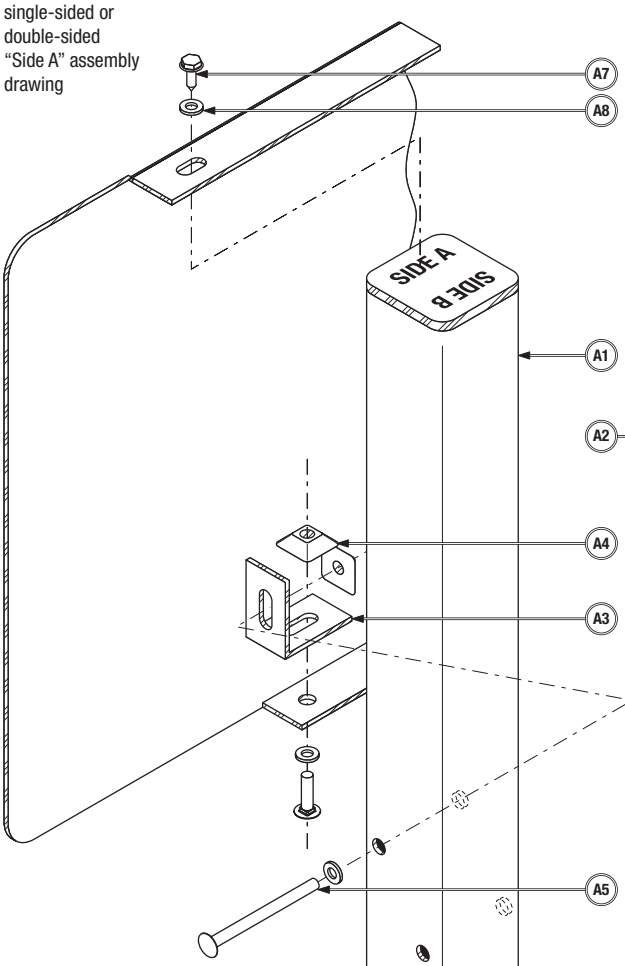
2-post Sign – Aluminum Posts (CONTINUED)

2-post Sign:
Cut Pattern

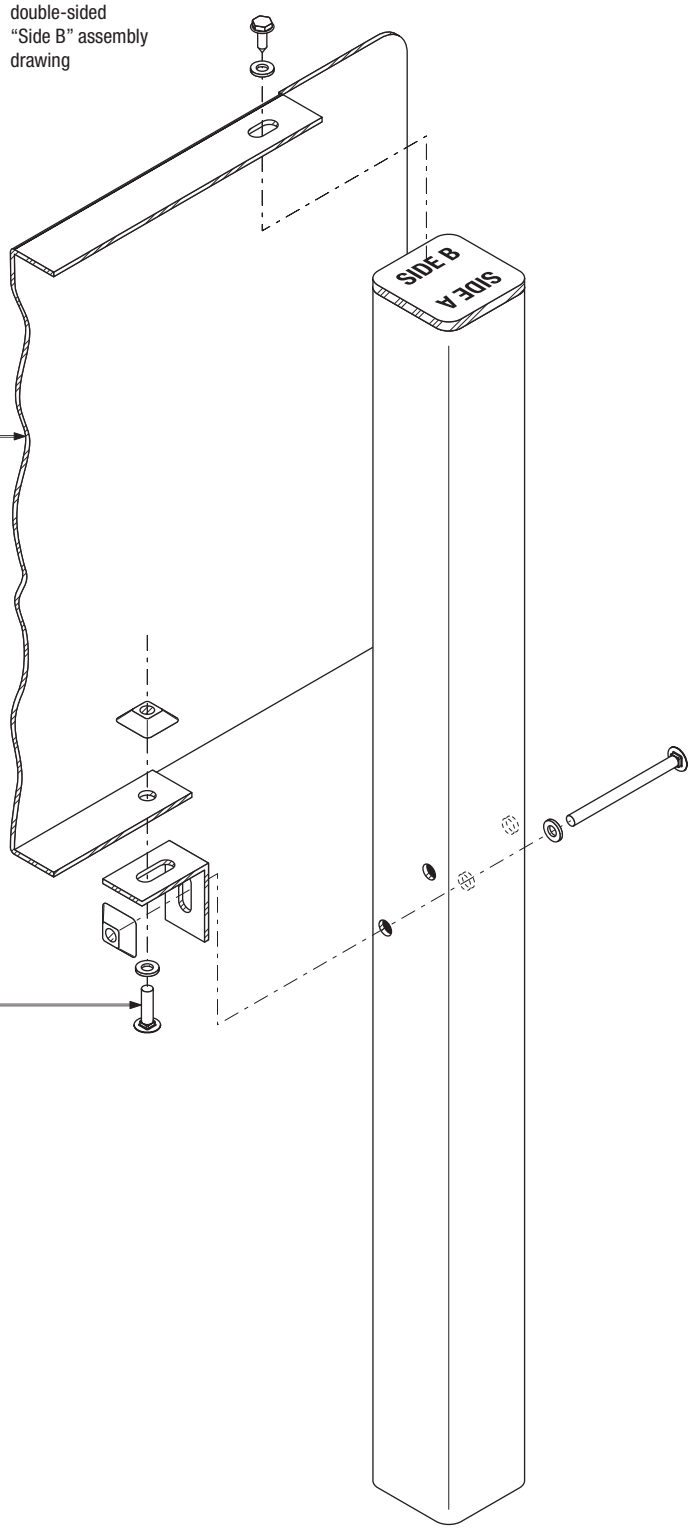


2-post Sign – Aluminum Posts (CONTINUED)

2-post sign:
single-sided or
double-sided
"Side A" assembly
drawing



2-post Sign:
double-sided
"Side B" assembly
drawing

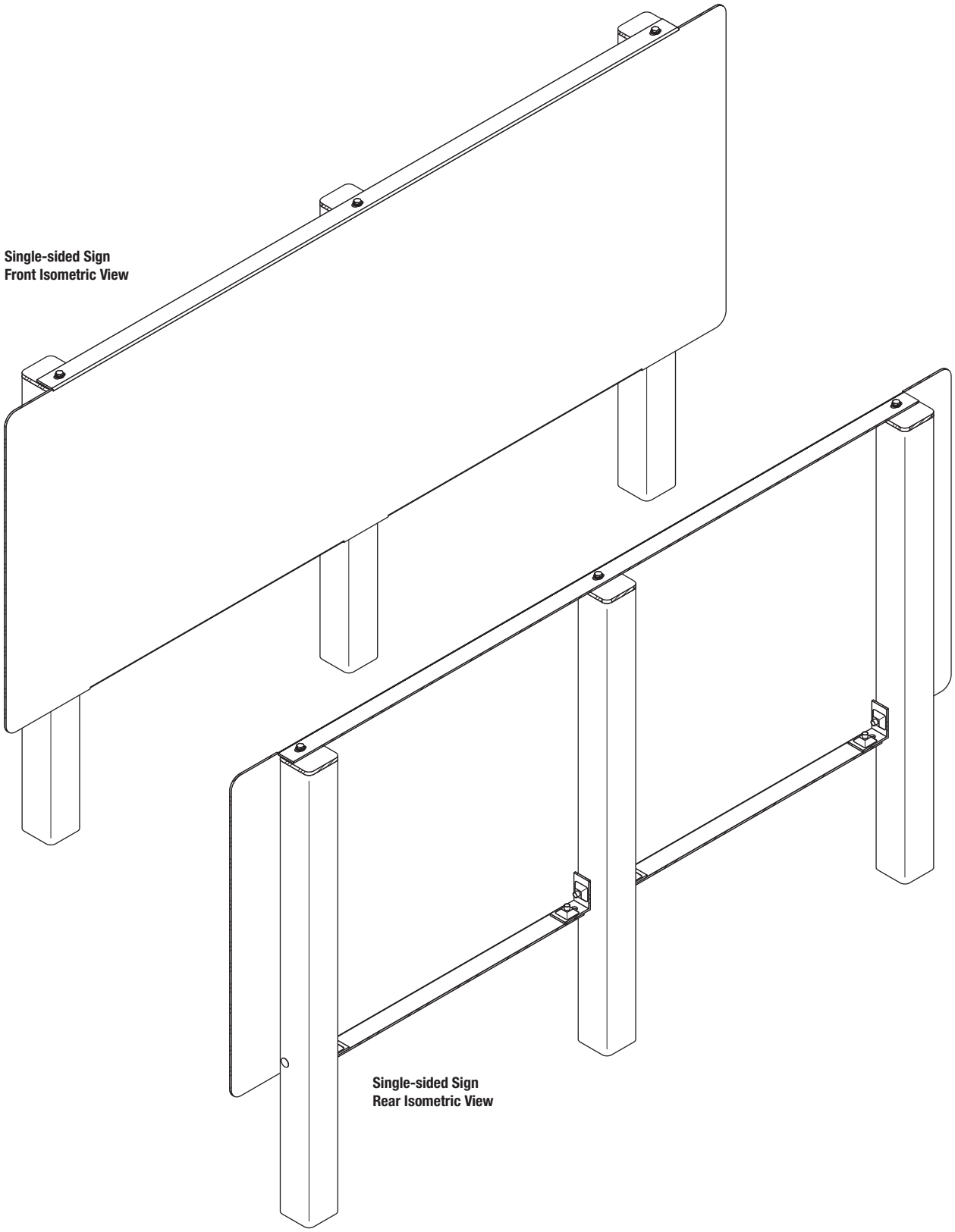


Parts List Per Side
2-post Sign – Aluminum Posts

Item #	Description	Qty
A1	2 1/2" x 2 1/2" x 1/4" thick aluminum post	2
A2	3 mm thick aluminum sign face	1
A3	30 mm x 50 mm x 50 mm x 3 mm thick aluminum L-bracket	2
A4	Tamper-resistant nut 1/4"-20 UNC, steel case-hardened, zinc-plated	4
A5	Carriage bolt round head, square neck, 1/4"-20 x 3 1/2"; 18-8 stainless steel	2
A6	Carriage bolt round head, square neck, 1/4"-20 x 1/2"; 18-8 stainless steel	2
A7	Hex screw, self tapping, #14 x 3/4"; 18-8 stainless steel	2
A8	Nylon 6/6, 1/4" flat washer	6

3-post Sign – Aluminum Posts

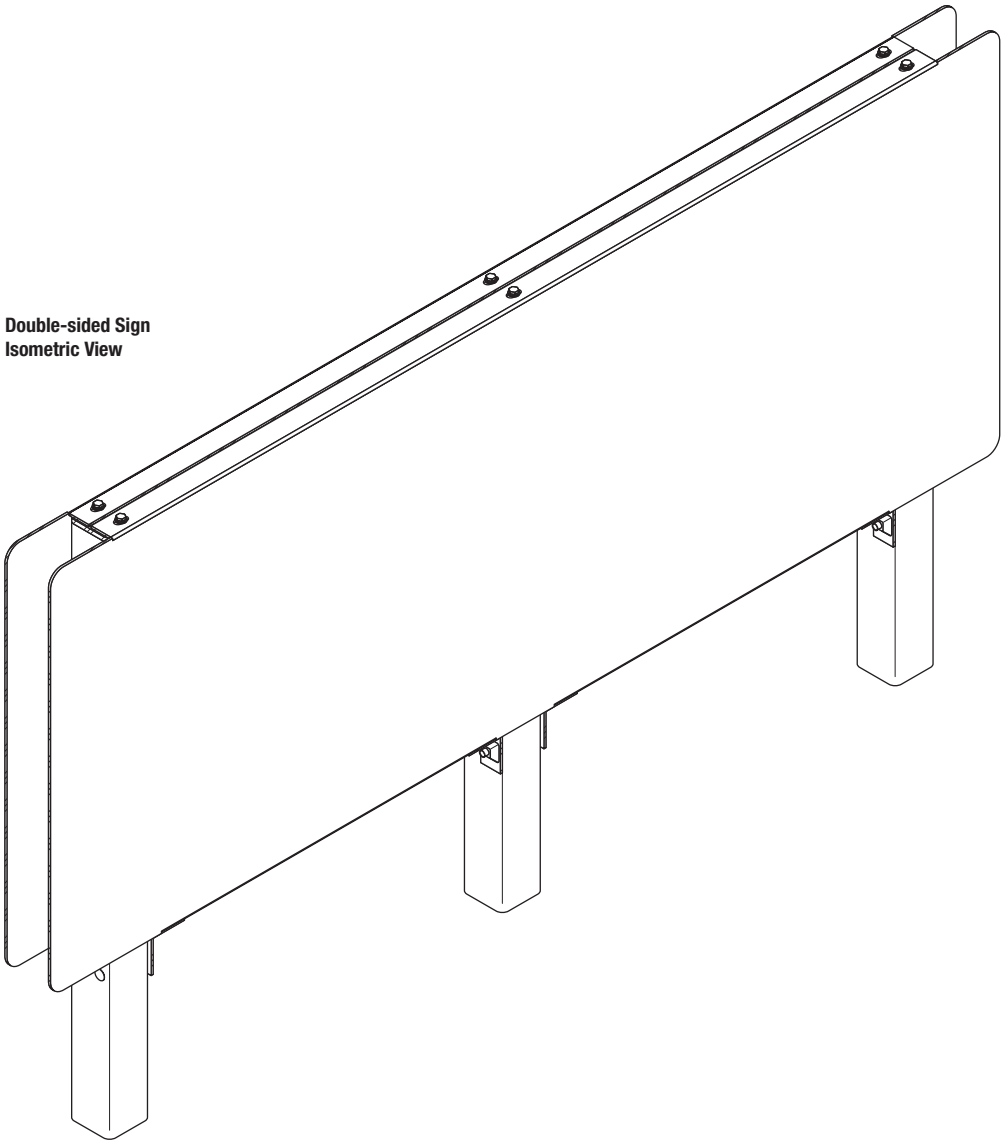
Single-sided Sign
Front Isometric View



Single-sided Sign
Rear Isometric View

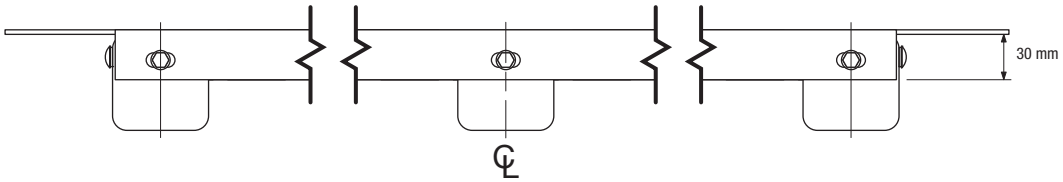
3-post Sign – Aluminum Posts (CONTINUED)

Double-sided Sign
Isometric View



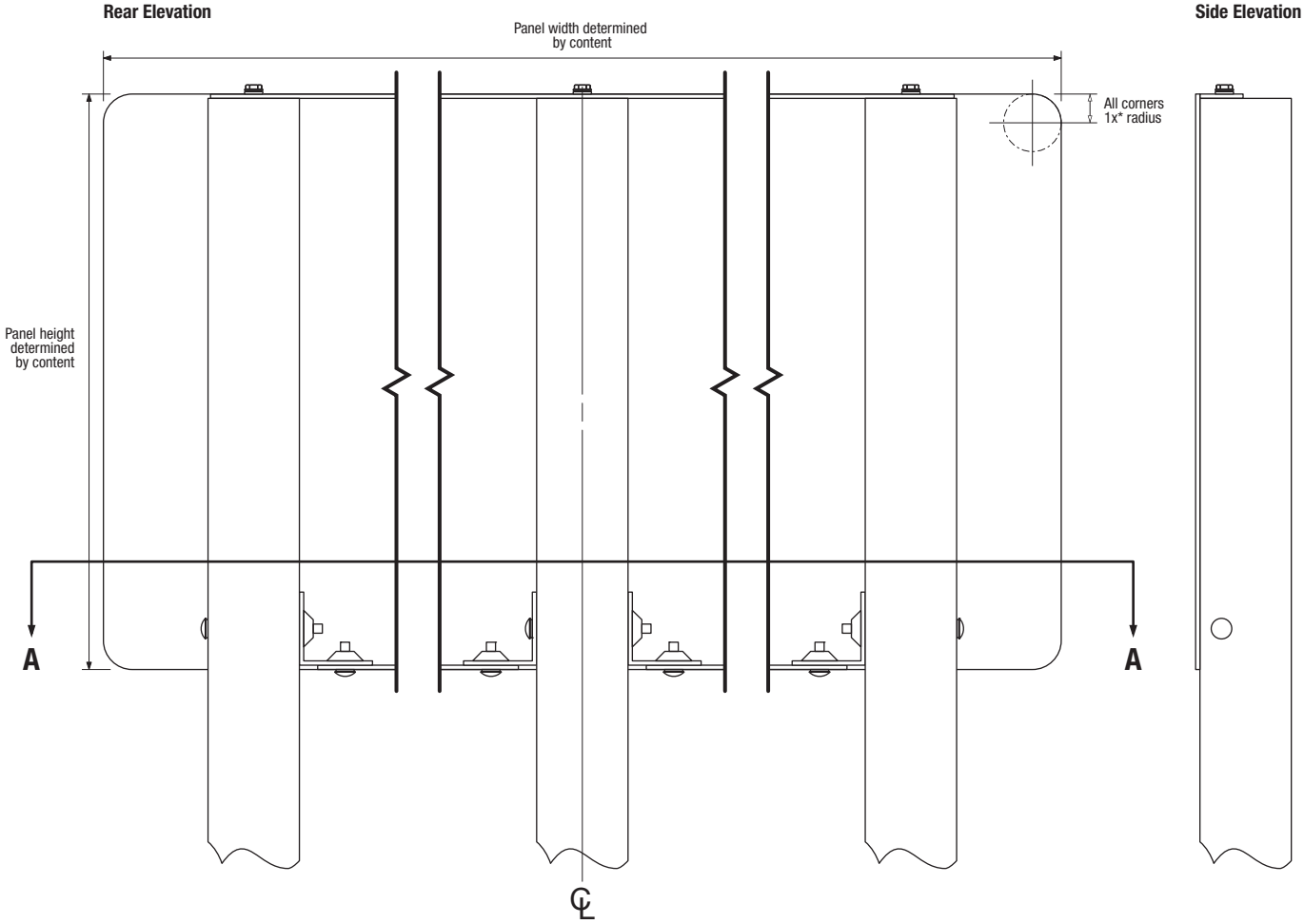
3-post Sign – Aluminum Posts (CONTINUED)

Top Plan View

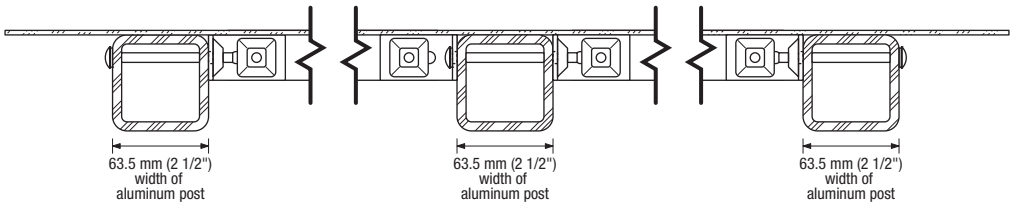


Rear Elevation

Side Elevation

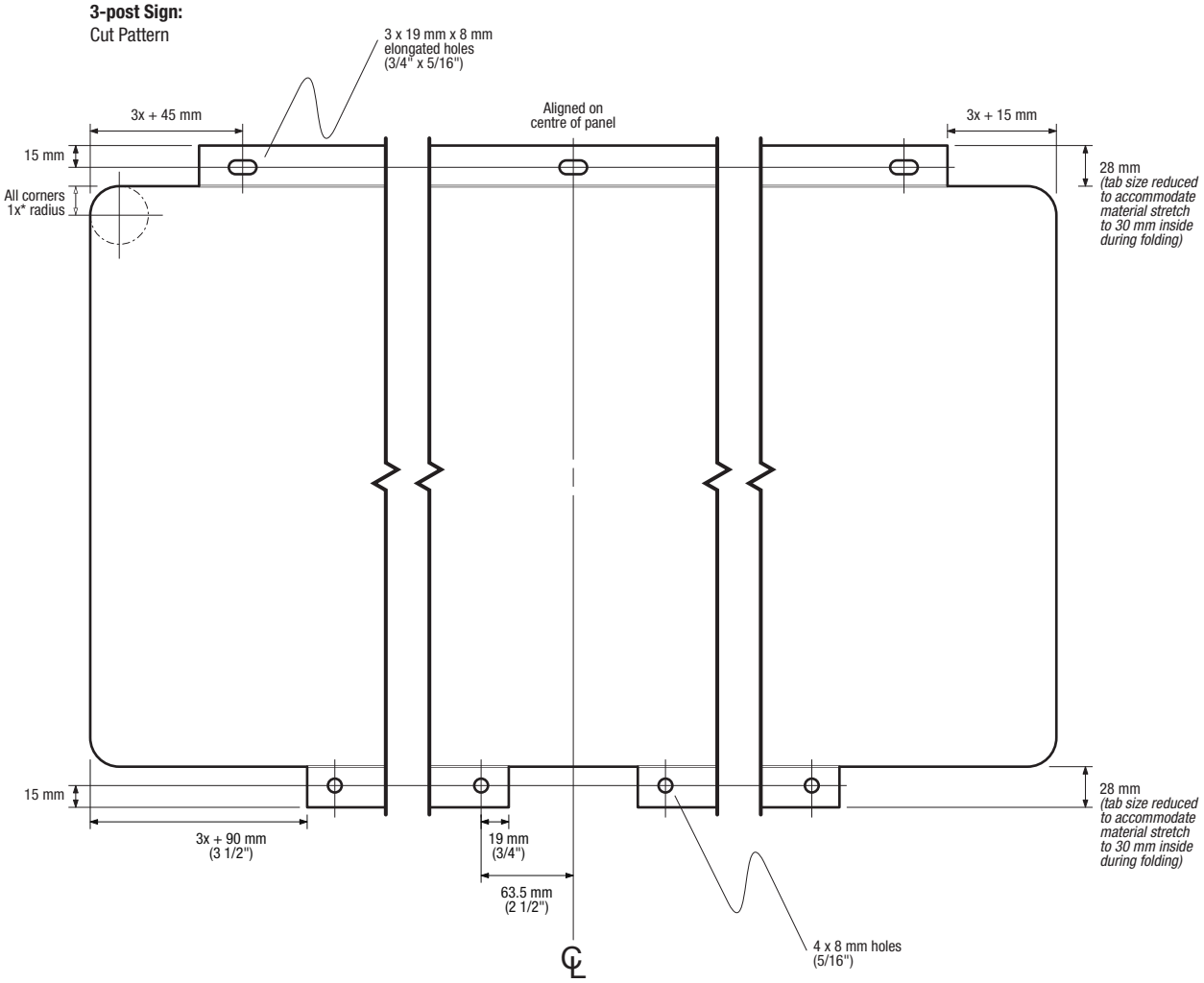


Section A-A



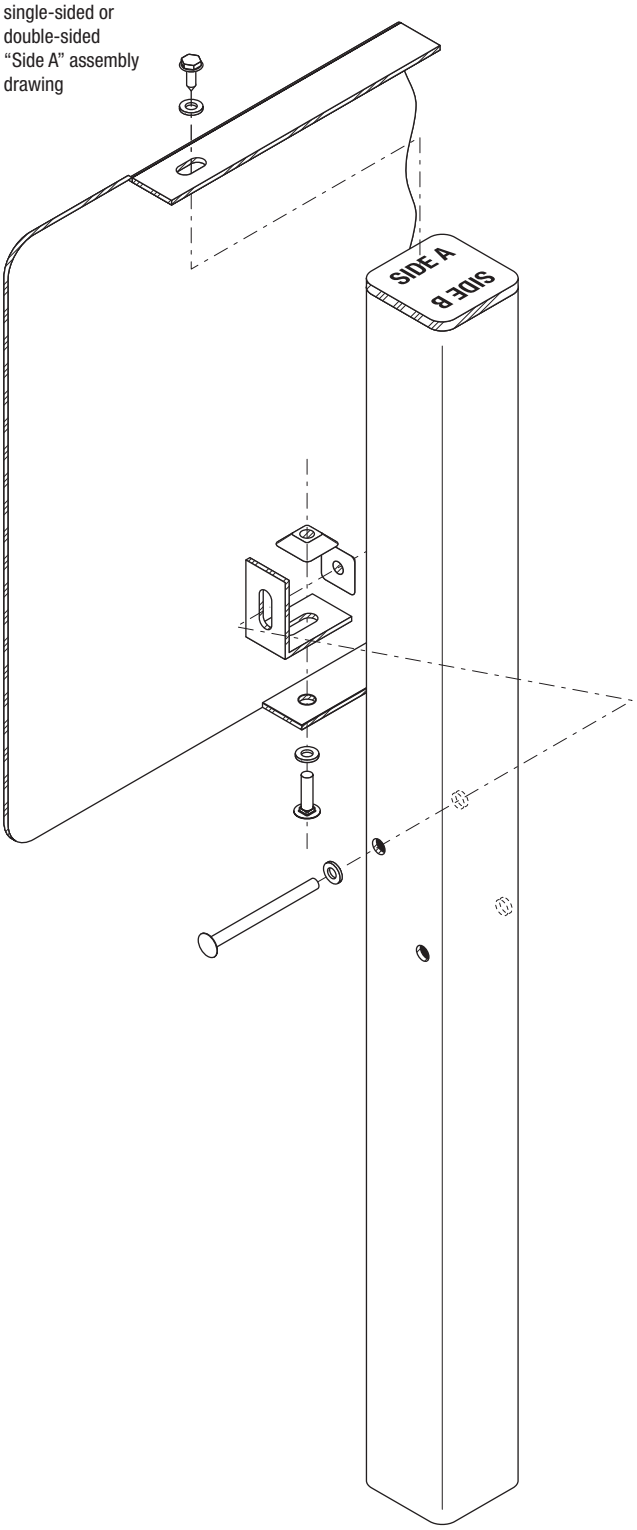
3-post Sign – Aluminum Posts (CONTINUED)

3-post Sign:
Cut Pattern

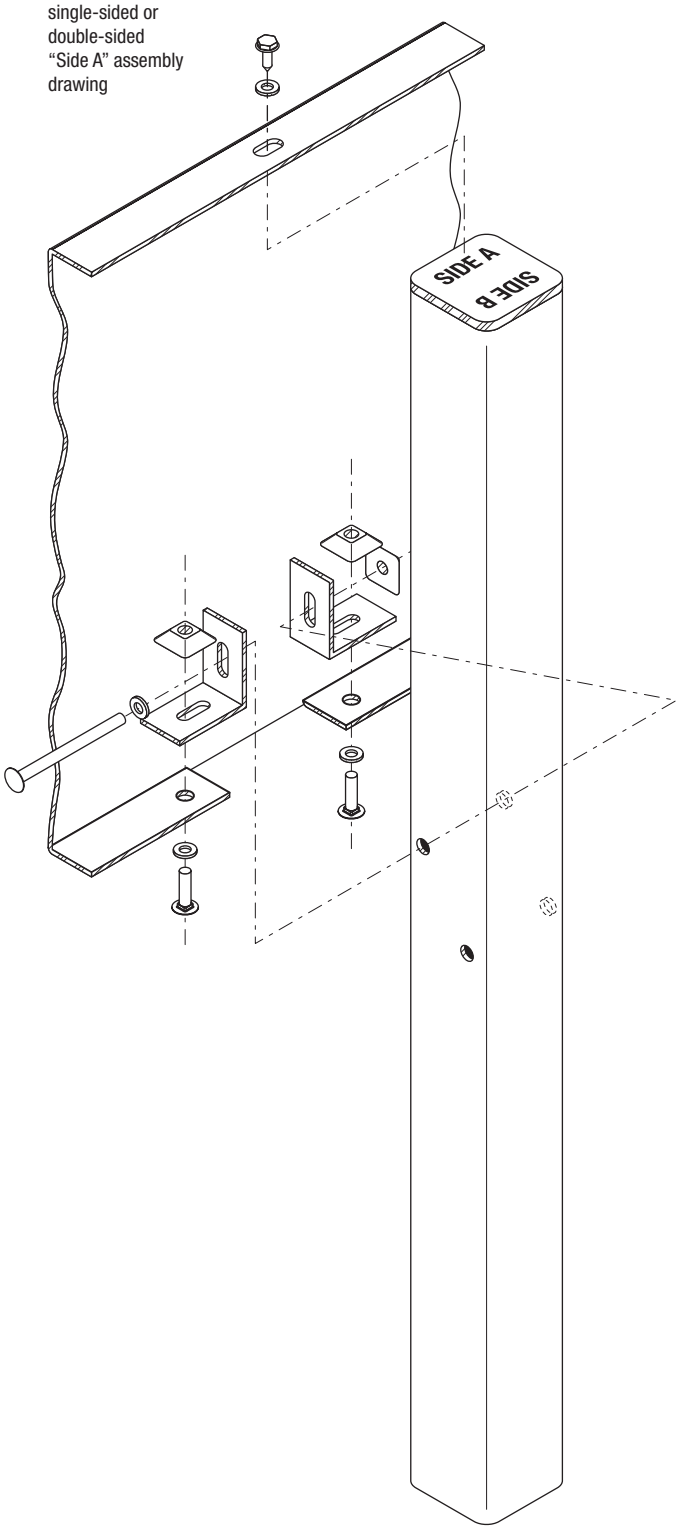


3-post Sign – Aluminum Posts (CONTINUED)

3-post End Post:
single-sided or
double-sided
"Side A" assembly
drawing

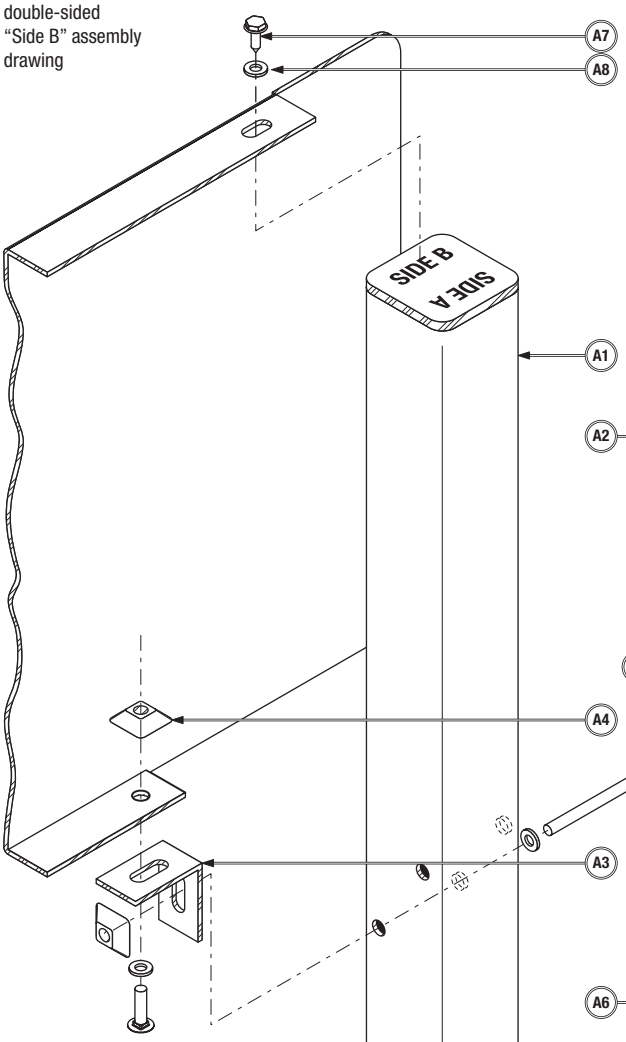


3-post Centre Post:
single-sided or
double-sided
"Side A" assembly
drawing

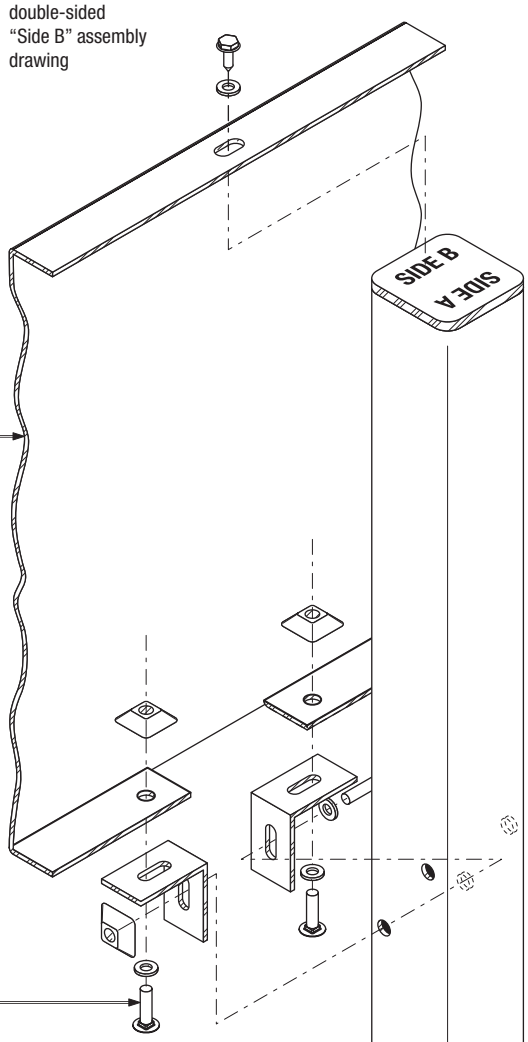


3-post Sign – Aluminum Posts (CONTINUED)

3-post End Post:
double-sided
"Side B" assembly
drawing



3-post Centre Post:
double-sided
"Side B" assembly
drawing

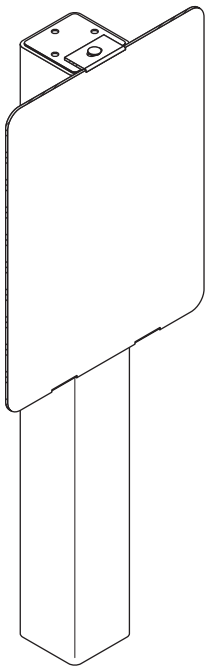


Parts List Per Side
3-post Sign – Aluminum Posts

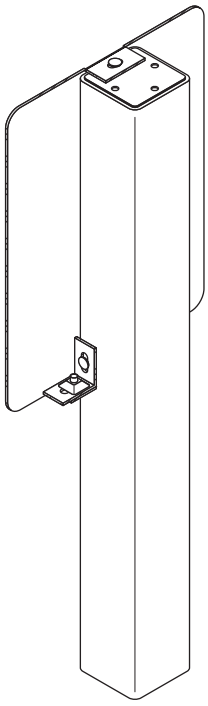
Item #	Description	Qty
A1	2 1/2" x 2 1/2" x 1/4" thick aluminum face	3
A2	3 mm thick aluminum sign face	1
A3	30 mm x 50 mm x 50 mm x 3 mm thick aluminum L-bracket	4
A4	Tamper-resistant nut 1/4"-20 UNC, steel case-hardened, zinc-plated	7
A5	Carriage bolt round head, square neck, 1/4"-20 x 3 1/2"; 18-8 stainless steel	3
A6	Carriage bolt round head, square neck, 1/4"-20 x 1/2"; 18-8 stainless steel	4
A7	Hex screw, self tapping, #14 x 3/4"; 18-8 stainless steel	3
A8	Nylon 6/6, 1/4" flat washer	10

1-post Sign – Wood Post

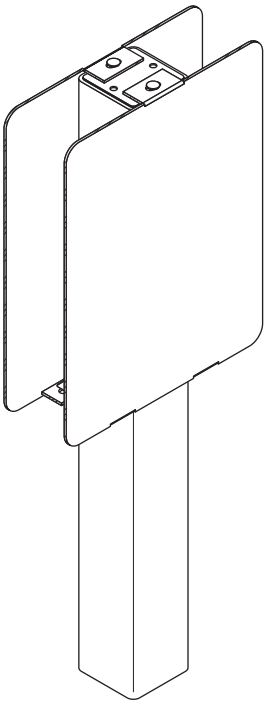
Single-sided Sign
Front Isometric View



Single-sided Sign
Rear Isometric View

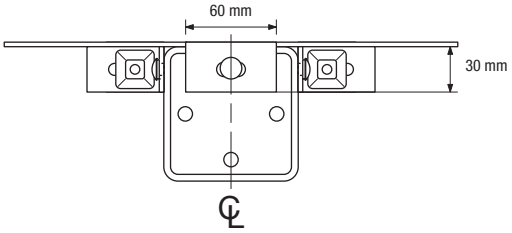


Double-sided Sign
Isometric View

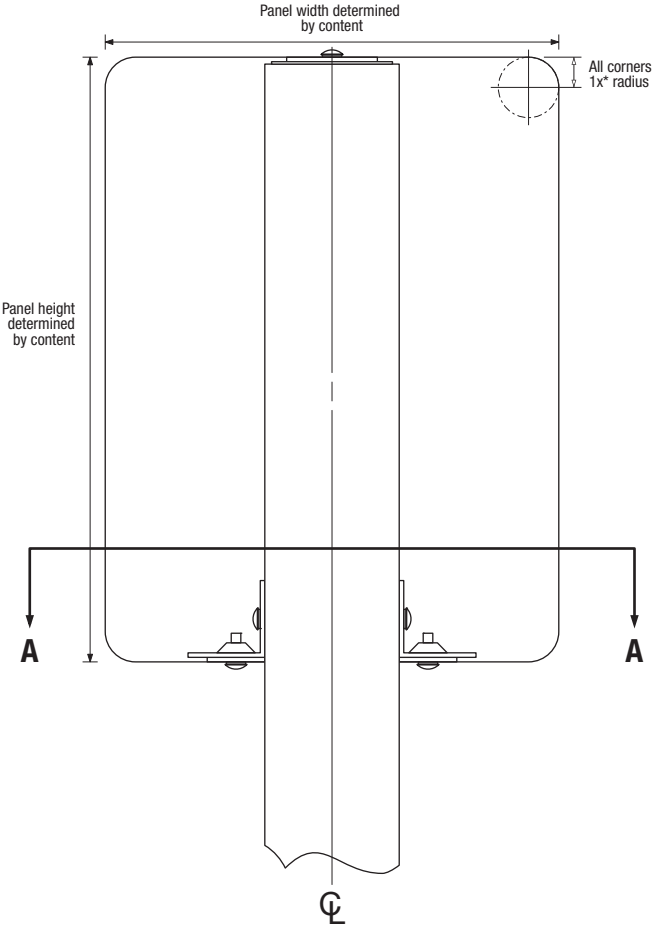


1-post Sign – Wood Post (CONTINUED)

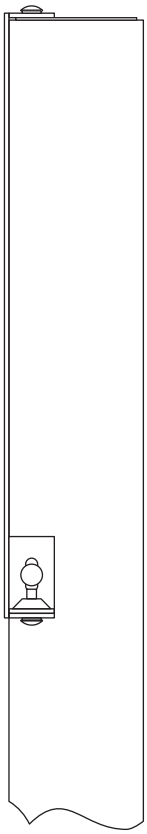
Top Plan View



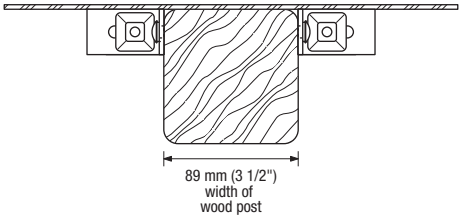
Rear Elevation



Side Elevation

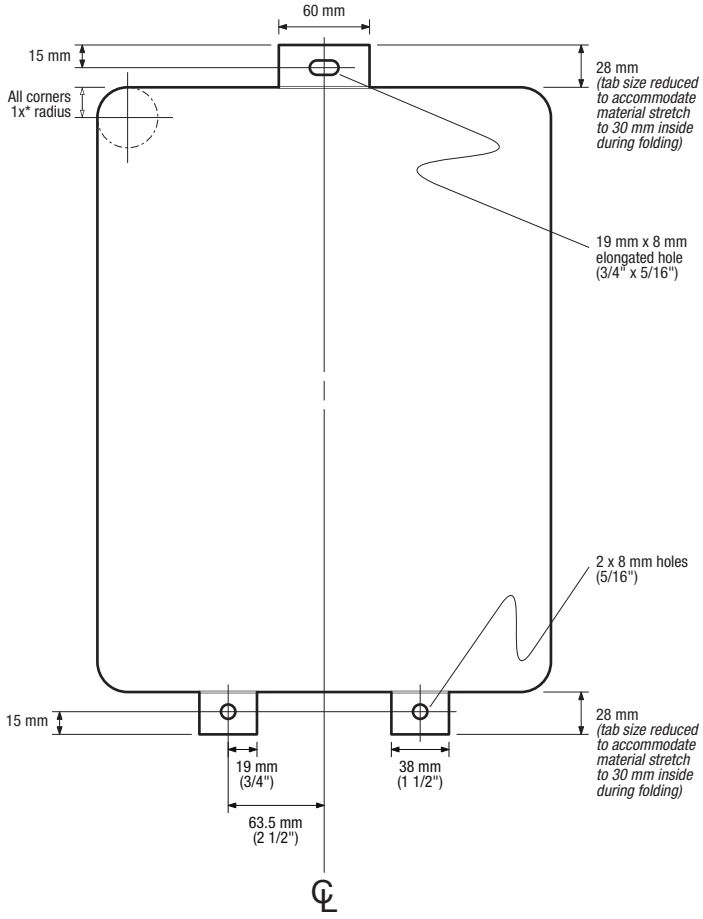


Section A-A



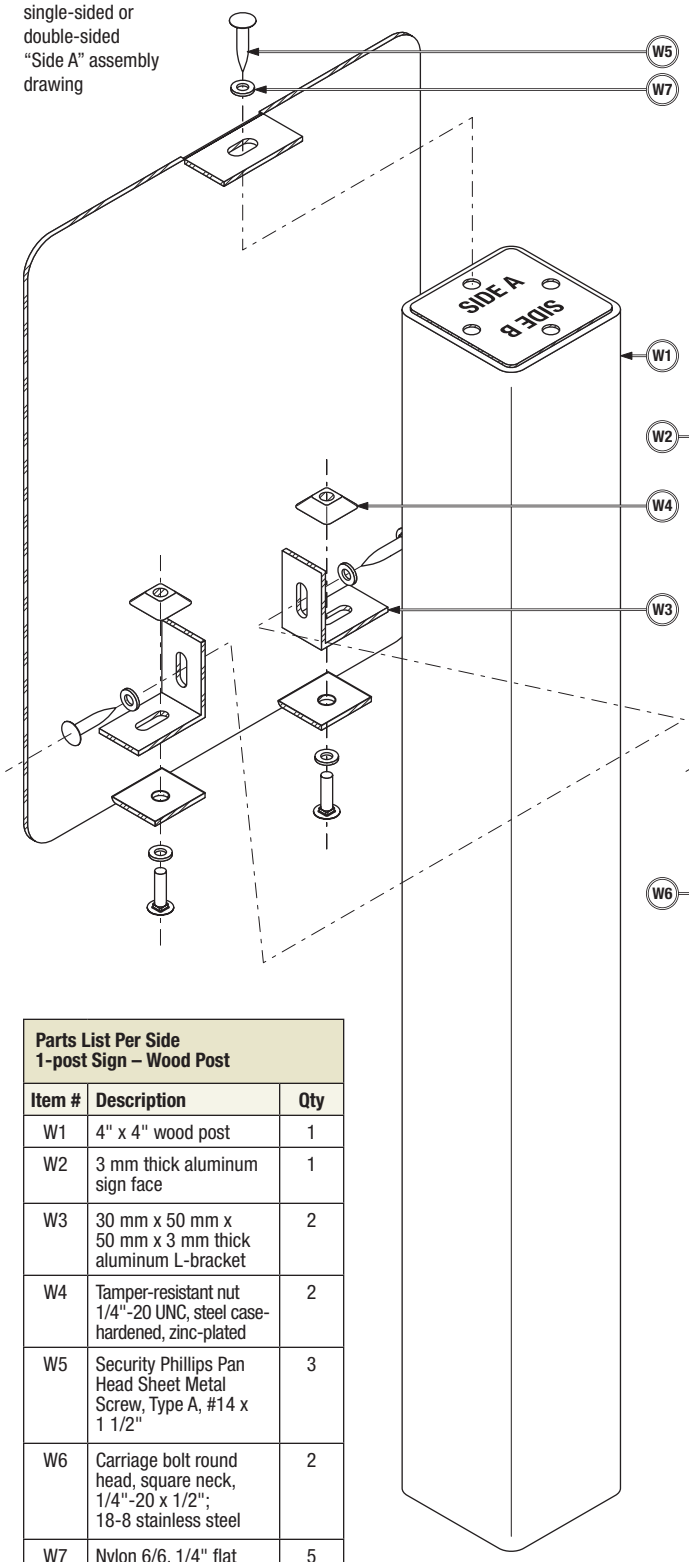
1-post Sign – Wood Post (CONTINUED)

1-post Sign: Cut Pattern

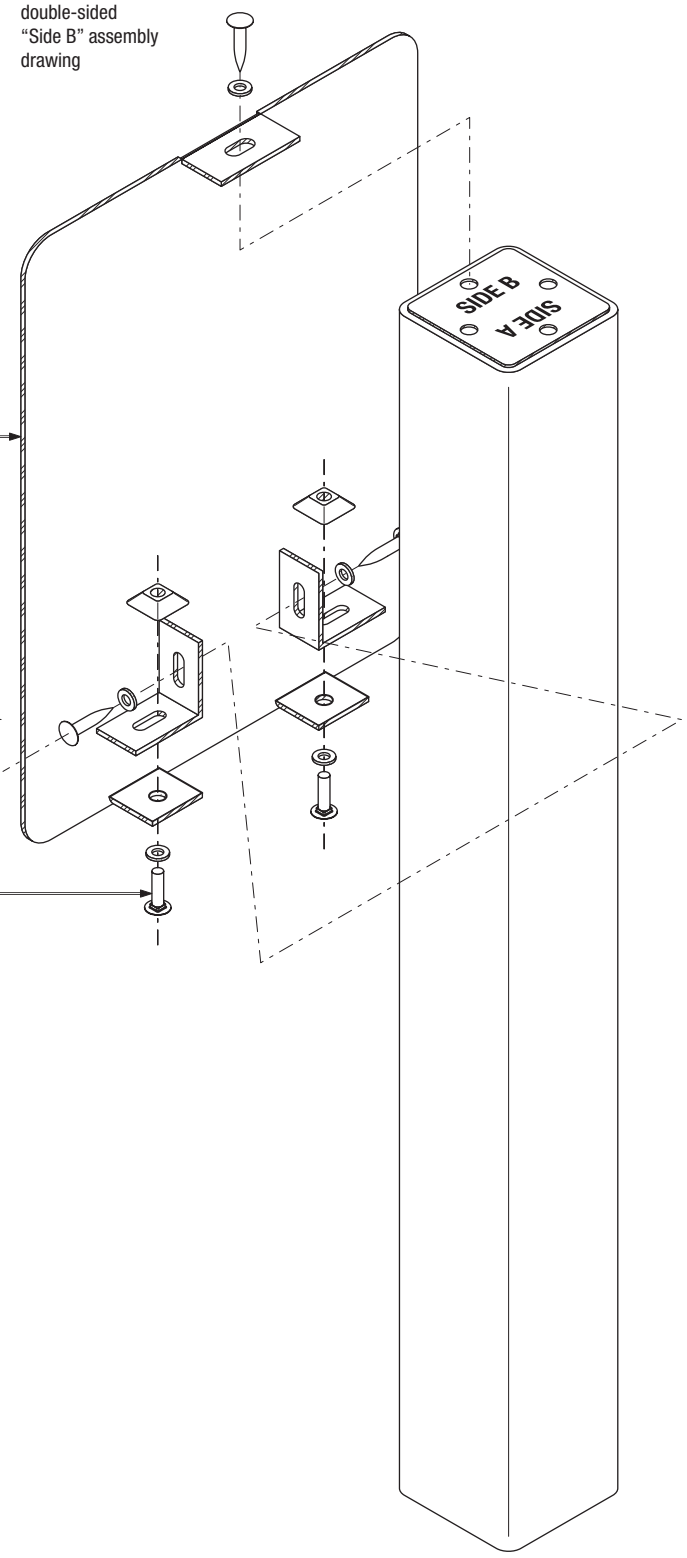


1-post Sign – Wood Post (CONTINUED)

1-post Sign:
single-sided or
double-sided
"Side A" assembly
drawing



1-post Sign:
double-sided
"Side B" assembly
drawing

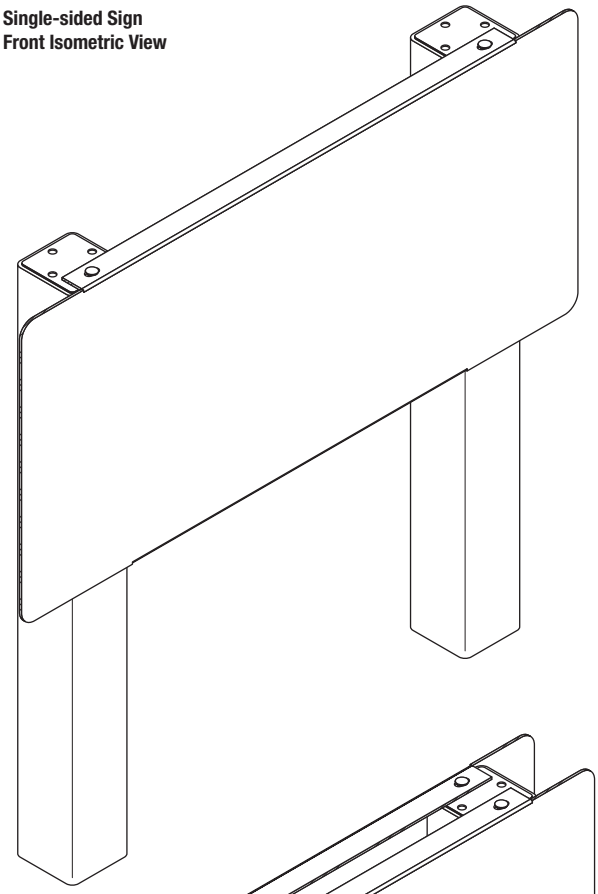


Parts List Per Side
1-post Sign – Wood Post

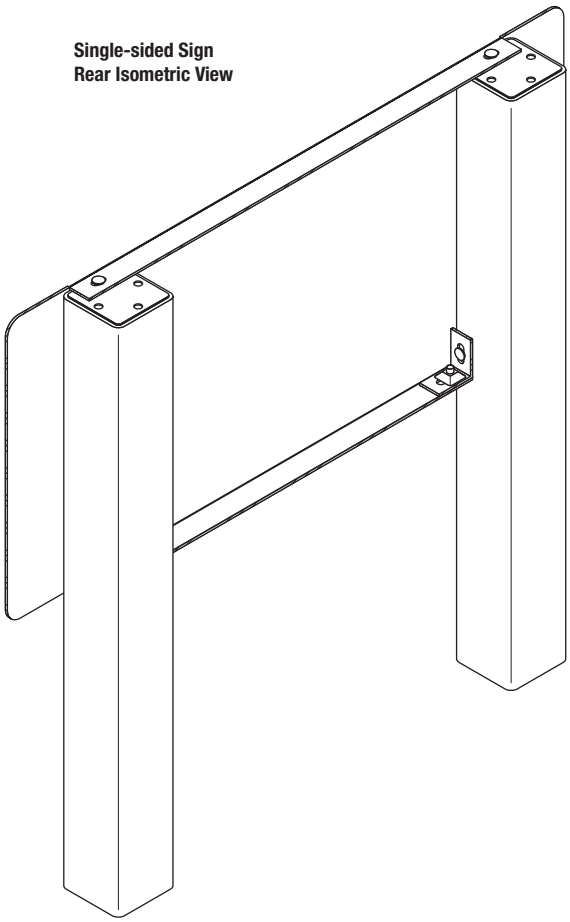
Item #	Description	Qty
W1	4" x 4" wood post	1
W2	3 mm thick aluminum sign face	1
W3	30 mm x 50 mm x 50 mm x 3 mm thick aluminum L-bracket	2
W4	Tamper-resistant nut 1/4"-20 UNC, steel case-hardened, zinc-plated	2
W5	Security Phillips Pan Head Sheet Metal Screw, Type A, #14 x 1 1/2"	3
W6	Carriage bolt round head, square neck, 1/4"-20 x 1/2"; 18-8 stainless steel	2
W7	Nylon 6/6, 1/4" flat washer	5

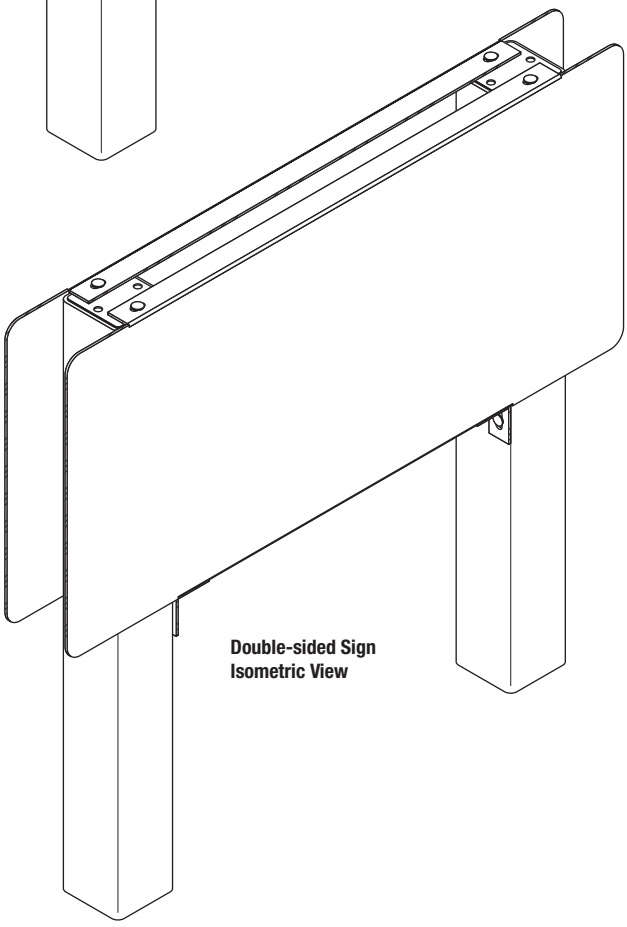
2-post Sign – Wood Posts

Single-sided Sign
Front Isometric View



Single-sided Sign
Rear Isometric View

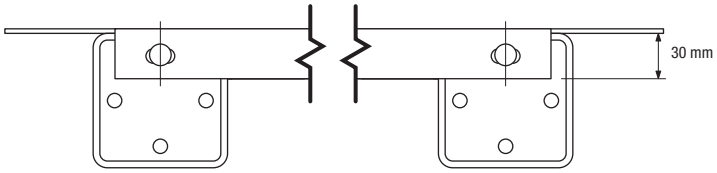




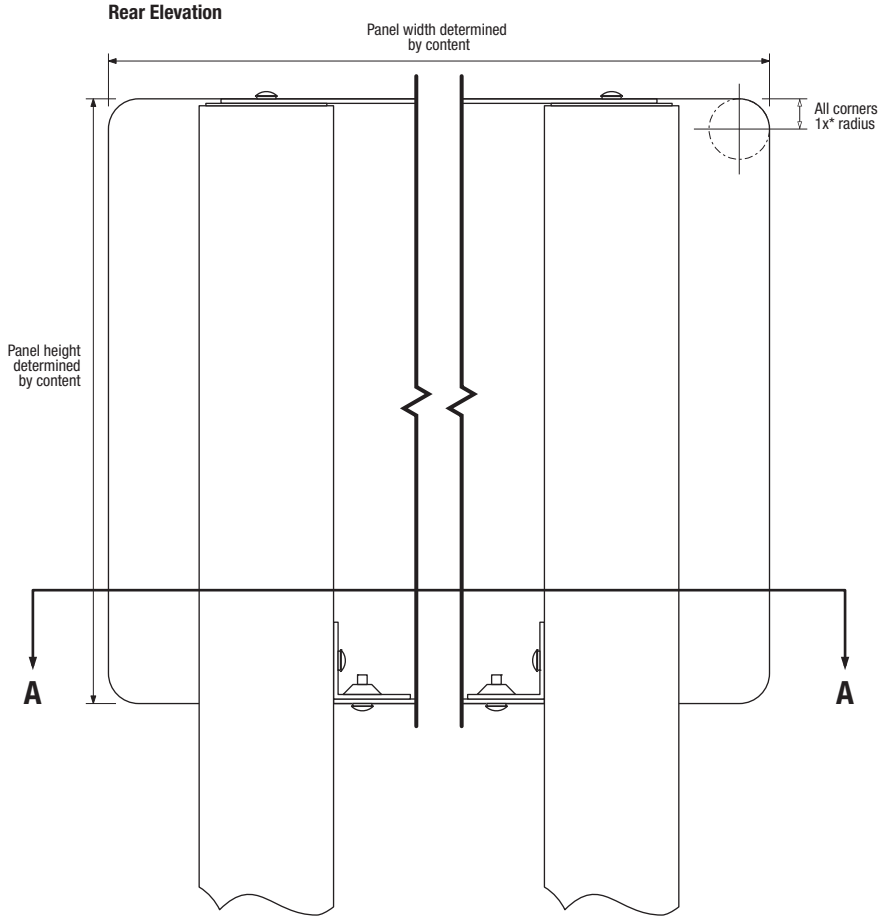
Double-sided Sign
Isometric View

2-post Sign – Wood Posts (CONTINUED)

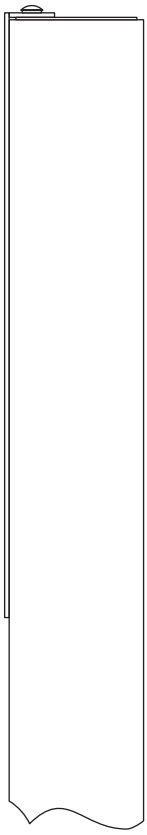
Top Plan View



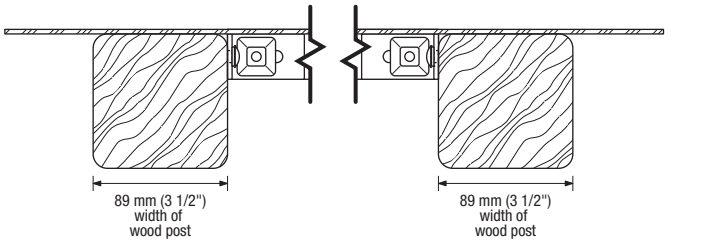
Rear Elevation



Side Elevation

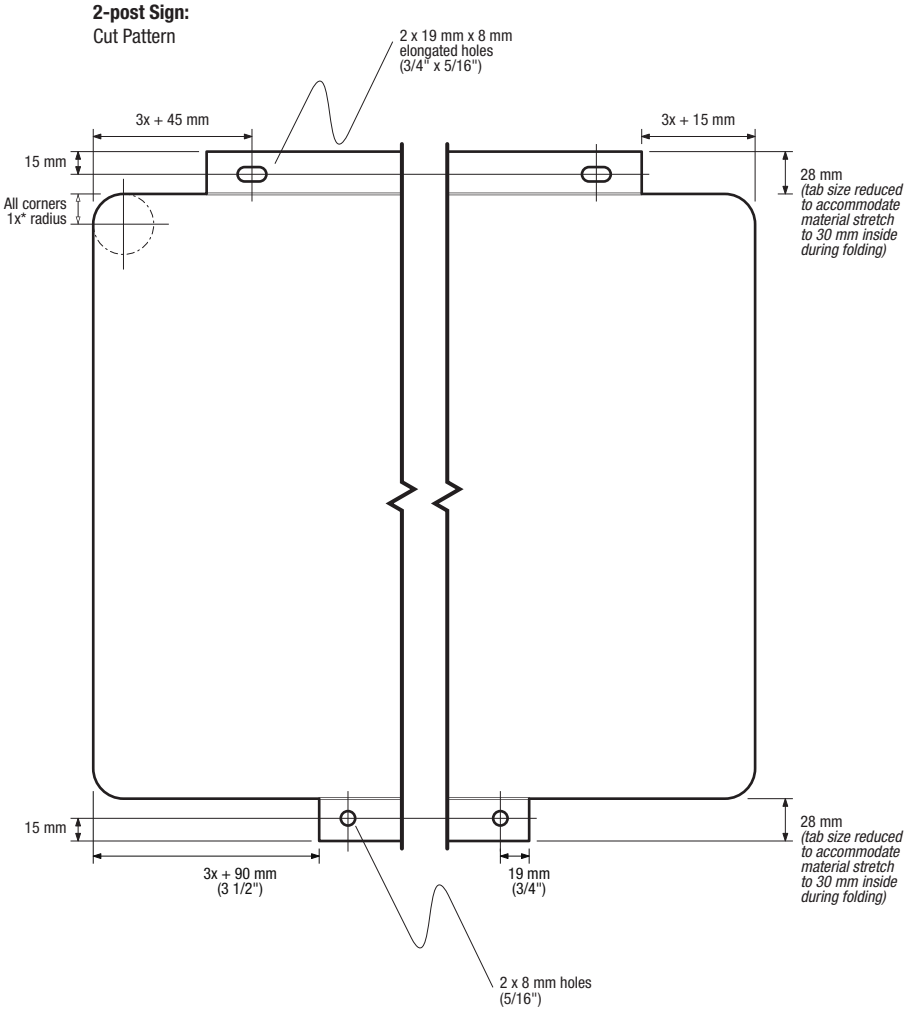


Section A-A



2-post Sign – Wood Posts (CONTINUED)

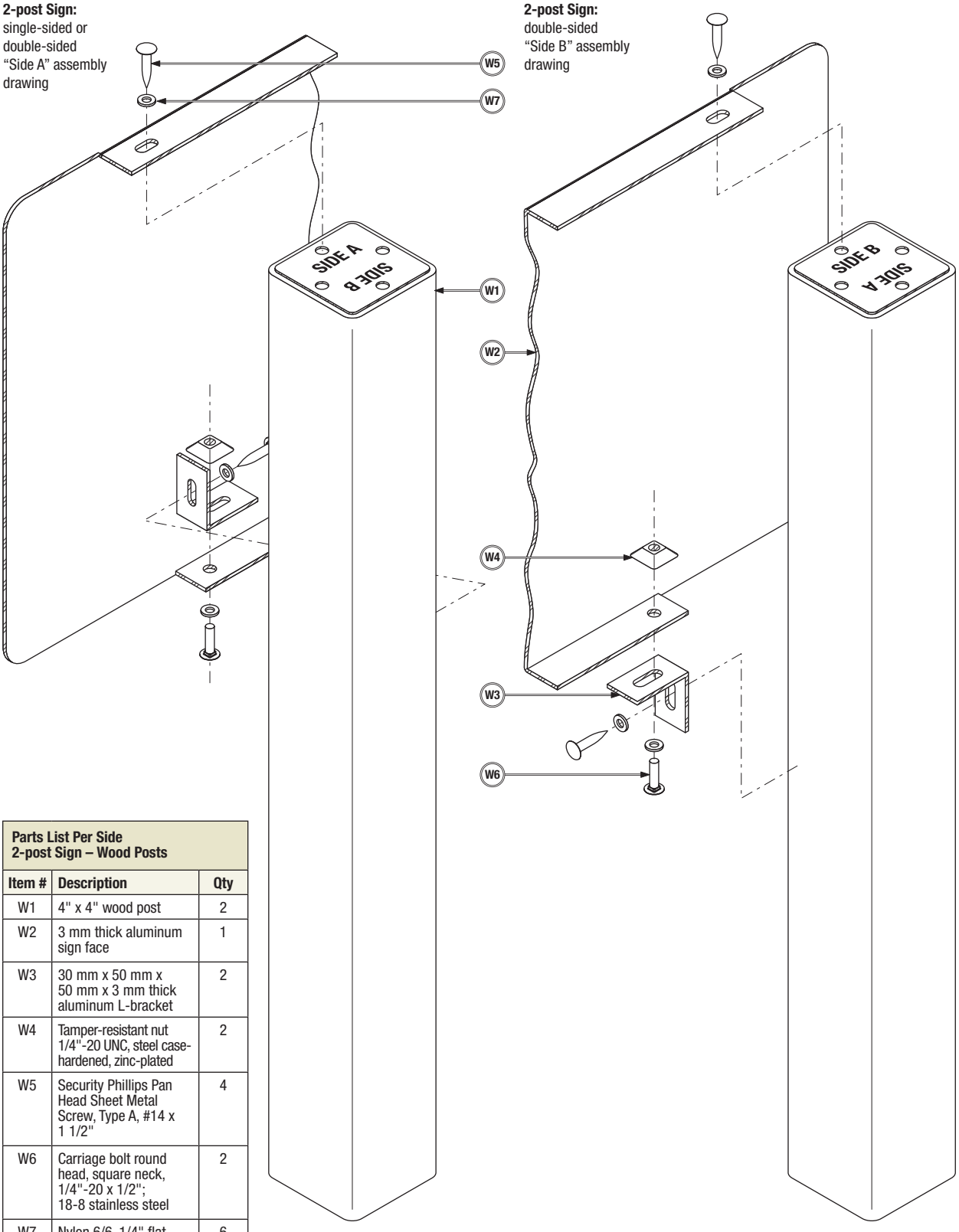
2-post Sign:
Cut Pattern



2-post Sign – Wood Posts (CONTINUED)

2-post Sign:
single-sided or
double-sided
"Side A" assembly
drawing

2-post Sign:
double-sided
"Side B" assembly
drawing

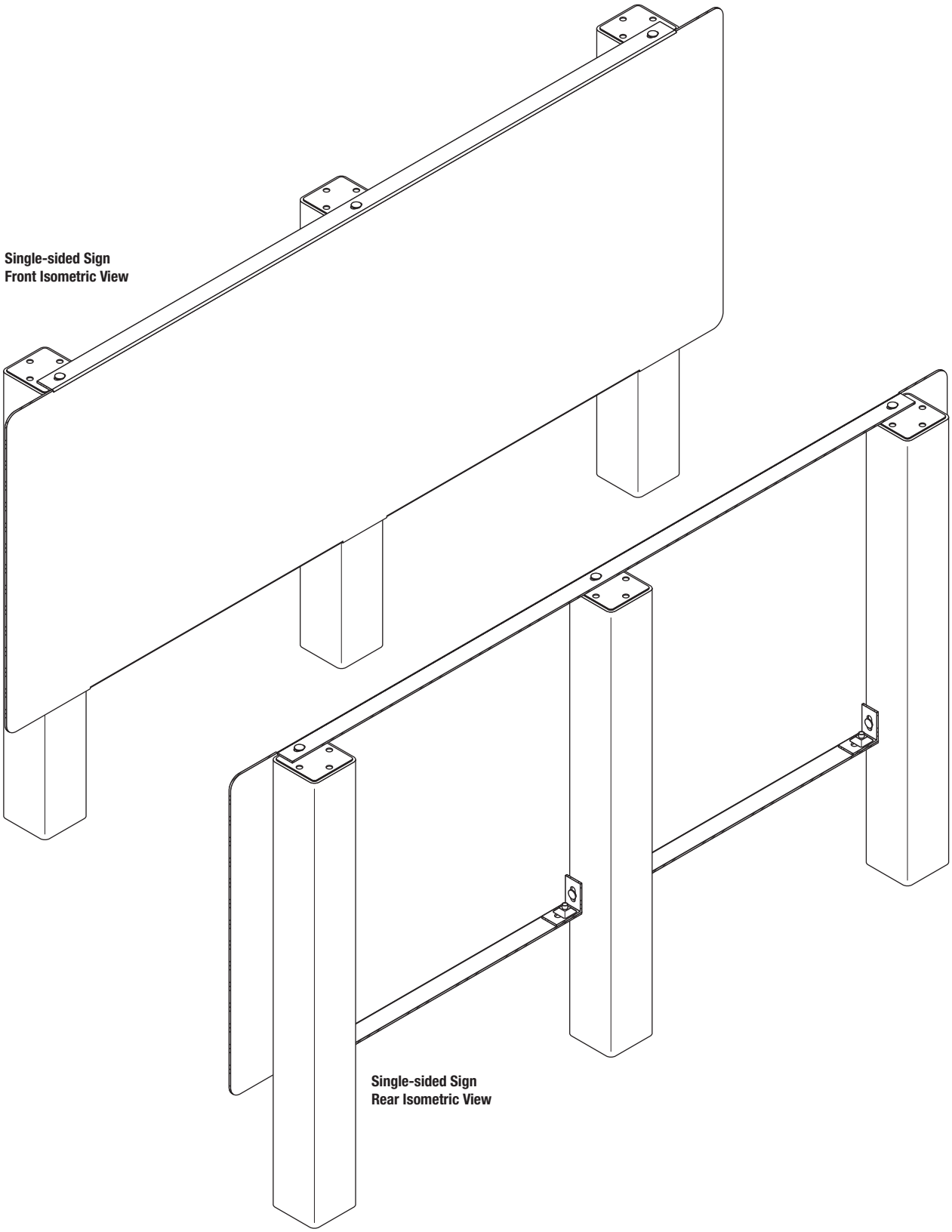


Parts List Per Side
2-post Sign – Wood Posts

Item #	Description	Qty
W1	4" x 4" wood post	2
W2	3 mm thick aluminum sign face	1
W3	30 mm x 50 mm x 50 mm x 3 mm thick aluminum L-bracket	2
W4	Tamper-resistant nut 1/4"-20 UNC, steel case-hardened, zinc-plated	2
W5	Security Phillips Pan Head Sheet Metal Screw, Type A, #14 x 1 1/2"	4
W6	Carriage bolt round head, square neck, 1/4"-20 x 1/2"; 18-8 stainless steel	2
W7	Nylon 6/6, 1/4" flat washer	6

3-post Sign – Wood Posts

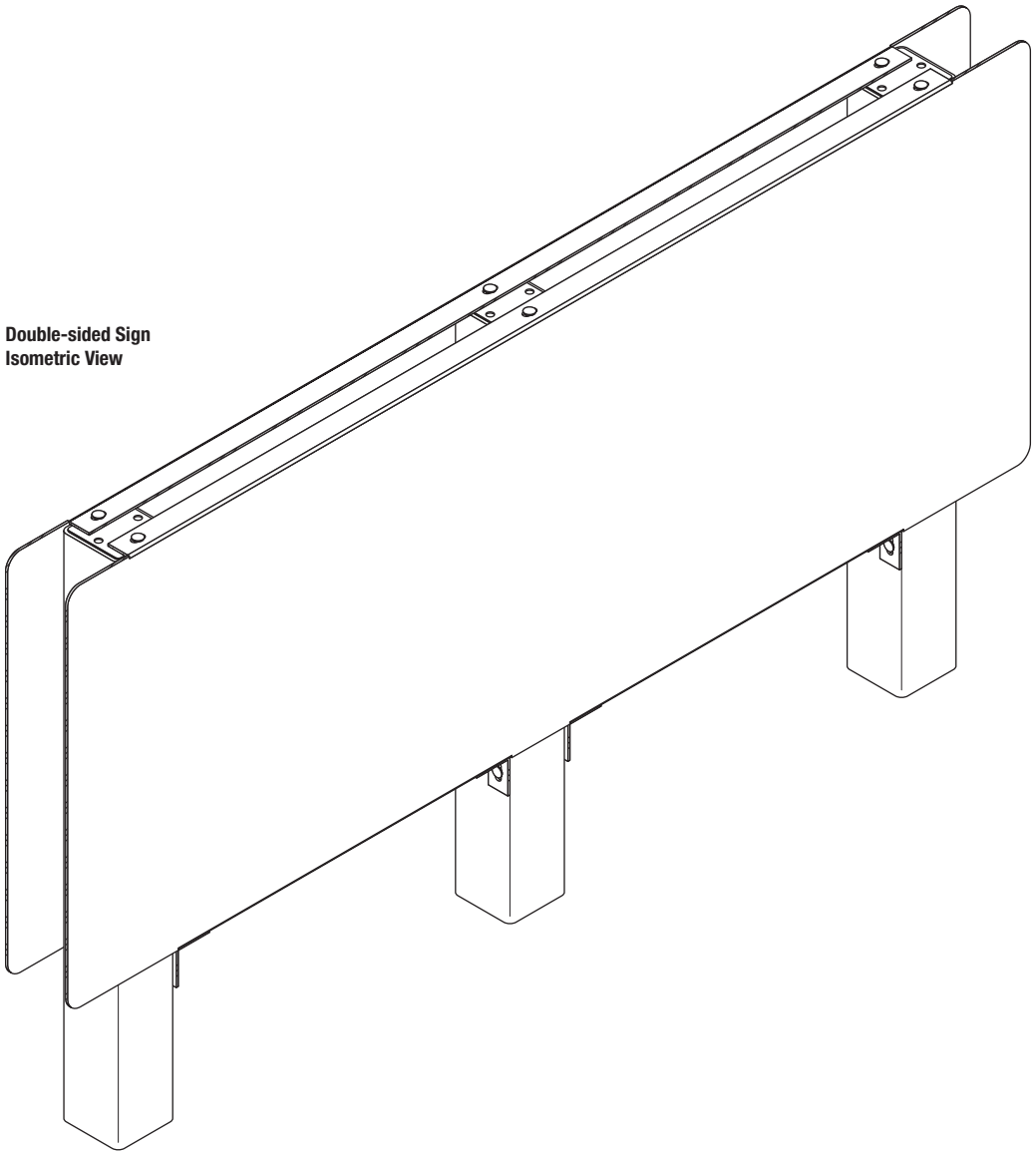
Single-sided Sign
Front Isometric View



Single-sided Sign
Rear Isometric View

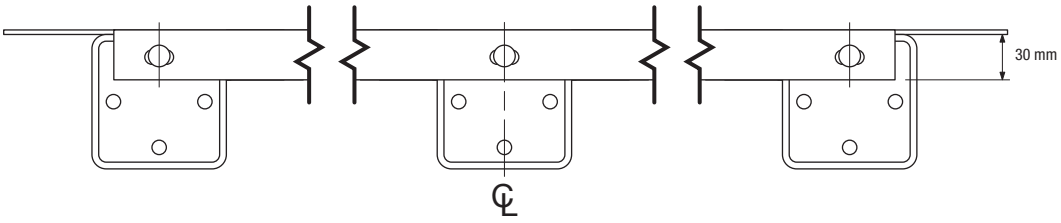
3-post Sign – Wood Posts (CONTINUED)

Double-sided Sign
Isometric View

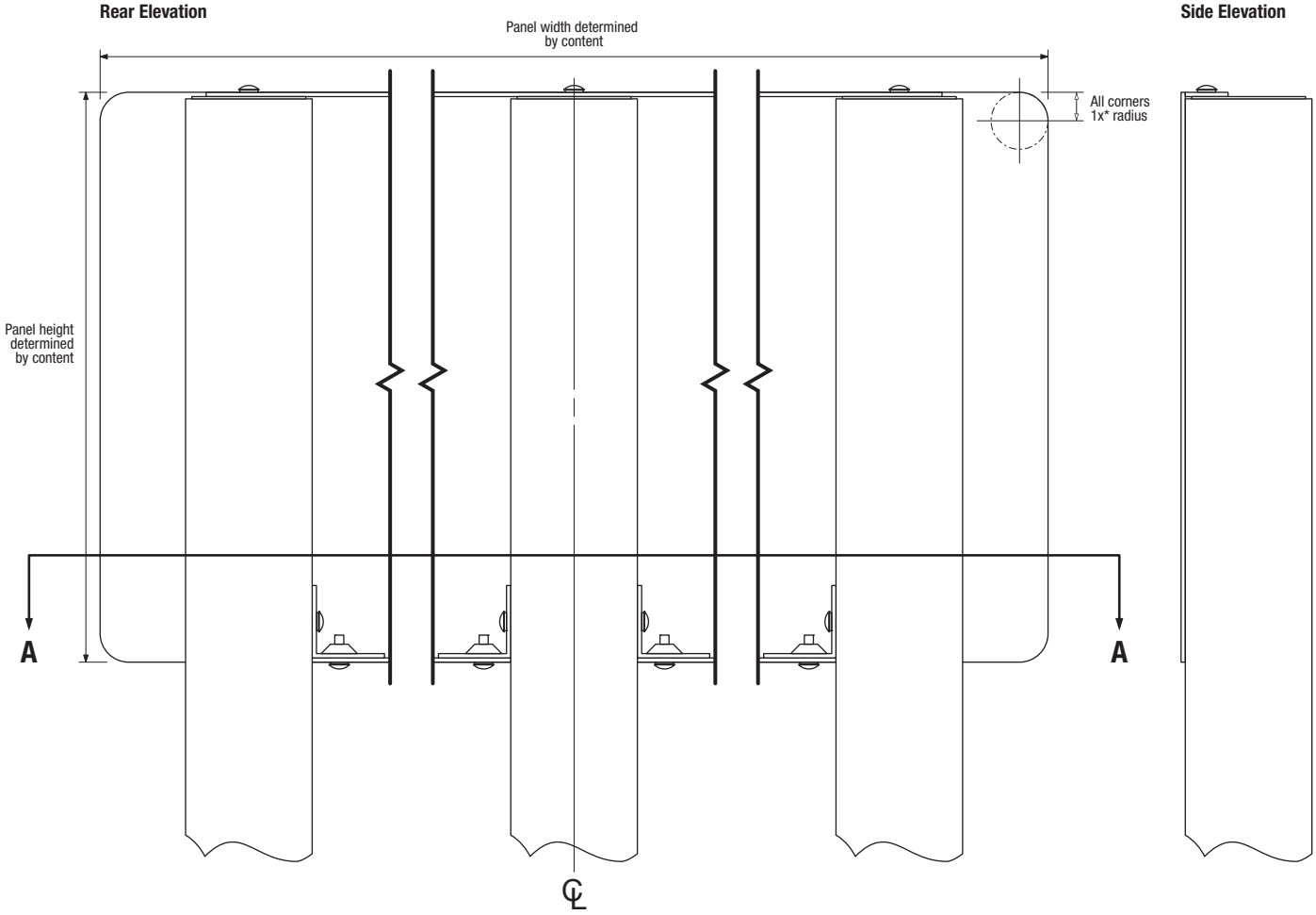


3-post Sign – Wood Posts (CONTINUED)

Top Plan View

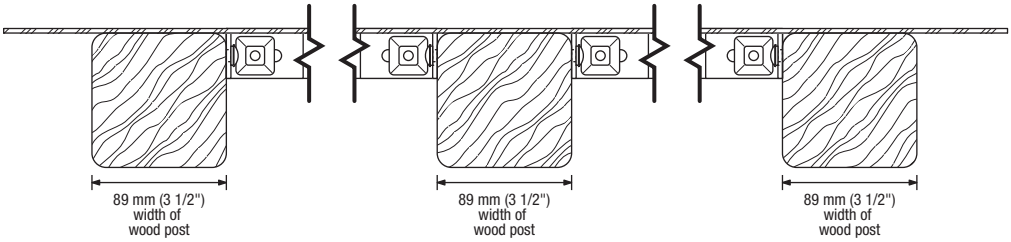


Rear Elevation



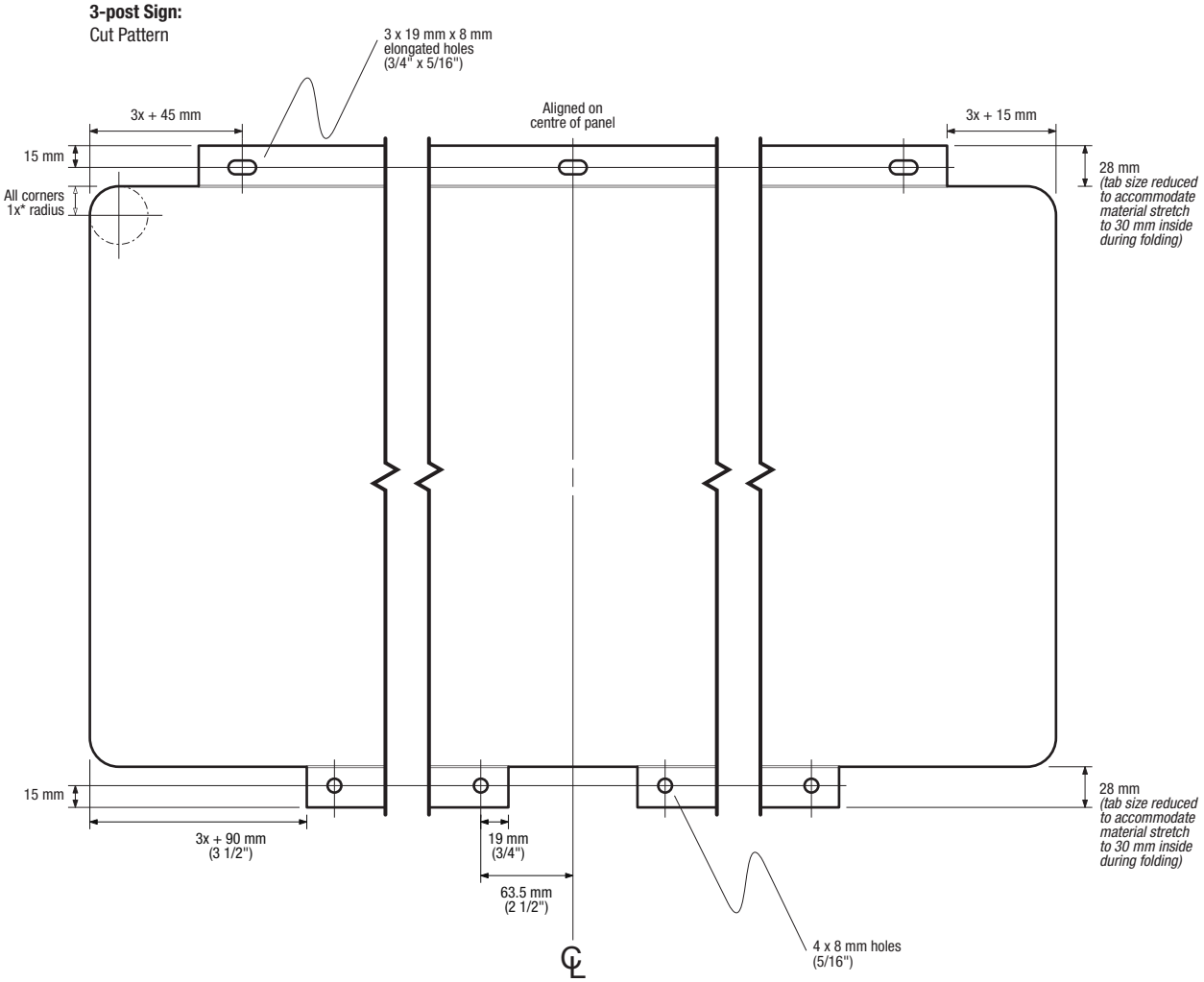
Side Elevation

Section A-A



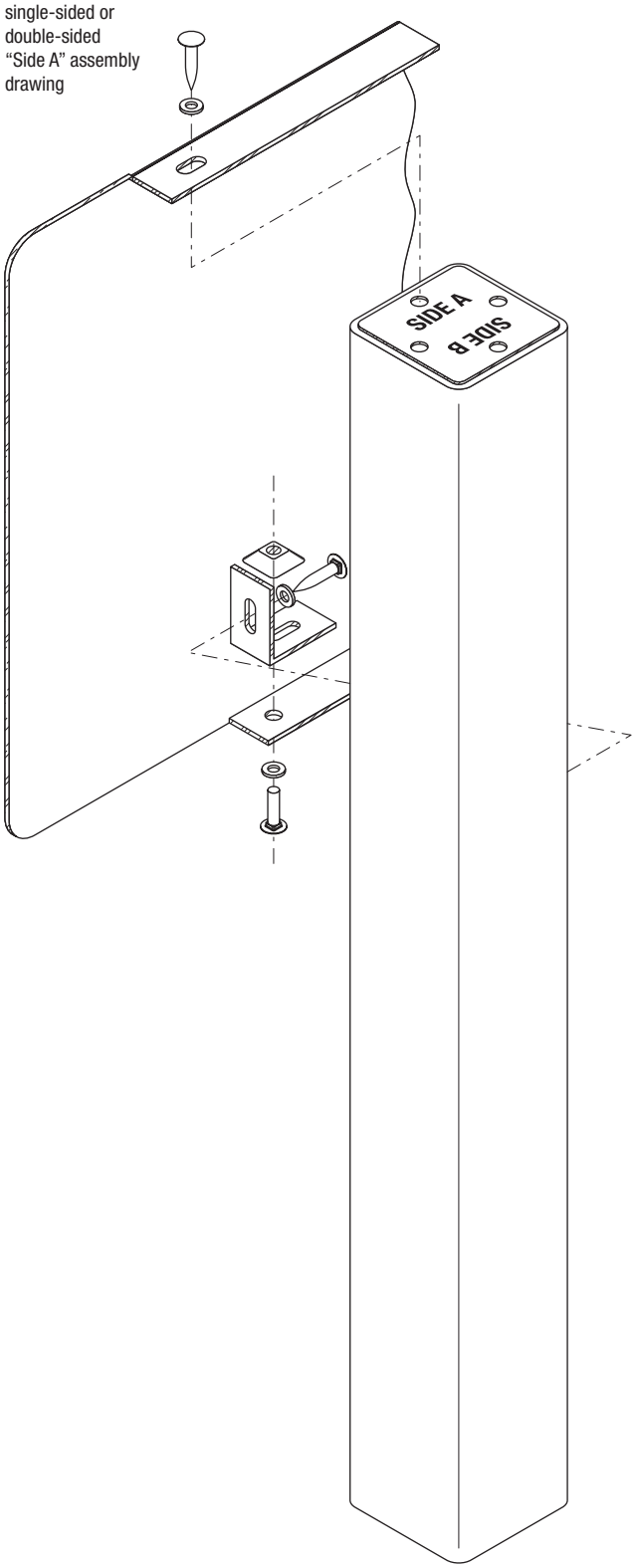
3-post Sign – Wood Posts (CONTINUED)

3-post Sign:
Cut Pattern

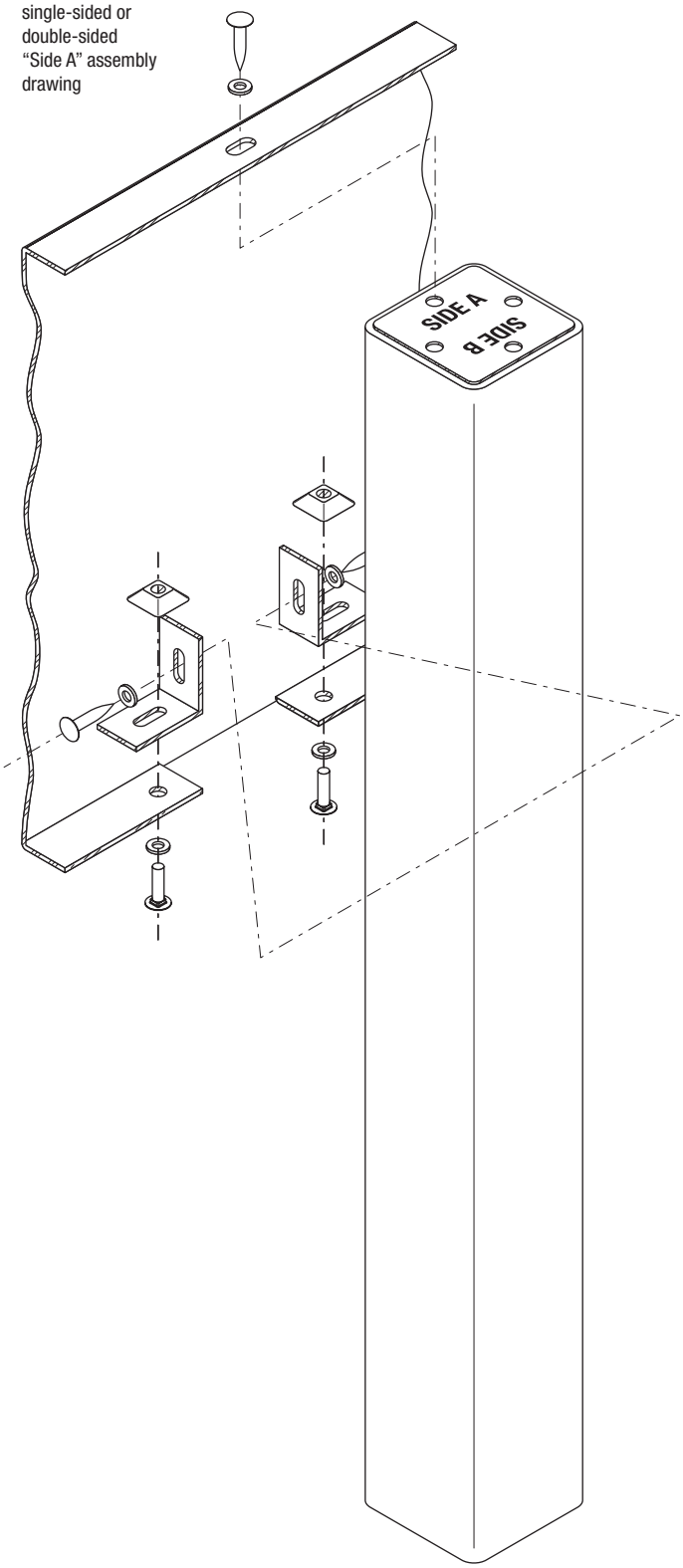


3-post Sign – Wood Posts (CONTINUED)

3-post Sign End Post:
single-sided or
double-sided
"Side A" assembly
drawing



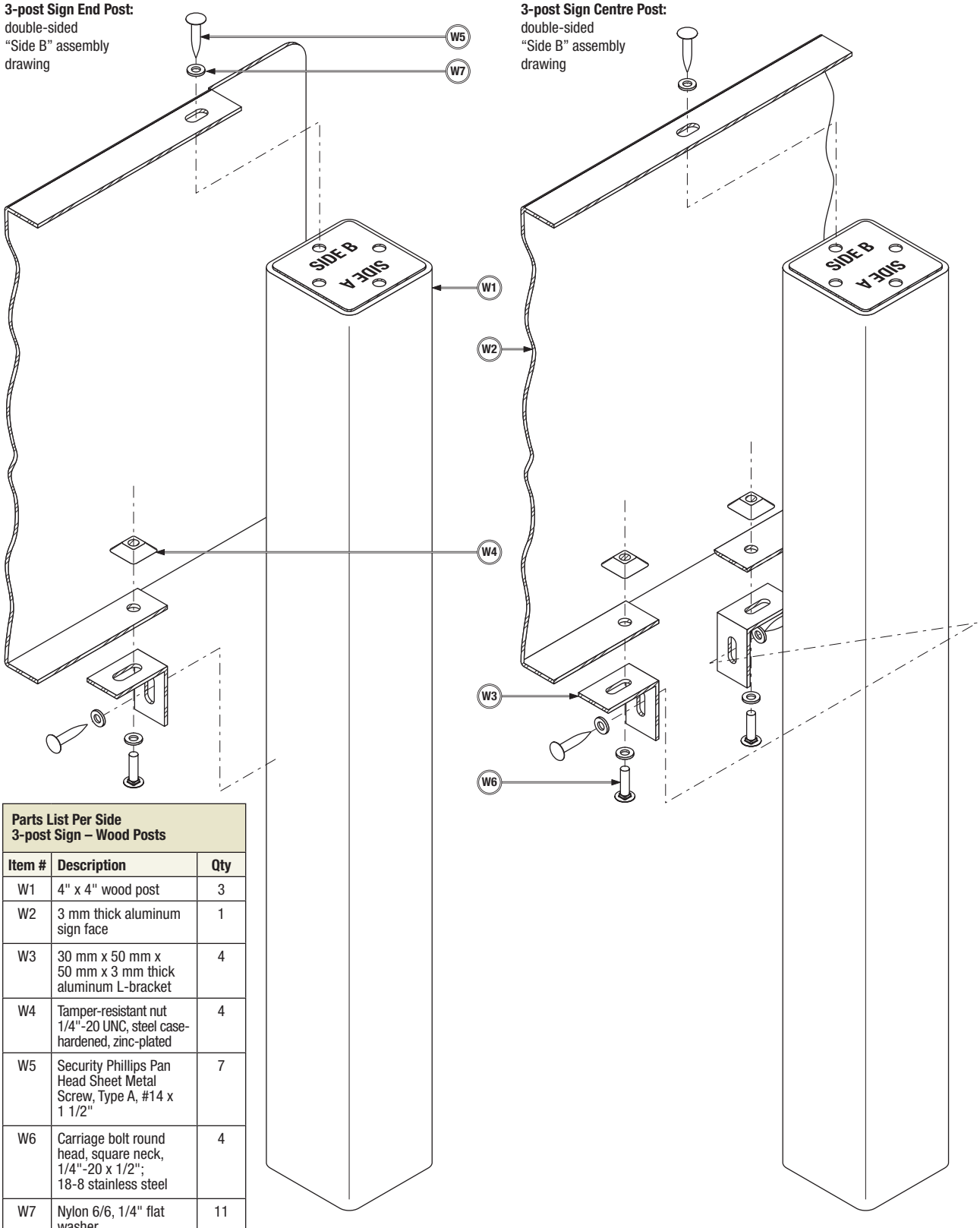
3-post Sign Centre Post:
single-sided or
double-sided
"Side A" assembly
drawing



3-post Sign – Wood Posts (CONTINUED)

3-post Sign End Post:
double-sided
"Side B" assembly
drawing

3-post Sign Centre Post:
double-sided
"Side B" assembly
drawing

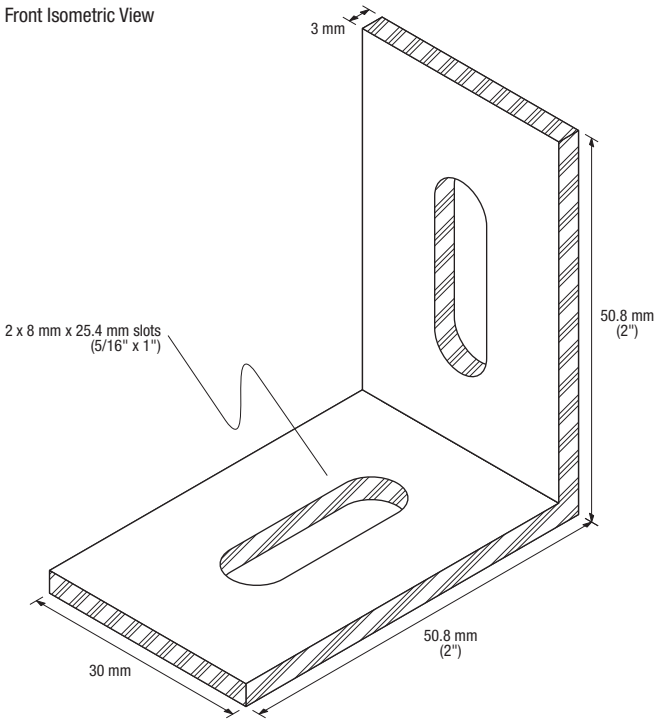


Parts List Per Side
3-post Sign – Wood Posts

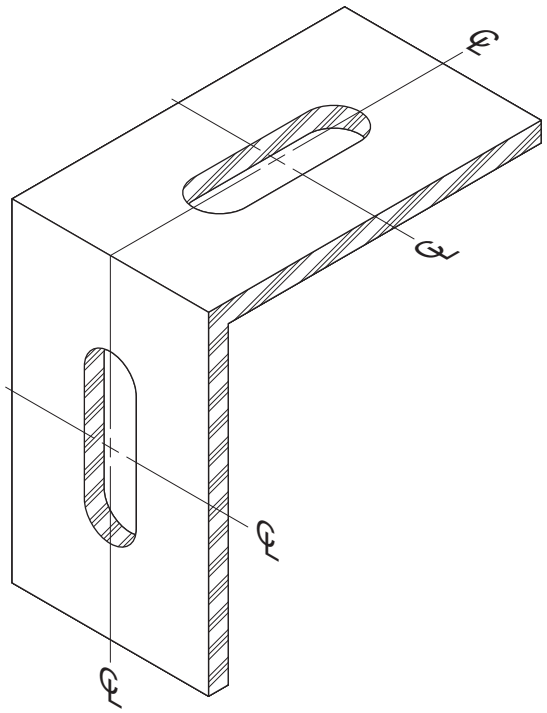
Item #	Description	Qty
W1	4" x 4" wood post	3
W2	3 mm thick aluminum sign face	1
W3	30 mm x 50 mm x 50 mm x 3 mm thick aluminum L-bracket	4
W4	Tamper-resistant nut 1/4"-20 UNC, steel case-hardened, zinc-plated	4
W5	Security Phillips Pan Head Sheet Metal Screw, Type A, #14 x 1 1/2"	7
W6	Carriage bolt round head, square neck, 1/4"-20 x 1/2"; 18-8 stainless steel	4
W7	Nylon 6/6, 1/4" flat washer	11

L-bracket

L-bracket:
Front Isometric View

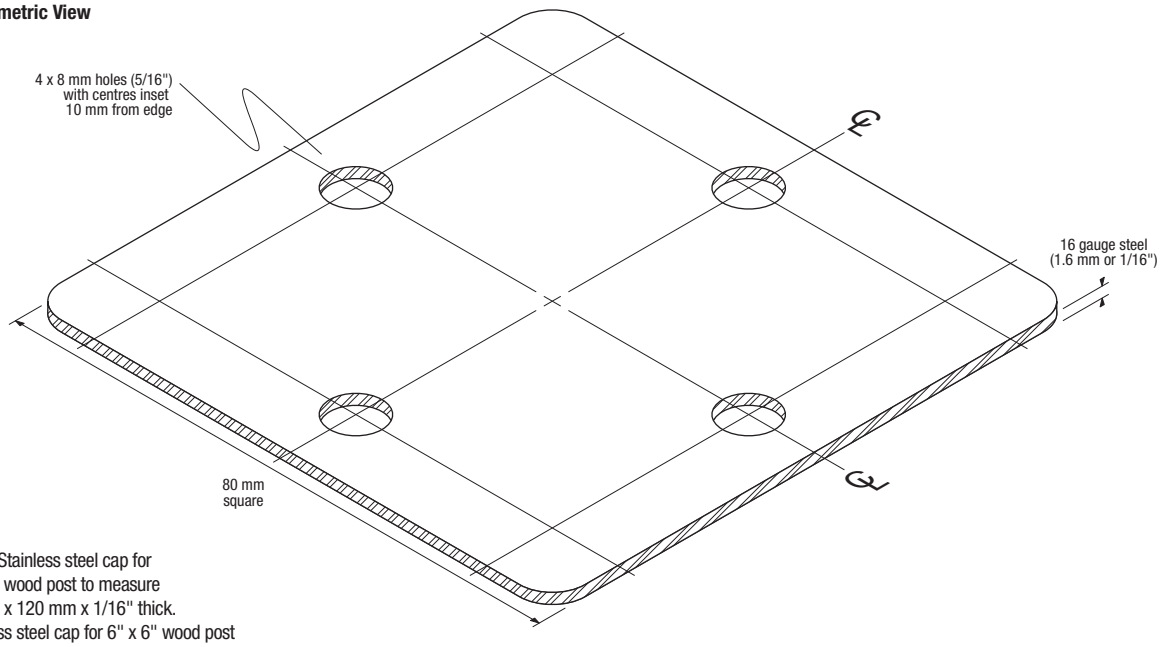


L-bracket:
Rear Isometric View



Steel Cap For Wood Posts

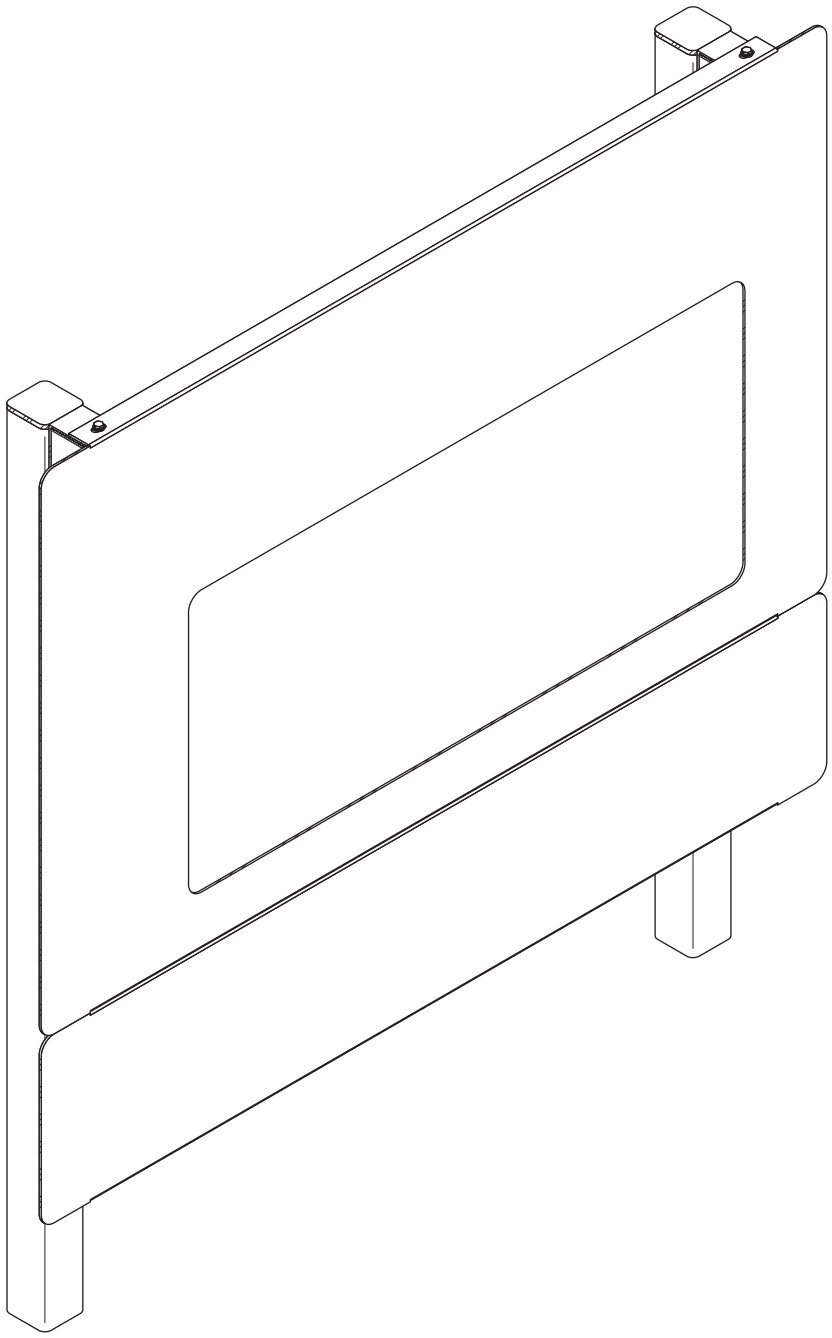
Steel Cap For
4" X 4" Wood Posts
Top Isometric View



NOTE: Stainless steel cap for 4" x 6" wood post to measure 80 mm x 120 mm x 1/16" thick. Stainless steel cap for 6" x 6" wood post to measure 120 mm x 120 mm x 1/16" thick. Holes and offsets as specified in drawing above.

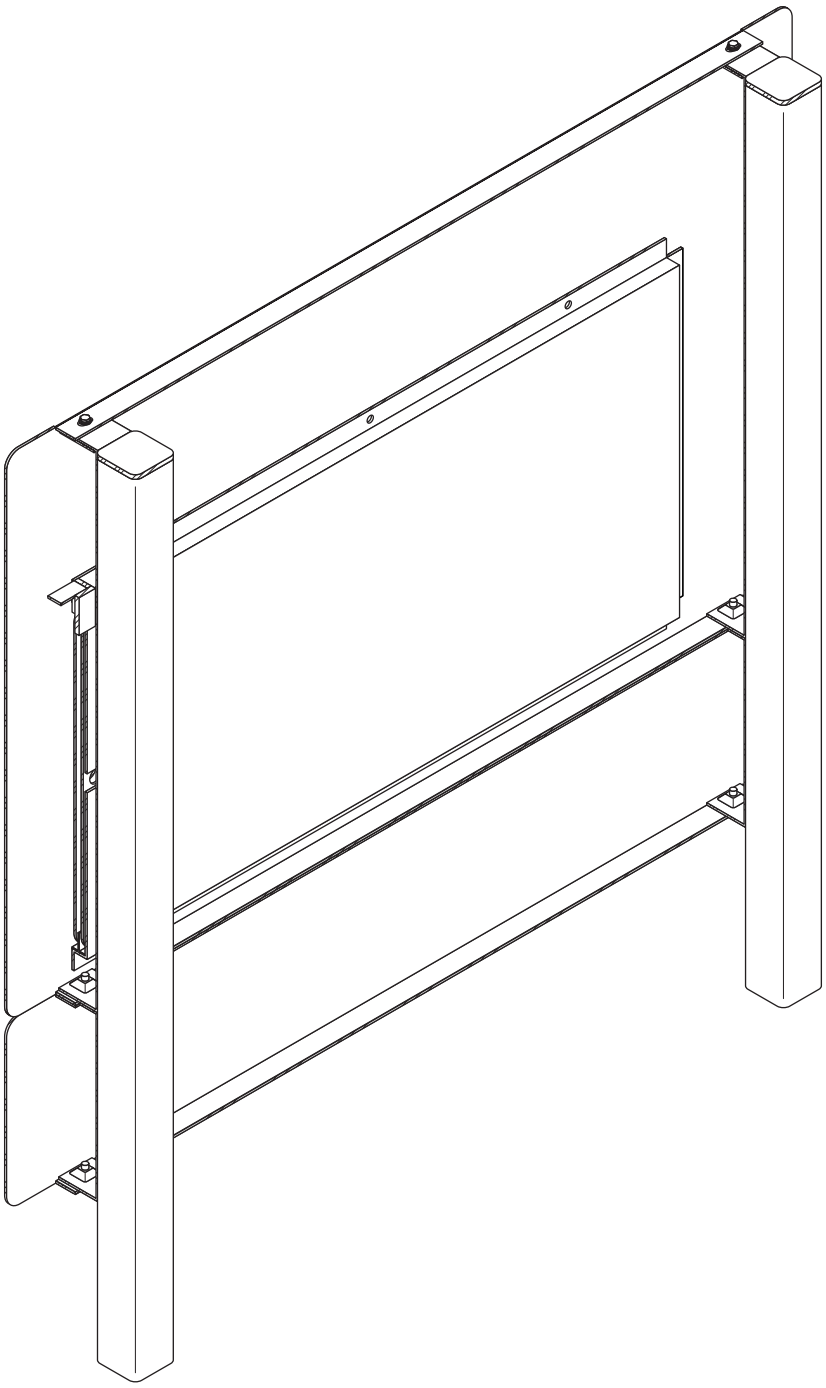
Fee Sign

Fee Sign
Front Isometric View



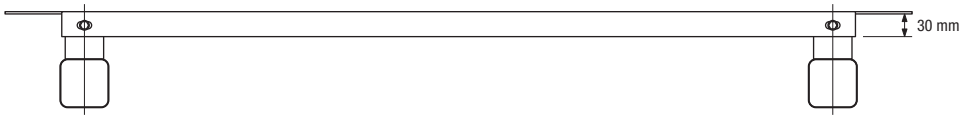
Fee Sign (CONTINUED)

Fee Sign
Rear Isometric View

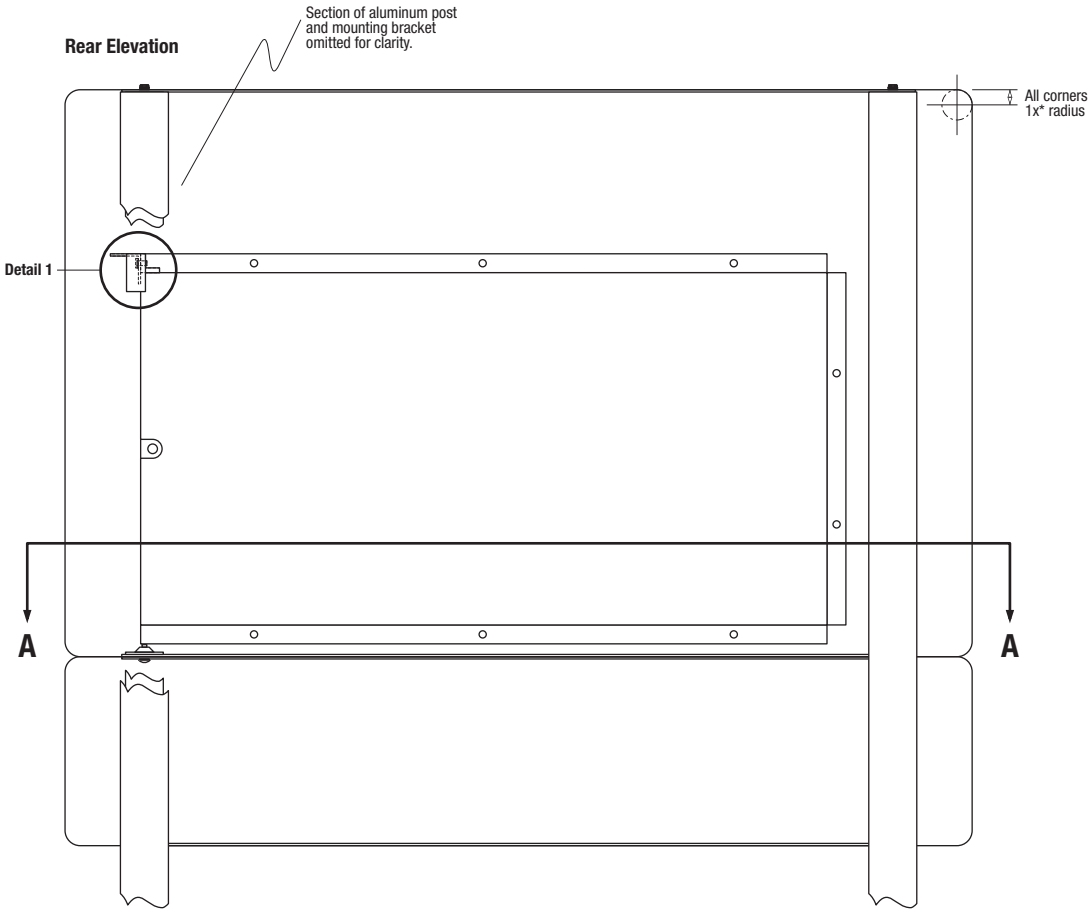


Fee Sign (CONTINUED)

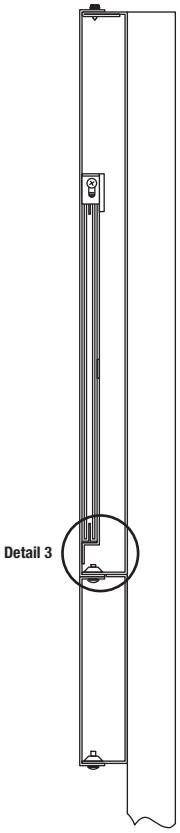
Top Plan View



Rear Elevation

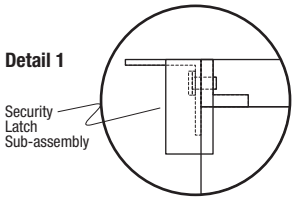


Side Elevation

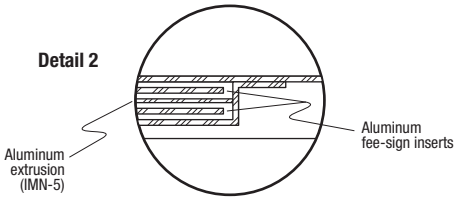


Section A-A

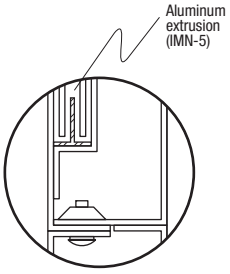
Detail 1



Detail 2

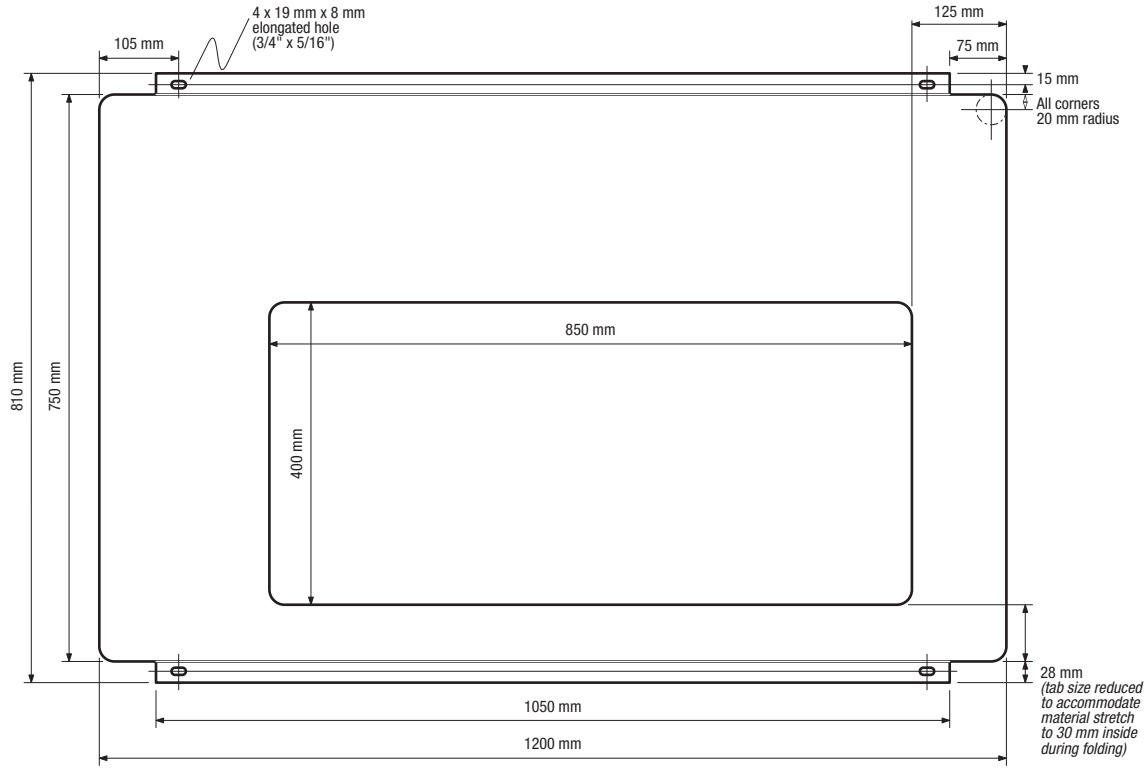


Detail 3

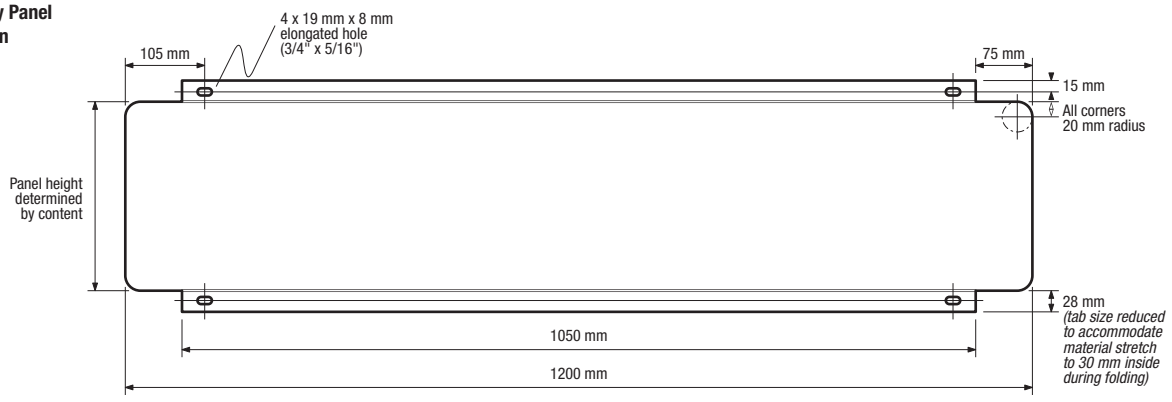


Fee Sign (CONTINUED)

Fee Sign Main Panel Cut Pattern

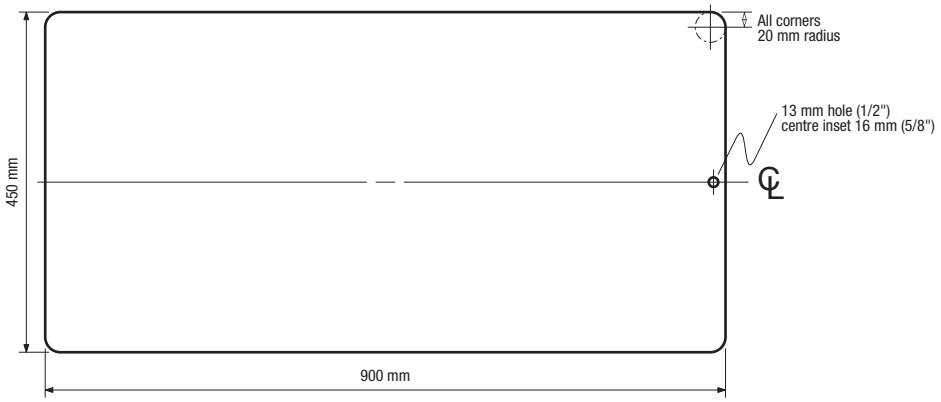


Fee Sign Optional Secondary Panel Cut Pattern

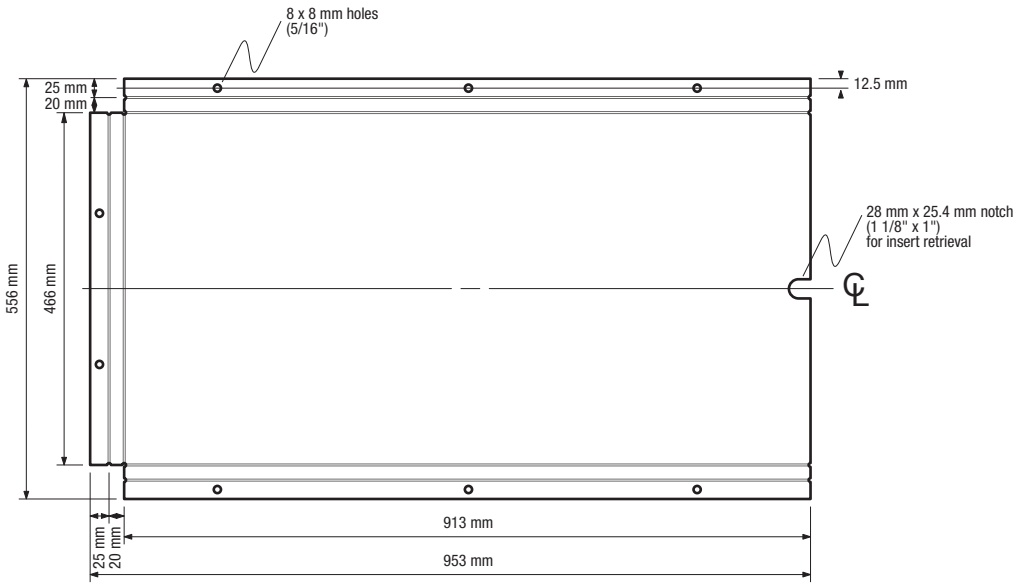


Fee Sign (CONTINUED)

Fee Sign Insert
Cut Pattern

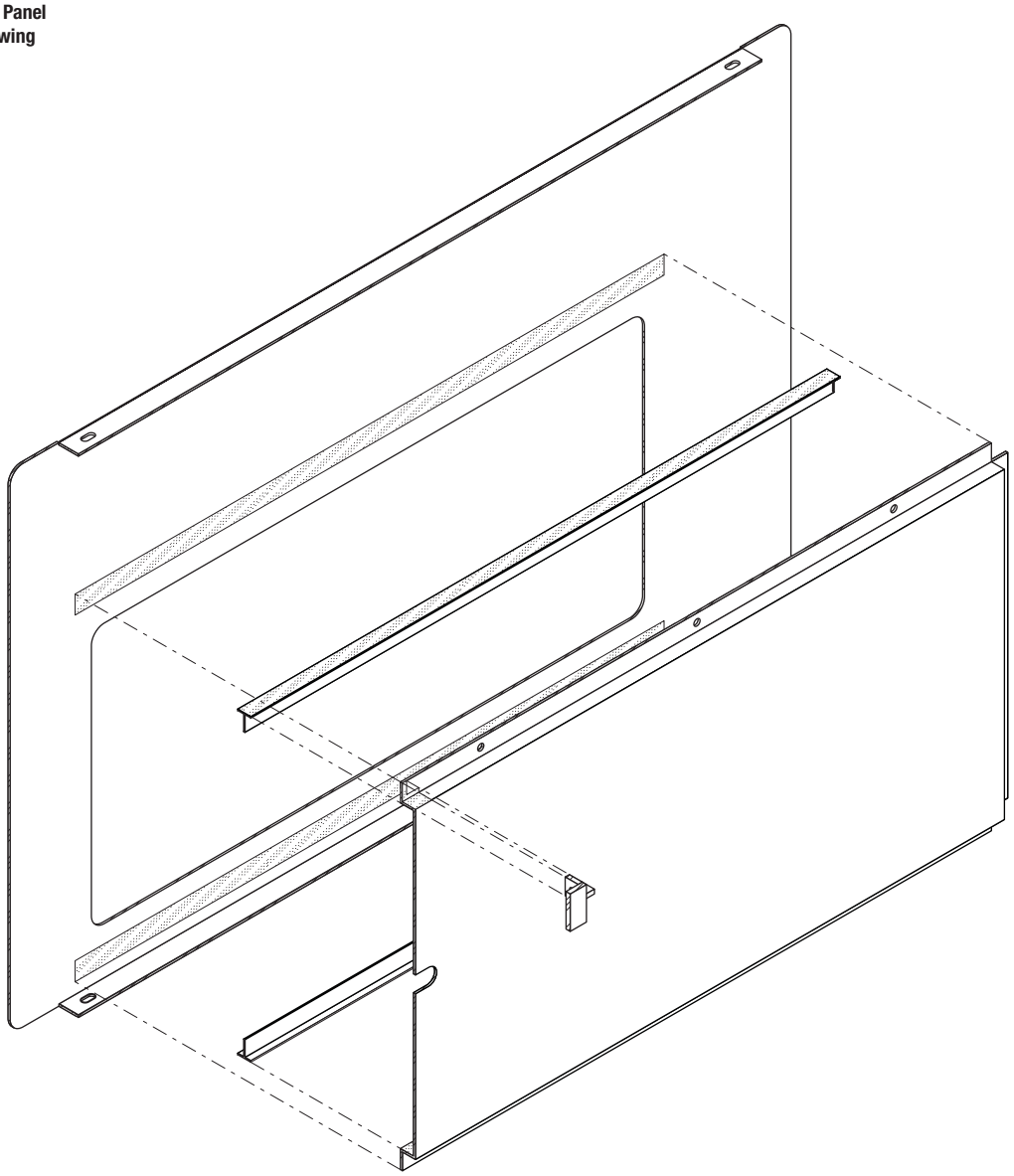


Fee Sign
Insert Housing
Cut Pattern



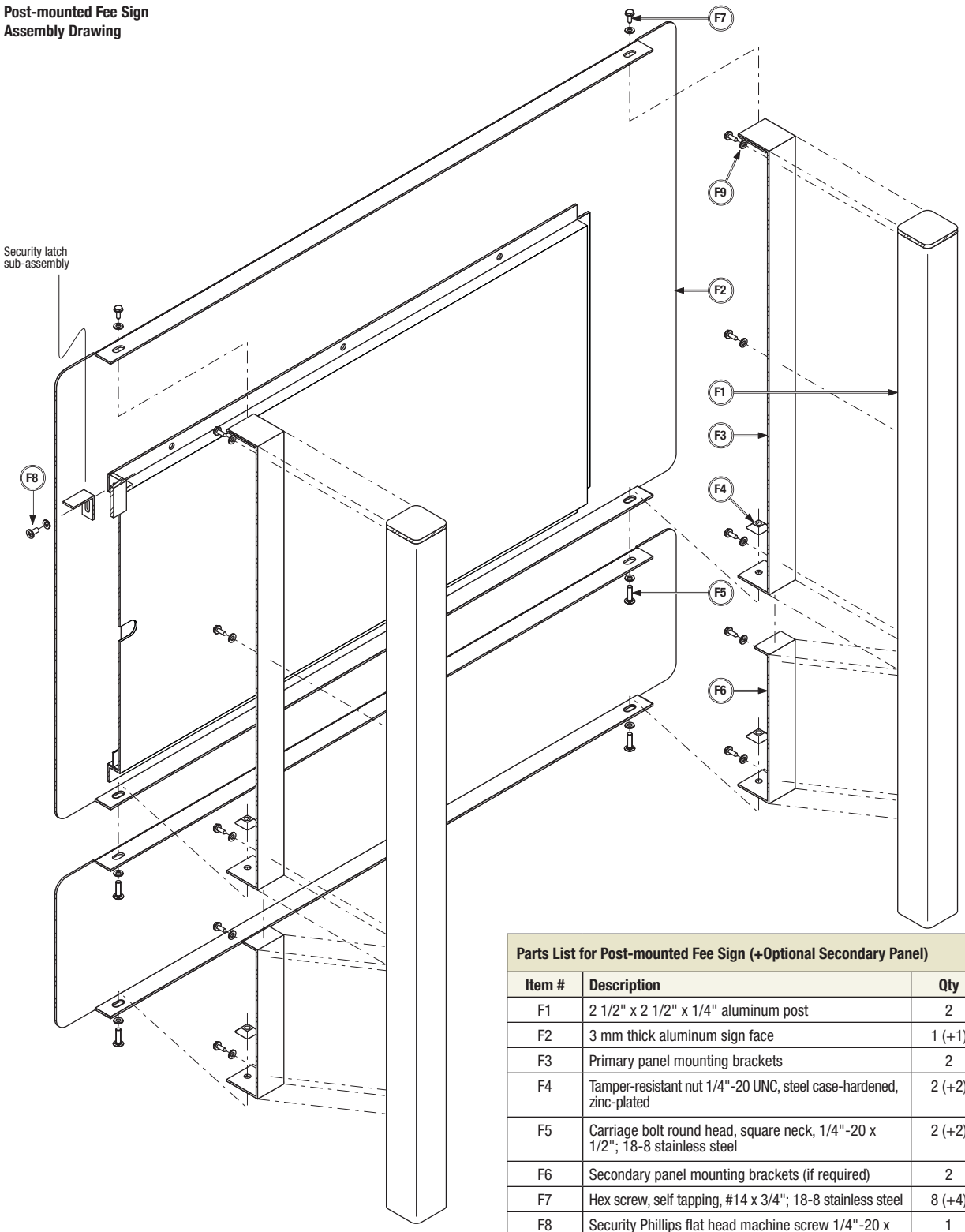
Fee Sign (CONTINUED)

Fee Sign Main Panel
Assembly Drawing



Fee Sign (CONTINUED)

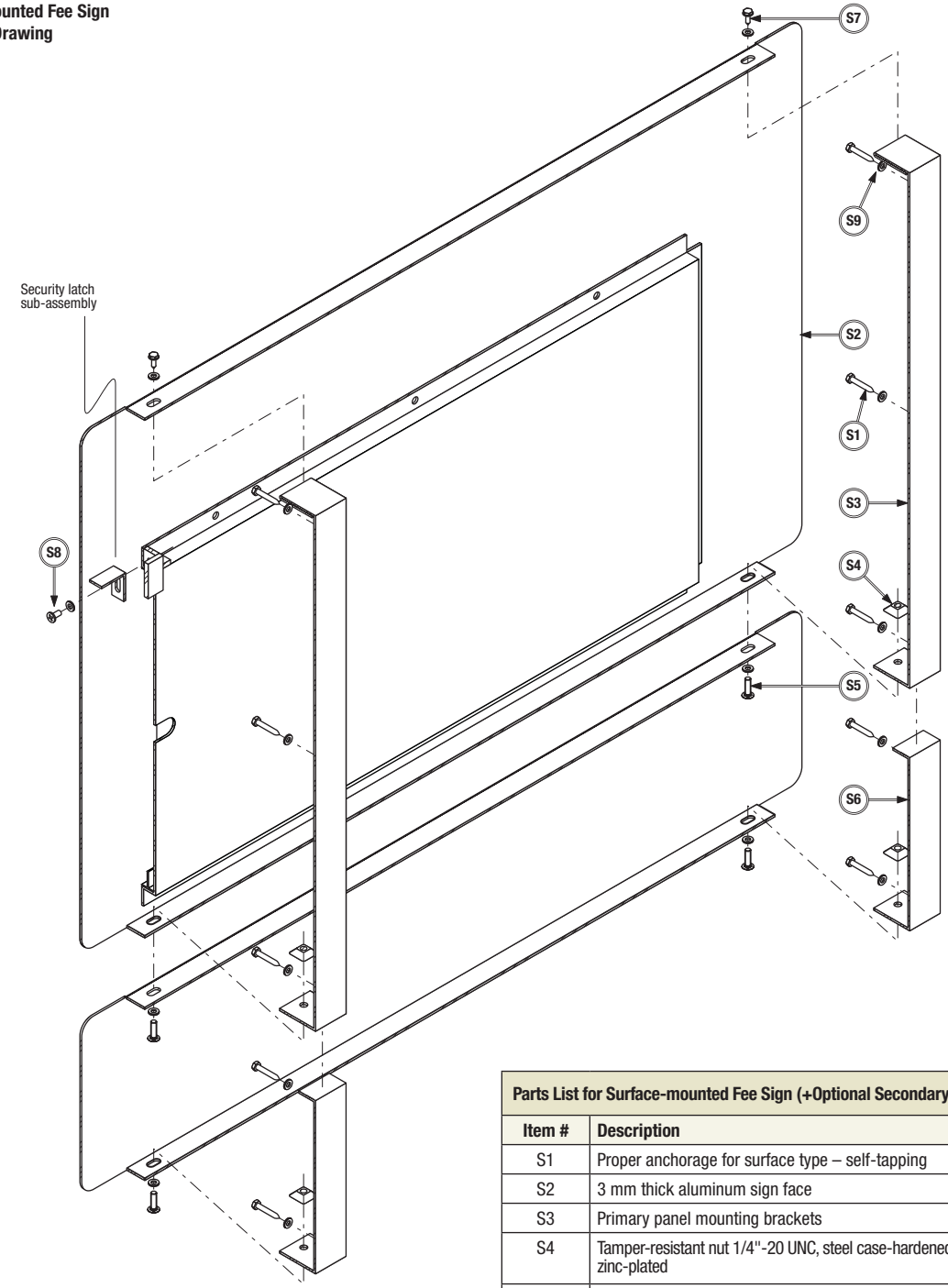
Post-mounted Fee Sign
Assembly Drawing



Parts List for Post-mounted Fee Sign (+Optional Secondary Panel)		
Item #	**Description**	**Qty**
F1	2 1/2" x 2 1/2" x 1/4" aluminum post	2
F2	3 mm thick aluminum sign face	1 (+1)
F3	Primary panel mounting brackets	2
F4	Tamper-resistant nut 1/4"-20 UNC, steel case-hardened, zinc-plated	2 (+2)
F5	Carriage bolt round head, square neck, 1/4"-20 x 1/2"; 18-8 stainless steel	2 (+2)
F6	Secondary panel mounting brackets (if required)	2
F7	Hex screw, self tapping, #14 x 3/4"; 18-8 stainless steel	8 (+4)
F8	Security Phillips flat head machine screw 1/4"-20 x 1/2" long	1
F9	Nylon 6/6, 1/4" flat washer	11 (+6)

Fee Sign (CONTINUED)

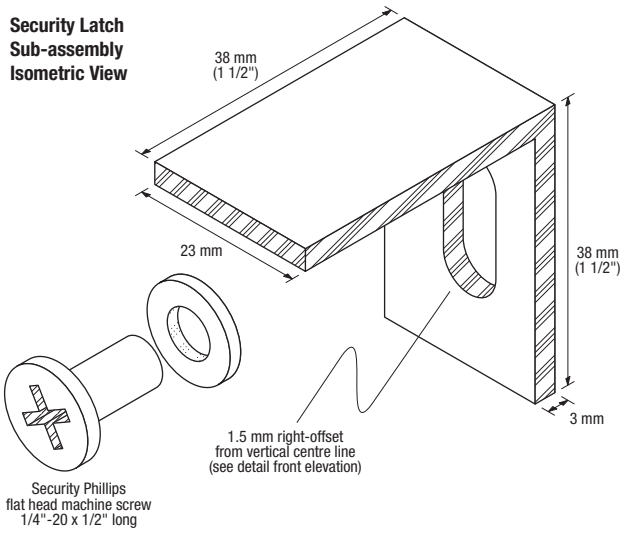
Surface-mounted Fee Sign
Assembly Drawing



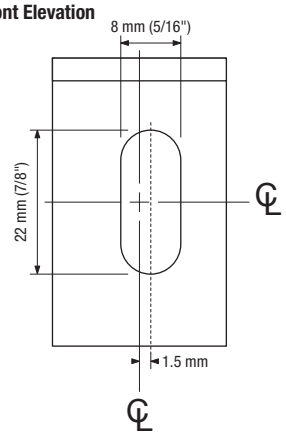
Parts List for Surface-mounted Fee Sign (+Optional Secondary Panel)		
Item #	**Description**	**Qty**
S1	Proper anchorage for surface type – self-tapping	6 (+4)
S2	3 mm thick aluminum sign face	1 (+1)
S3	Primary panel mounting brackets	2
S4	Tamper-resistant nut 1/4"-20 UNC, steel case-hardened, zinc-plated	2 (+2)
S5	Carriage bolt round head, square neck, 1/4"-20 x 1/2"; 18-8 stainless steel	2 (+2)
S6	Secondary panel mounting brackets (if required)	2
S7	Hex screw, self tapping, #14 x 3/4"; 18-8 stainless steel	2
S8	Security Phillips flat head machine screw 1/4"-20 x 1/2" long	1
S9	Nylon 6/6, 1/4" flat washer	11 (+6)

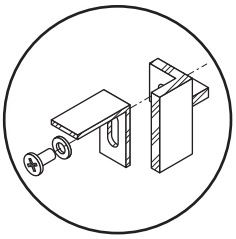
Security Latch Sub-assembly

Security Latch Sub-assembly Isometric View



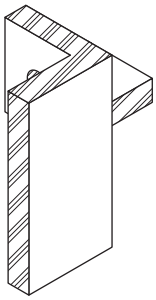
Theft-proof Sliding Latch Front Elevation

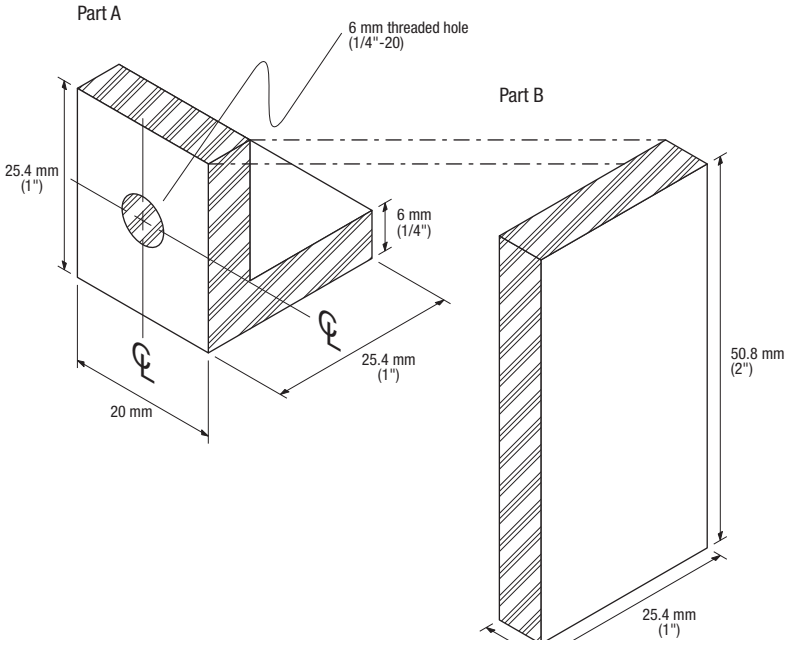


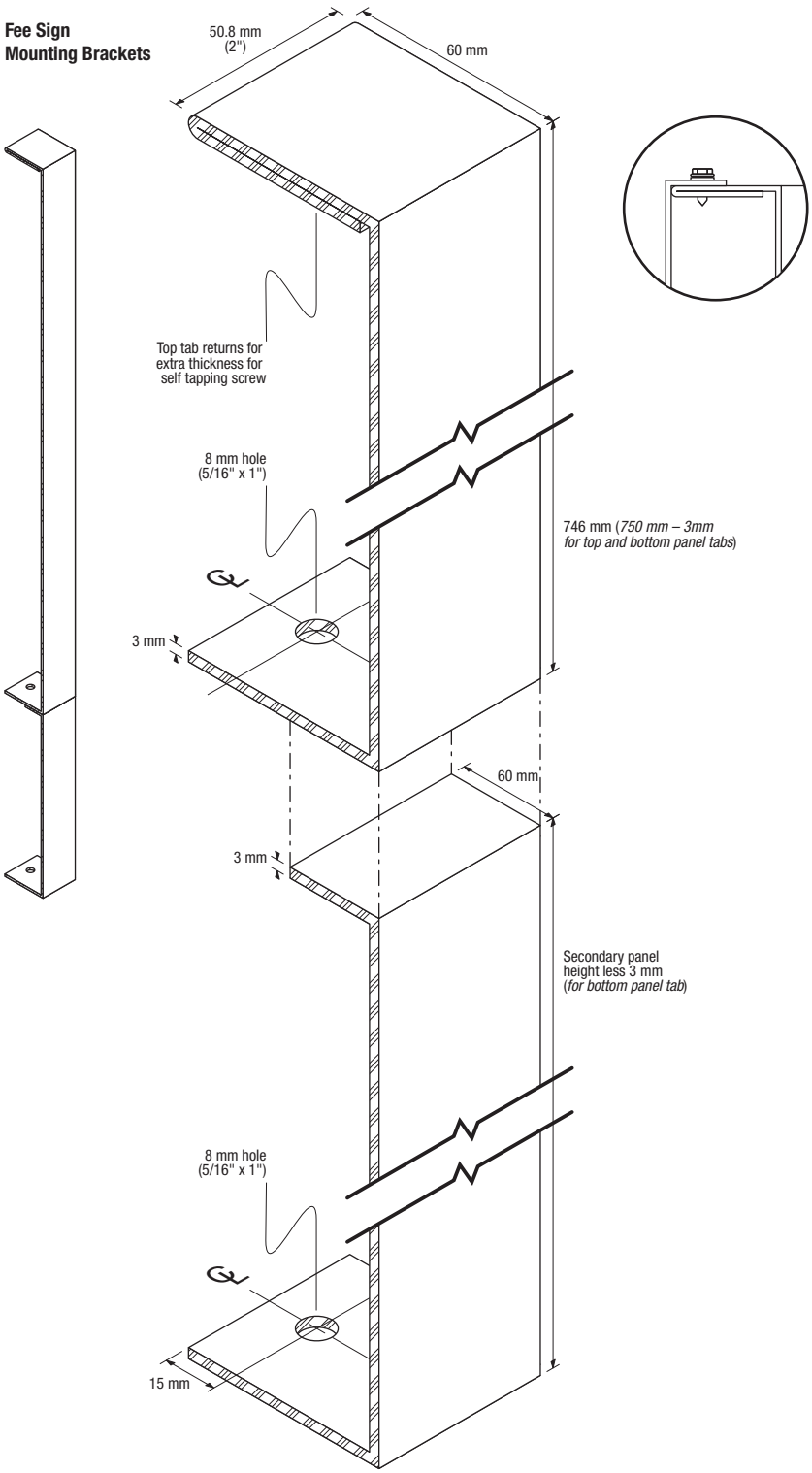


Theft-proof Latch Mount Isometric View

Parts A and B
Welded Together





Fee Sign Mounting Brackets

Fee Sign Mounting Brackets